



WOKINGHAM BOROUGH COUNCIL

A Meeting of the **PLANNING COMMITTEE** will be held virtually on **WEDNESDAY 10 FEBRUARY 2021 AT 7.00 PM**

Susan Parsonage
Chief Executive
Published on 2 February 2021

This meeting will be filmed for inclusion on the Council's website.

Note: The Council has made arrangements under the Coronavirus Act 2020 to hold this meeting virtually via Microsoft Teams. The meeting can be watched live using the following link: <https://youtu.be/xQrZlohxJ1g>

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WOKINGHAM BOROUGH COUNCIL

Our Vision

A great place to live, learn, work and grow and a great place to do business

Enriching Lives

- Champion outstanding education and enable our children and young people to achieve their full potential, regardless of their background.
- Support our residents to lead happy, healthy lives and provide access to good leisure facilities to complement an active lifestyle.
- Engage and involve our communities through arts and culture and create a sense of identity which people feel part of.
- Support growth in our local economy and help to build business.

Safe, Strong, Communities

- Protect and safeguard our children, young and vulnerable people.
- Offer quality care and support, at the right time, to prevent the need for long term care.
- Nurture communities and help them to thrive.
- Ensure our borough and communities remain safe for all.

A Clean and Green Borough

- Do all we can to become carbon neutral and sustainable for the future.
- Protect our borough, keep it clean and enhance our green areas.
- Reduce our waste, improve biodiversity and increase recycling.
- Connect our parks and open spaces with green cycleways.

Right Homes, Right Places

- Offer quality, affordable, sustainable homes fit for the future.
- Build our fair share of housing with the right infrastructure to support and enable our borough to grow.
- Protect our unique places and preserve our natural environment.
- Help with your housing needs and support people to live independently in their own homes.

Keeping the Borough Moving

- Maintain and improve our roads, footpaths and cycleways.
- Tackle traffic congestion, minimise delays and disruptions.
- Enable safe and sustainable travel around the borough with good transport infrastructure.
- Promote healthy alternative travel options and support our partners to offer affordable, accessible public transport with good network links.

Changing the Way We Work for You

- Be relentlessly customer focussed.
- Work with our partners to provide efficient, effective, joined up services which are focussed around you.
- Communicate better with you, owning issues, updating on progress and responding appropriately as well as promoting what is happening in our Borough.
- Drive innovative digital ways of working that will connect our communities, businesses and customers to our services in a way that suits their needs.

MEMBERSHIP OF THE PLANNING COMMITTEE

Councillors

Simon Weeks (Chairman)	Chris Bowring (Vice-Chairman)	Stephen Conway
Gary Cowan	Carl Doran	Pauline Jorgensen
Abdul Loyes	Andrew Mickleburgh	Malcolm Richards
Angus Ross	Rachelle Shepherd-DuBey	

ITEM NO.	WARD	SUBJECT	PAGE NO.
50.		APOLOGIES To receive any apologies for absence.	
51.		MINUTES OF PREVIOUS MEETINGS To confirm the Minutes of the Meeting held on 9 December 2020, and the Minutes of the Extraordinary Meeting held on 16 December 2020.	5 - 28
52.		DECLARATION OF INTEREST To receive any declaration of interest	
53.		APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS To consider any recommendations to defer applications from the schedule and to note any applications that may have been withdrawn.	
54.	Emmbrook	PATH CREATION ORDER AT JUBILEE AVENUE Recommendation: That the order is made	29 - 106
55.	Twyford	APPLICATION NO.201022 - OLD SCHOOL HALL, HIGH STREET, TWYFORD Recommendation: Conditional approval	107 - 158
56.	Winnersh	APPLICATION NO.203439 - 36 AND 39-48 GROVELANDS PARK, WINNERSH, WOKINGHAM, RG41 5LD Recommendation: Conditional approval	159 - 192
57.	Charvil	APPLICATION NO.202106 - PADDICKS PATCH, WAINGLES ROAD, CHARVIL, RG10 0UA Recommendation: Conditional approval	193 - 216
58.	South Lake	APPLICATION NO.203344 - HIGHWOOD BUNGALOW, FAIRWATER DRIVE, WOODLEY, RG5 3JE Recommendation: Conditional approval	217 - 234

Any other items which the Chairman decides are urgent

A Supplementary Agenda will be issued by the Chief Executive if there are any other items to consider under this heading.

GLOSSARY OF TERMS

The following abbreviations were used in the above Index and in reports.

C/A	Conditional Approval (grant planning permission)
CIL	Community Infrastructure Levy
R	Refuse (planning permission)
LB	(application for) Listed Building Consent
S106	Section 106 legal agreement between Council and applicant in accordance with the Town and Country Planning Act 1990
F	(application for) Full Planning Permission
MU	Members' Update circulated at the meeting
RM	Reserved Matters not approved when Outline Permission previously granted
VAR	Variation of a condition/conditions attached to a previous approval
PS	Performance Statistic Code for the Planning Application
Category	

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**MINUTES OF A MEETING OF THE
PLANNING COMMITTEE
HELD ON 9 DECEMBER 2020 FROM 7.00 PM TO 10.59 PM**

Committee Members Present

Councillors: Simon Weeks (Chairman), Chris Bowring (Vice-Chairman), Stephen Conway, Gary Cowan, Pauline Jorgensen, Abdul Loyes, Andrew Mickleburgh, Malcolm Richards, Angus Ross and Rachelle Shepherd-DuBey

Councillors Present and Speaking

Councillors: John Kaiser and Stuart Munro

Officers Present

Catherine Brimble, Landscape Architect
Connor Corrigan, Service Manager - Planning and Delivery
Lyndsay Jennings, Legal Specialist
Judy Kelly, Highways Development Manager
Clare Lawrence, Assistant Director – Place Services
Chris Traill, Director - Place & Growth
Justin Turvey, Operational Manager – Development Management
Callum Wernham, Democratic & Electoral Services Specialist

Case Officers Present

Nick Chancellor
Mark Croucher
Andrew Fletcher
Senjuti Manna
Sophie Morris

33. APOLOGIES

An apology for absence was submitted from Carl Doran.

34. MINUTES OF PREVIOUS MEETING

The Minutes of the meeting of the Committee held on 14 October 2020 were confirmed as a correct record and would be signed by the Chairman at a later date.

35. DECLARATION OF INTEREST

Pauline Jorgensen declared an interest in item number 38, on the grounds that she had previously been the Executive Member for Housing. Pauline added that she had not formed a view regarding this item, and would keep an open mind during discussions. Pauline stated that she would take part in the vote.

Angus Ross declared an interest in item number 37, on the grounds that he was involved in the concept during his time as an Executive Member. In addition, Angus stated that he was a member of the Ramblers association. Angus added that he had not made any determination regarding this item, and would go into discussions with an open mind. Angus stated that he would take part in the vote.

Angus Ross declared an interest in item number 39, on the grounds that he had originally had some involvement with this site. Angus stated that he would go into discussions with

an open mind, and had taken no involvement in this planning application. Angus stated that he would take part in the vote.

Gary Cowan declared a prejudicial interest in item number 39, on the grounds that he had made comments in the past relating to this application. Gary added that he would take no part in this item.

Simon Weeks declared a prejudicial interest in item number 39, on the grounds that he had made comments in the past relating to this application. Simon added that he would take no part in this item, and the Vice Chairman would Chair this particular item.

Simon Weeks declared an interest in item number 38, on the grounds that he was the Ward Member for the area and had been involved with the residents on the estate. Simon added that he had not formed a view on this application, and he would go into discussions with an open mind. Simon added that he would take part in the vote.

Stephen Conway and Pauline Jorgensen stated that they had not taken part in some or all of the previously conducted site visits. Both Members added that they would listen to all discussions relating to these items, and would take part in the votes.

36. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

No applications were recommended for deferral, or withdrawn.

37. DIVERSION OF SHINFIELD FOOTPATH 12

Proposal: Application for the diversion of part of Shinfield Footpath 12 under Section 257 Town and Country Planning Act 1990

Applicant: Shinfield West Housebuilder Consortium

The Committee received and reviewed a report, set out in agenda pages 13 to 20.

The Committee were advised that there were no Members' Updates.

RESOLVED That the order be made.

38. APPLICATION NO.202133 - LAND EAST OF GORSE RIDE SOUTH, SOUTH OF WHITTLE CLOSE AND TO THE NORTH AND SOUTH OF BILLING AVENUE, FINCHAMPSTEAD, RG40 9JF

Proposal: Full planning application for the proposed redevelopment of the existing Gorse Ride South Estate, comprising demolition of existing buildings and replacement with 249 no. dwellings (mixed-tenure flats and houses) together with associated access, parking, landscaping, public open space and drainage

Applicant: Wokingham Borough Council c/o Agent: Pegasus Planning Group

The Committee received and reviewed a report about this application, set out in agenda pages 21 to 76.

The Committee were advised that the Members' Update included:

- Attached printed leaflet with summary information pertaining to the application;

- Clarification that the age of the existing development and its intended functional lifespan were not relevant planning considerations for the purpose of the current application;
- Clarification that the Local Planning Authority would also require a planning obligation in the form of a financial contribution towards local bus service contributions;
- Revision to Condition 16;
- Clarification regarding the affordable housing requirements;
- Substitute wording of Informative 6;
- Additional Informative 17;
- Correction to paragraph 21, to now refer to 88 households.

John Kaiser, Executive Member for Finance and Housing, spoke in favour of the application. John stated that this project would consist of 74 percent affordable housing, with 185 new homes for the community. John added that although Council budgets were under pressure, it was crucially important for this project to move forward. John stated that the current properties were in poor condition, and Wokingham Borough Council (WBC) were committed to supporting the local community via provision of high quality homes. John added that this project had been adapted to meet the local need of the area, and the houses would be climate friendly and carbon friendly. John urged the Committee to support the proposals.

Steve Bowers, Chair of the Gorse Ride Residents Steering Group, spoke in support of the application. Steve stated that he had lived on the estate for 36 years, and had raised 5 children, all of whom now owned their own houses. Steve added that he wanted this for future generations, and he wanted people to be proud to live on the estate. Steve stated that the next step for the estate now needed to be taken, and the residents steering group in conjunction with the Parish and Borough Councils, amongst other key parties, were committed to enabling the delivery of this project. Steve added that people needed to be given other opportunities for good quality affordable housing, and he could not emphasise enough just how important this project was for the local area.

Gary Cowan stated that he was supportive of the proposals, and felt that the properties would be delivered in the right area. Gary queried whether affordable housing schemes could apply to former and current military personnel. Simon Weeks stated that key worker lists could be updated to include a variety of groups.

Stephen Conway stated that he was supportive of the proposals. Stephen queried why more apartment blocks were now included within the proposals. Nick Chancellor, case officer, stated that the proposals would be of a higher density when compared to the existing dwellings. Nick added that 4 stories were deemed acceptable, as they were mitigated by public open space, and the sense of openness of the overall site. Nick added that the proposals were acceptable in appearance.

Rachelle Shepherd-DuBey queried whether there was bicycle storage in the blocks of flats, whether the properties would be Council owned or managed via a housing association, and why the current properties were not built to last. Nick Chancellor stated that cycle storage would be located within each apartment block. The site would be conveyed to Loddon Homes, which was wholly owned by WBC. Nick added that the site would offer a suitable mix of different types of housing including social rent, discounted rent and shared ownership. Simon Weeks clarified that the current properties were of a Swedish fabricated design, which had a shorter lifespan than more expensive designs.

Simon added that the proposals would be of significantly better quality, and therefore a significantly better lifespan.

Angus Ross queried why condition 24 only stated provision of 40 percent affordable housing, and how contributions towards SANG provision would be devised. Nick Chancellor stated that the 40 percent provision of affordable housing was just the policy requirement, however the proposals would far exceed this amount. Nick stated that the contributions towards SANG provision would only be sought from the uplift in dwellings compared to the current site.

Abdul Loyes stated that he was delighted to see the proposals progressing, and queried whether the roads would be adopted. Judy Kelly, Highways Development Manager, confirmed that the roads would be adopted.

Malcolm Richards queried whether lifts would be present in the apartment blocks, queried whether some parking would be provided in clusters, queried whether there would be separate designated areas for electric charging, queried whether cladding would be present on the apartment blocks, and queried whether lighting would be present on all pathways across the site. Nick Chancellor stated that he thought lifts were not proposed within the apartment blocks. Nick stated that car parking would be well distributed across the site, and the site was designed so that allocated parking was close by to dwellings. Some electric charging points would be located on driveway plots, and some within unallocated spaces. The detail of placement of electric vehicle charging point was secured by condition and would come later. The exact materials to be used on the apartment blocks was secured by condition, and would be provided later, however the materials were likely to be a mix of brick, tile brick, and metal balconies. Nick stated that the intention was to illuminate all pathways, and this would form part of landscaping conditions.

Andrew Mickleburgh sought assurance that the garages were suitable for a modern vehicle, and the ban on business and residential usage of the garages would remain permanently. Andrew queried how the energy SAP rating compared to the Gold Standard. Nick Chancellor stated that the garages would be suitable for a modern size car, and the condition regarding acceptable use of the garages would remain in perpetuity unless appealed. Nick added that he could not compare the proposals to the gold Standard rating at this stage, however the properties would be very energy efficient, including features such as district heating of apartment blocks, and home air source heat pumps.

Rachelle Shepherd-DuBey queried whether permitted development would be allowed on the proposed dwellings, and queried whether photovoltaic panels would be present on the proposed dwellings. Nick Chancellor confirmed that permitted development would be removed by way of condition. Nick added that he could not confirm whether photovoltaic panels would be included, however it was possible that some would be included.

Committee Members were unanimous in their support for high quality affordable housing projects, such as this application.

RESOLVED That application number 202133 be approved, subject to conditions and informatives as set out in agenda pages 22 to 36, revision to condition 16, rewording of informative 6, and addition of informative 17 as set out in the Members' Update.

39. APPLICATION NO.163547 - HOGWOOD PARK, PARK LANE, BARKHAM, WOKINGHAM RG404PT

Simon Weeks and Gary Cowan withdrew from this item and took no part in discussions nor the vote.

Simon Weeks withdrew from the Chair, and Chris Bowring assumed the Chair.

Proposal: HYBRID APPLICATION: Outline application (all matters reserved except access to the site) for up to 140 residential units (Use Class C3) and all associated parking, soft and hard landscaping within the site and ancillary works. (Means of access into the Site off Park Lane, demolition of existing buildings and 2.83ha of SANG to be determined in full detail)

Applicant: Reading Football Club

The Committee received and reviewed a report about this application, set out in agenda pages 77 to 142.

The Committee were advised that the Members' update included:

- Replacement wording of Condition 26 ;
- Additional Condition 42.

Graeme Dexter, Barkham Parish Council, spoke in objection to the application. Graeme stated that the proposals did not fit well with the Arborfield and Barkham neighbourhood plan, nor did it fit with the nearby Hogwood industrial estate. Graeme stated that the extraction fans on the industrial estate were not suitable to be located in close proximity to a residential estate. Graeme added that the site was not located within the existing SDL, and was not in close proximity to local shops. Graeme stated that there would be no direct road access to the district centre, which would force more traffic onto Park Lane, as there was no link to the Nine Mile Ride road extension. Graeme added that it was not possible to safely walk to California Country Park, which would force yet more traffic onto Park Lane. Graeme felt that green areas and landscaping should be used as a buffer between the proposed homes and the industrial estate. Graeme stated that the site required a dedicated link to the nearby SDL, and more SDL infrastructure was required prior to development.

Nina Sharp, agent, spoke in support of the application. Nina stated that this application was in hybrid form, and included up to 140 dwellings and a SANG. Nina added that the site would be vacant from December 2020, and the proposals would provide a safe cycle route to Bohunt School, shops, and community facilities. Nina stated that 5 hectares of public open space would be provided, including a SANG extension. Surface upgrades to the SANG would be secured by a S106 legal agreement, and the site would provide 40 affordable homes. Nina stated that all properties would be of two storey design, with an average density of 27 properties per hectare. Nina concluded by saying that the site was included within the emerging draft local plan, and there had been no objections from statutory consultees regarding this application.

John Kaiser, Ward Member, spoke in objection to the application. John stated that this was not a sustainable development, and by allowing this development it would allow developments to be built anywhere via precedent. John was of the opinion that there was no highways justification for this application, and this application would set a very dangerous precedent. John stated that this was a case of SDL creep, as the site was 'tacked' on to the edge of an SDL, which was not sustainable. John stated that the

proposals were not in line with the overarching SDL vision, and the access to the site would be via a dangerous and overstretched country road, with no safe crossing point. John was of the opinion that over 1000 jobs and livelihoods could be lost at the Hogwood industrial estate should they be forced to close due to environmental concerns from the proposed residential development. John stated that the site would force residents to drive to shops and amenities, which did not promote the Council's green agenda. John urged the Committee to refuse this application until concerns had been adequately addressed.

Chris Bowring queried the relationship in planning terms between the proposed dwellings and the industrial estate. Sophie Morris, case officer, stated that the nearest property to the industrial estate was located 30m from the industrial estate, and no environmental concerns had been raised during the application stage. Sophie added that the houses in the southern portion of the SDL were located adjacent to the industrial estate. Sophie stated that there would be levels of background noise, however there were no objections based on these grounds.

Rachelle Shepherd-DuBey queried what measures would be taken to meet climate emergency objectives, queried whether the roads would be adopted, queried whether the SUDs would be adopted, and queried why only one main entrance and exit was proposed. Sophie Morris stated that a sustainability report would be submitted, with a number of measures intended to be taken forward, at the reserved matters stage. Some features of the development would include photovoltaic panels and solar water heat pumps. Judy Kelly, Highways Development Manager, stated that at the outline stage it was too premature to know if the applicant intended to offer the roads or SUDs for adoption, however the S106 agreement would either require adoption or for the roads and SUDs to remain private via a maintenance company with conditions attached. Judy stated that there would be two points of access, one main and one secondary, and the peak projected vehicle movements from the development would equate to 74 vehicle movements per hour on to Park Lane.

Abdul Loyes queried whether the SANG was inside or outside of the red line boundary, queried whether Natural England had objected to an access point from Park Lane, and whether the Education service had commented regarding the impact of the development on school places. Sophie Morris confirmed that the SANG on the application site was located within the red line boundary, with linkages to the existing northern SANG. Detailed discussions had not taken place with Natural England, however a pedestrian / cycle path along Park Lane would result in the loss of some SANG, and it would be very likely that Natural England would not support such a proposal. Sophie added that Bohunt School had not responded to the proposals to date however Education have not raised any issues with the impact on schools.

Angus Ross queried the purpose of the alternate recommendation C, queried if the application was premature based on Wokingham's land supply, and stated that noises and odours were present during the Members site visit. Connor Corrigan, Service Manager - Planning and Delivery, stated that the Committee were being asked to approve all recommendations in from them, with recommendation C allowing delegated refusal in the absence of the agreed S106 contributions. Connor stated that the NPPF was clear that an application could not be refused if it was deemed sustainable, and the assessment considered that the development would be sustainable given its location next to the SDL District Centre. He confirmed it would have been preferable if the site was originally planned and included as part of the wider SDL, however the planned infrastructure to be delivered as part of the wider SDL would make this site sustainable. Sophie Morris stated

that an odour assessment had not been submitted, as this was usually submitted for applications nearby facilities such as a sewage treatment plants. Sophie stated that a condition could be added requiring completion of a satisfactory odour assessment.

Angus Ross suggested that a condition requiring a noise and odour assessment should be added if Members were minded to approve the application.

Malcolm Richards queried whether a smaller width pavement than standard could be installed, as without the option of walking to local shops and amenities, residents would be forced to drive. Judy Kelly stated that although some sections of Park Lane could fit a standard sized pavement, other sections would force the pavement to encroach on green space, and it was therefore not possible to provide a pavement. Sophie Morris added that internal footways within the site would provide a safe and attractive link to Bohunt School.

Pauline Jorgensen queried the density of dwellings per hectare, queried whether green space could potentially be reduced by the applicant should parking not meet standards, sought more details regarding the informal school from off point, and queried what would happen to the bus service provision once the S106 money ran out. Sophie Morris confirmed that the density on site would equate to 27 units per hectare, inclusive of the SANG area. Connor Corrigan confirmed that the SANG could not be reduced in order provide additional car parking. In addition, other green space would be sought to be maintained in order to meet WBC guidelines. Regarding the school drop off point, Connor stated that this would be an informal drop off point, and would allow students to be dropped off safely as currently this occurred within the industrial park, which was not ideal. Judy Kelly stated that S106 monies would be added to top up the Arborfield SDL bus strategy. More money was needed at the beginning of new and improved bus routes whilst passenger numbers were comparatively low due to full SDL housing numbers being in the delivery stage. As additional dwellings were constructed and occupied, bus services would become more profitable and self-sufficient.

Andrew Mickleburgh sought confirmation that it was not a planning consideration to consider what would happen to this site should it not be developed, and queried whether point 12 within the agenda report was contradictory as it stated the proposals were not considered as "so substantial" when they would contribute to a 9.3% increase to the southern SDL area. Connor Corrigan stated that WBC had looked at proposals to take on the site in its current form, however it was not a planning consideration to consider what the site would be should development not occur, and the Committee had to consider the application in front of them. Connor stated that officers had considered the proposals, including capacity of the existing and proposed local amenities, and had deemed the proposals to be sustainable and met the planning tests.

Chris Bowring sought clarification that if the proposals did not cause harm, then their position outside of the SDL was not a planning issue. Connor Corrigan stated that the NPPF was very clear that if a development was sustainable and caused no adverse harm, then they should be approved. Connor added that the planning tests had been looked at, and officers considered them to have passed, with the proposals not causing significant adverse harm.

Stephen Conway was concerned that this application could be considered as premature, and queried whether the development would be sustainable without the infrastructure being present. Connor Corrigan stated that prematurity could not be used as an agreement on sustainable sites and that the same argument could apply to the rest of the

development in the SDL. However the approach is that supporting facilities and services generally come later on which also helps ensure their viability. Sophie Morris stated that reserved matters were yet to come, and they would provide a lot of detail. Sophie added that should this application be approved, reserved matters would be unlikely to return for at least six to twelve months. As such, units may not be developed until 2023, by which time facilities in the wider SDL should be developed.

Angus Ross proposed that standard conditions relating to noise and smell be added, and the results thereof be agreed in conjunction between the Chairman, Vice-Chairman and the proposer. This was seconded by Chris Bowring, carried, and subsequently added to the list of conditions.

RESOLVED That application number 163547 be approved, subject to conditions and informatives as set out in agenda pages 79 to 96, replacement wording of condition 26 and additional condition 42 as set out in the Members' Update, and additional standard conditions related to noise and smell as resolved by the Committee.

40. APPLICATION NO.200711 - BARTLETTS FARM, SWALLOWFIELD ROAD, ARBORFIELD

Simon Weeks resumed the Chair.

Proposal: Full planning application for Installation of a Solar park to include 40000 solar photovoltaic panels, 11 inverter/transformer cabins, a single control building and associated works to include vehicle access and fencing with Environmental Statement.

Applicant: Wessex Solar Energy

The Committee considered a report about this application, set out in agenda pages 143 to 174.

The Committee were advised that the Members' Update included a typographical correction to paragraph 24.

Jonathan Wheelwright, Swallowfield Parish Council, spoke in objection to the application. Jonathan stated that Swallowfield Parish Council were in support of climate emergency objectives, however this was the wrong location for a solar farm. Jonathan was of the opinion that this would be more likened to a solar industrial site, built on top of good quality 3A graded agricultural land which was not low quality land. Jonathan stated that it would be hard to find a more visible or intrusive location. Jonathan added that this application had received 89 resident objections, all of who supported the green energy initiative, however not in such an unsuitable location. The site would cover around 46 acres of Greenfield, including installation of approximately 40,000 panels each 8 feet in height. Additional security fencing and outbuildings would also be developed on the Greenfield. Jonathan stated that the fields would be lost to foraging animals, and the alternative site assessment had shown other more suitable sites. Jonathan concluded by stating that sustainable energy was important, however sites should not be placed on good quality farmland, situated in highly visible locations.

Harry Cannon, resident, spoke in objection to the application. Harry stated that he was speaking on behalf of a number of residents, and their view was that this was a good idea, but it was situated in the wrong place. Harry added that this would be a large industrial development located within local countryside on rising ground. As such, the proposals

would be visible for miles around, including from farmland and paths which were used enormously by locals. Harry stated that the proposed development would be located next to listed buildings, and would breach a variety of planning policies and guidelines. Harry stated that one report categorised the views from paths as negligible or minor, which was not the case at all. Harry added that the development would be seen from as far away from Earley, Arborfield and Eversley. Harry concluded by stating that the assessment for alternative sites was incredibly weak, and these proposals would in fact damage the environment by placing an industrial site within the countryside which was valued by residents.

Richard Wearmouth, applicant, spoke in support of the application. Richard stated that this application was for the development of a solar park, and the proposed park was entirely suitable for a site of this scale, in part due to the 33kv powerline running underneath the site. Richard added that the WBC landscape officer had deemed the screening proposals to be appropriate, and the site would see a net ecological gain in the form of hedgerows, trees, and wildflowers. Richard stated that gates would be present to allow for animals such as hedgehogs and badgers to pass through the site safely. Richard added that the proposals would improve drainage in the area, and the flood risk assessment had shown that the proposals would not pose any additional risk when it came to flooding. Natural England had concluded that there was only a limited use for the land, Richard added. Richard stated that the power lines would be located underneath the site, which was desirable for a site of this nature. Richard concluded by stating that this site would not be used indefinitely in the proposed way, and the applicant was not placing an application for an indefinite designation, and the site would be safely decommissioned at the end of the site's lifespan.

Stuart Munro, Ward Member, spoke in objection to the application. Stuart stated that this was a rare occasion whereby he had received quite so many comments in opposition to an application. Stuart added that there was clear support for solar panels and solar farms within the Borough, however the question needed to be answered, could a site be positioned in a more suitable location. Stuart urged the Committee to consider residents' concerns and submissions prior to making a decision.

Simon Weeks sought clarification regarding a number of points raised by speakers, namely whether the existing hedgerow was planned to be reinforced, whether the proposals could be viewed from miles around, and what animals would be present on the site. Mark Croucher, case officer, stated that there were plans to reinforce the existing hedgerows, and this would follow in detail should the application be approved. Mark stated that the western parcel of the site was flat and would be very well screened, and where the ground did rise to the east more distant views would be present, however the visual envelope would still be fairly well contained. Catherine Brimble, Landscape Architect, stated that most views would be contained very locally, with gaps mainly present, through vegetation, footpaths, and gates. Longer distance views could possibly be seen from the north, and middle distance views would be really restricted to existing vegetation. Regarding the animals present on the site, Mark Croucher stated that this would be primarily deer and sheep.

Gary Cowan was of the opinion that there was next to nothing presented which gave support to this application. Gary stated that it was very hard to make a judgement regarding the stated low grade land until a high resolution map from Natural England was provided. Gary added that there were no pictures showing the dimensions of the site as it stood. Gary stated that the site elevated considerably, which could lead to views from a

long distance. The highest point of the site was approximately 65m to 70m, with the lowest point being 55m, which showed a considerable 10m to 15m rise across the site. Gary added that the vast majority of the land was good quality 3A as stated by Natural England, which should not be used for proposals such as this. In addition to the plethora of solar panels on site, Gary added that the proposals included hundreds of feet of buildings, some being up to 10 feet in height. Gary stated that planning policies stated that developments in the countryside should seek to protect and enhance the natural landscape, and significant development should only be delivered in areas of poor quality land. Gary was of the opinion that 3A land should be protected, and the proposed 8 foot fence would urbanise a rural location.

It was at this point of the meeting that Simon Weeks proposed the meeting be extended by a maximum of 30 minutes to 11pm. This was seconded by Rachelle Shepherd-DuBey and carried.

Stephen Conway recognised that this was a difficult recommendation to make for the case officer, as there were a variety of policies going in different directions. Stephen stated that there were a variety of national and local planning policies which suggested that there were problems with this site, including landscape harm and the quality of the agricultural land that would be used. Stephen stated that there appeared to be a lack of information regarding potential alternative sites, and why they were deemed to not be suitable. Stephen suggested that a deferral may be appropriate, in order to receive information regarding alternative sites and how they had been considered. Mark Croucher stated that one of the key benefits of this particular site was the 33kv power line running underneath the ground. Mark added that the climate emergency action plan would require seven solar farm sites of this site in order to achieve its objectives. With that in mind, Mark stated that some impacts were inevitable and other sites had other problems, and therefore there would always be an impact regardless of the location. Mark stated that the Natural England map was broad, and the soil tests were accurate at showing 58 percent of the land was of 3A grade, and 42 percent being 3B or 4 grade, being moderate or poor quality land. Mark added that there were two grades of land which were classified as better land for agricultural purpose than 3A grade.

Rachelle Shepherd-DuBey was of the opinion that the proposals were a good idea in a reasonable location, and would help Wokingham Borough Council (WBC) to achieve its climate emergency agenda.

Abdul Loyes queried how visible the site would be from the highway, and queried whether flooding was a major concern. Mark Croucher stated that cars may see a small view from the bypass. Mark Croucher stated that that the only change to the site's surface would be the stands for the panels to be mounted on. In addition, extra SUDs would be provided.

Malcolm Richards stated that views would not be present from most nearby roads, and the Borough needed a variety of facilities of this scale. Malcolm was of the opinion that stating that the site should be placed somewhere else was not a valid reason for refusal.

Angus Ross was of the opinion that the proposed location was a totally inappropriate site, and grade 3A land should not be used for a solar farm, and therefore the application should be refused.

Chris Bowring stated that he would support a deferral of this application, as the Committee needed to know what the criteria were for site selection, such as profitability, proximity to

the electrical grid, the sightlines, and the weighting applied to all of these considerations. Chris queried whether the Executive's climate emergency declaration had any change on current planning policies. Justin Turvey, Operational Manager – Development Management, stated that the climate emergency declaration did not change planning policy. Justin commented that this site could potentially generate enough energy to accommodate for 5 percent of the Borough's energy needs.

Pauline Jorgensen stated that she was not convinced that the site could potentially be viewed from miles around based on her experience at the site visit. Pauline queried whether the site would be protected in future from a developer stating that the site had been used in an industrial nature, and therefore wanting to place either an industrial site or housing on the land. Mark Croucher confirmed that the change of use would only be granted for 40 years, after which the use would revert to its current form.

Andrew Mickleburgh queried whether the site had no formal designation, and queried what criteria was used to select sites for renewable energy generation, as these were not allocated within the Local Plan. Mark Croucher stated that the site was deemed as essentially countryside land, rather than a formal designation. Mark stated that Local Plan policy CCO5 set out what would be required in terms of renewable energy. Any of the normal material considerations would be taken into account when looking at an application of this type.

Angus Ross proposed that the application be reduced, on the grounds that the land being used was of high quality agricultural land. This proposal was not seconded, and therefore fell.

Stephen Conway suggested that the application be deferred, to allow for further information to be gathered regarding the assessment and consideration of alternative sites, and why those sites were rejected in favour of the proposed site. Gary Cowan added that the returning information should include better maps and plans that included the profile of the site, rather than just the planned view.

The above suggestion was proposed by Angus Ross, and seconded by Chris Bowring.

RESOLVED That application number 200711 be deferred, to allow for further information to be gathered regarding the assessment and consideration of alternative sites, and why those sites were rejected in favour of the proposed site, and in addition include better maps and plans that included the profile of the site, rather than just the planned view.

41. APPLICATION NO.202103 - LAND AT MAIDENHEAD ROAD, WOKINGHAM, RG40 5RN

Proposal: Full planning application for the change of use of land from agriculture to the keeping of horses, plus erection of a hay barn and stable building.

Applicant: Mr L Proctor

The Committee received and reviewed a report about this application, set out in agenda pages 175 to 194.

The Committee were advised that the Members' Update included a slight variation of wording to Condition 5.

Simon Weeks commented that the application had received no objection, and had received the support of the case officer.

RESOLVED That application number 202103 be approved, subject to conditions and informatives as set out in agenda pages 176 to 178, and variation to condition 5 as set out in the Members' Update.

**MINUTES OF AN EXTRAORDINARY VIRTUAL MEETING OF THE
PLANNING COMMITTEE
HELD ON 16 DECEMBER 2020 FROM 7.00 PM TO 10.40 PM**

Committee Members Present

Councillors: Simon Weeks (Chairman), Chris Bowring (Vice-Chairman), Stephen Conway, Gary Cowan, Carl Doran, Pauline Jorgensen, Abdul Loyes, Andrew Mickleburgh, Malcolm Richards, Angus Ross and Rachelle Shepherd-DuBey

Councillors Present and Speaking

Councillors: Stuart Munro and Imogen Shepherd-DuBey

Officers Present

Madeleine Shopland
Connor Corrigan
Judy Kelly
Mary Severin
Justin Turvey
Catherine Brimble

Case Officers Present

Nick Chancellor
Mark Croucher
Jeanette Davey
Graham Vaughan

42. APOLOGIES

There were no apologies for absence.

43. DECLARATION OF INTEREST

There were no declarations of interest.

44. APPLICATIONS TO BE DEFERRED AND WITHDRAWN ITEMS

There were no applications to be deferred or withdrawn.

45. APPLICATION NO.201515 - ASHRIDGE FARM, WARREN HOUSE ROAD, WOKINGHAM, RG40 5QB

Proposal: Full application for the erection of 153 dwellings comprising a mix of 1,2,3 & 4 beds with associated landscaping, parking, open space, drainage; construction of a new access onto Warren House Road and Bell Foundry Lane; provision of an area of Suitable Alternative Natural Greenspace (SANG) and all other associated development works (including demolition of existing buildings and provision of temporary site sales).

Applicant: Barratt David Wilson Homes

The Committee received and reviewed a report about this application as set out in agenda pages 5 to 74.

The Committee was advised that the Members' Update included the following:

- Amendment of the cited Heads of Terms on Pages 8 and 25;
- Amendment of condition 2, to include the list of drawings to be approved;

- Amendment of condition 25 regarding stream alignment;
- Amendment of condition 37 regarding odour mitigation measures;
- Additional conditions 45 and 46;
- Clarification regarding parking space number and parking ratio;
- Additional consultation responses;
- Clarification regarding the installation of acoustic screens around the Odour Control Unit in order to address the noise issues associated with this, in order for it to commence use again.

Abby Tebboth, Wokingham Town Council, spoke in objection to the application. She stated that the Town Council was concerned about the odour levels coming from the sewage works. The odour contouring had been noted but the Odour Impact Assessment still predicted that odour exposures across the site would potentially be two or three times the level at which odour became a nuisance, which would have a significant impact on residents' use of their homes, gardens and the amenity space. Abby Tebboth went on to state that the proximity of the housing to Ashridge Stream presented a considerable flood risk even if the diversion works were completed. An area of the development was in the high level of flood risk, which was not in line with the Core Strategy. She added that there was an increased risk of flooding to the existing houses in the area. Abby Tebboth indicated that residents had concerns around the safety of the access to Warren House Road, to the playpark and the SANG, to existing houses. Increased pressure on local infrastructure was also potentially an issue. Abby Tebboth went on to state the application would develop some of the only remaining green space in Norreys.

Jessica Sparkes, spoke in favour of the application, on behalf of the Applicant. She commented that the proposals had been shaped following extensive discussions with officers and consultees. The number of dwellings now aligned with the emerging Local Plan, landscaping and open space were now increased and overall density on a par with other developments in the Strategic Development Location. Jessica Sparkes indicated that the application would allow for the completion of the Northern Distributor Road and that additional SANG and public open space would also be provided, which would complement the existing SANG across the north of Wokingham. The proposals also offered policy compliant affordable housing all of which would be provided within the development. Jessica Sparkes commented that the proposals gave regard to the water treatment works to the north and Listed Building to the south. She highlighted that Thames Water had not raised an objection to the proposals, had confirmed that the layout accorded with the recent odour report and that no further works were required from their perspective. In addition, Historic England had not submitted an objection and the applicant had sought to enhance the views that would be possible from the new public open space to the listed building by providing wide open space at the centre of the site for the veteran trees, a planted orchard and by retaining a granary building which would enhance the character of the open space at the centre of the site.

Malcolm Richards commented that the application would enable the completion of the Northern Distributor Road, which was positive.

Malcolm Richards questioned whether there was a Construction Management Plan. Judy Kelly confirmed that there would.

Malcolm Richards commented that there were three entry points to the development, but the plans suggested that the development was in two separate halves. He asked whether the roads allowed travel from one side of the development to the other. He went on to ask

about the internal road width and if they would be sufficiently wide for the predicted volume of traffic. In addition, Malcolm Richards asked if there would be pavements or shared spaces throughout the development. Connor Corrigan confirmed that there were two halves to the development with a linkage road through the middle. Judy Kelly added that the linkage road was more an emergency link and that there would be bollards in place and would be pedestrian and cycle only. The site accesses had been subject to road safety audits and no particular issues had been identified. With regards to road widths, Judy Kelly indicated that they would vary, but would generally be between 5-6m. A swept path exercise had been successfully carried out using a Council refuse vehicle. Judy Kelly added that there would be some shared spaces, such as at the end of cul-de-sacs, otherwise footpaths would be on one or both sides of the roads.

Malcolm Richards queried what percentage of the development was at high risk of flooding. Connor Corrigan stated that land to the south of the NDR was in Zones 1 and 2 and the realignment of the stream would create betterment and would put the development in Zone 1, which was acceptable. North of the NDR would flood but was open space, which was again considered acceptable. He informed Members that the Environment Agency had a standing objection to the timing of the development. They had wanted the flood modelling to be undertaken after the alignment of the stream. However, following discussions, the Environment Agency was satisfied with revised condition 25.

Malcolm Richards asked about the odour control system and potential noise and odour resulting from it. Connor Corrigan commented that Officers had relied on Thames Water and Environmental Health to look at the odour report. A detailed Odour Modelling Assessment had been produced by an independent consultant appointed by Thames Water. The odour control system had reduced the amount of odour on site. Noise mitigation and screening would be put in to mitigate against any noise issues.

In response to a question from Malcolm Richards, Judy Kelly stated that there would be 238 allocated parking spaces and 60 unallocated or visitor spaces.

Simon Weeks sought clarification on the safety of the access points. Judy Kelly confirmed that they had been subject to a Stage 1 Road Safety Audit.

Stephen Conway noted that some of the public open space was within Zone 3 of the odour contour map, and questioned whether there was a risk of the public open space becoming unusable on some occasions. He also queried potential odour nuisance beyond Zone 3. Simon Weeks commented that approximately 75% of the time the prevailing wind was from the south west to the north east. Connor Corrigan added that the odour contour mapping was based on the level of complaints received. The number of dwellings proposed had responded to the odour contour mapping zone. There was enough open space to ensure that people did not have stand in an area on occasions when it was particularly odorous. Stephen Conway added that sewage treatment works were usually situated away from housing developments. As development in the area increased so would the pressure on the sewage treatment system.

Rachelle Shepherd-Dubey questioned whether prospective residents would be informed of the sewage treatment works prior to purchase. Connor Corrigan commented that anyone visiting the development would be able to see the site.

Rachelle Shepherd-Dubey sought clarification as to how bad flooding would potentially be and how flooding control on the site would work. Connor Corrigan indicated that the site

was designed with SUDS in place. It was for the applicant to decide how they were managed but they could decide to hand it over to the Council to manage should they wish. The drainage system would be improved by the betterment of the stream.

Rachelle Shepherd-DuBey asked whether there would be crossings on the NDR to enable crossing to Cantley. Connor Corrigan stated that there were two crossings and that Cantley was a key destination. A Walking and Cycling Strategy condition sought a new pedestrian crossing to Cantley, which would also benefit the residents of the Kentwood Development. There would be cycleways and footpaths and Cantley would have a new cycleway. A S106 contribution was in place to improve pedestrian and cycleway connectivity. Rachelle Shepherd-Dubey queried if the cycleway would be shared or separate and was informed that it would be shared.

Carl Doran welcomed the provision of 35% affordable housing on site. He went on to question the tenure split (50% social rent, 20% affordable rent and 30% shared ownership) and if it was strictly policy compliant. Connor Corrigan stated that the tenure split had been worked up with the Affordable Housing team based on local need. There was a good mixture of provision across North Wokingham. Carl Doran felt that there should be more 1 and 2 bedroom homes.

With regards to odour, Carl Doran queried whether there had been changes to the sewage treatment centre over the last ten years. He pointed out that the North Wokingham SDL Supplementary Planning Document said that the sewage treatment works may exclude residential development in the Ashridge Farm area, due to odours, and that rigorous testing would be required. He questioned whether this testing had been carried out. Connor Corrigan indicated that the sewage works had had a major upgrade. The SDL originally did not include the site as it was seen as a constraint. However, when sites were allocated, it was done on a high-level basis. Industry standard testing had been carried out by an independent contractor with regards to odour.

Carl Doran continued to raise concerns regarding potential odour. He commented that the highest exposure levels were in the south of the site where the preliminary and primary effluent treatment took place. The odour control unit did not cover this area. Connor Corrigan responded that Environmental Health and Thames Water had not objected to the application. Environmental Health had received two complaints regarding odour in 2015 and one in 2019.

Andrew Mickleburgh asked what the minimum road width was in the development and if it was adequate to ensure that there was no parking on verges and pavements. Judy Kelly stated the minimum road width was generally 5m although there was a very small section of private road that would be 4.5m. She did not envisage issues with parking.

Andrew Mickleburgh went on to ask about flooding in the SANG and how often it was anticipated that the SANG would flood and require closure. Connor Corrigan confirmed that there would be a degree of flooding. The waster course fed into the Emm Brook. However, there was sufficient space to ensure that the SANG would not become unusable.

Andrew Mickleburgh referred to air quality and air pollution. He expressed concern regarding the cumulative impact of increased vehicle movements along Warren House Road and Wiltshire Road. Connor Corrigan indicated that the NDR would actually take traffic off the current route.

Pauline Jorgensen asked whether the estate to the left by the lagoon had experienced issues with odour. Connor Corrigan clarified that no complaints had been received from the new developments.

Pauline Jorgensen queried whether the roads would be built to adoptable standards. Judy Kelly stated that they would. An obligation was being secured in the Heads of Terms for the Section 106, for the developer to either complete a Section 38 Highways Adoption Agreement, or enter into a separate private roads Section 106 Agreement prior to commencement of development.

Pauline Jorgensen commented that the standards for cycleways were changing and that there was now a presumption in favour of separate cycleways. She questioned if there could be a condition that where appropriate, cycleways be separate. Judy Kelly suggested that condition 29 be refined to reflect this.

Pauline Jorgensen went on to ask if there would be hedgerow replacement. She was informed that there would be reprovision of the hedgerows to link in the SANGs.

Simon Weeks commented that to the north of the NDR was the proposed SANG, linking to SANGs in the east and west. Beyond that between the SANG and the proposed route of the NDR was additional recreational ground. He sought clarification as to why this recreation ground had not been included. Connor Corrigan reminded the Committee that only a certain amount of SANG was required to meet policy requirements. Any greater would incur greater maintenance costs. Pauline Jorgensen questioned whether it would be protected from future development, and was informed that it would be.

Chris Bowring commented that the odour report was a technical document and should be taken into account as such.

RESOLVED: That application 201515 be approved subject to the completion of a S106 agreement inclusive of the following Heads of Terms set out on page 8 of the agenda, conditions and informatives as set out in pages 8 to 25 of the agenda, amended condition 29, and the additional conditions and amended conditions as detailed within the Members' Update.

46. APPLICATION NO. 201573 - BOUNDOAK INDUSTRIAL ESTATE, WOKINGHAM LANE, ARBORFIELD, RG2 9PN

Proposal: Full application for the erection of 4No buildings for B1c,B2 and B8 industrial and commercial use with the addition of a sui generis Energy Centre comprising a Combined Heat and Power facility and a Lithium Ion Battery store in Unit 1 to include landscaping and access works, following demolition of the existing buildings.

Applicant: Musketeer Properties Ltd

The Committee received and reviewed a report about this application as set out in agenda pages 75 to 120.

The Committee was advised that the Members' Update included the following:

- Amendment to condition 4 in order to allow up to two units to be occupied by the same tenant;

- Amendment to condition 16 around walking and cycling.

Douglas Bond, agent, spoke in support of the application. He stated that the site was currently an unattractive, poor quality estate which needed development. The application would result in an improvement for tenants and employees. Amended plans had been submitted to allay the concerns of residents. The proposed layout was sensitive to surroundings and allowed important trees to be retained, in addition to opportunities for new native planting. The Countryside Officer had concluded that the application would result in a minor beneficial effect to the wildlife. Douglas Bond also commented that there would be sufficient onsite parking. He went on to refer to improved sustainability features. He emphasised that the proposals also included provision for an energy centre, which enhanced the scheme's sustainability credentials.

In response to a question from Gary Cowan, Judy Kelly explained that there was a pelican crossing for pedestrians on the A327M, the main strategic road to the east of the site. There was also a bridleway (Pegasus crossing). There was bridleway that came down to the north and to the west of the site there was a byway that continued on. Residents had expressed concern that the uncontrolled pedestrian crossing was not well constructed. A condition sought to improve this and a dropped kerb crossing with tactile paving would be put in place, with a continuous footway leading into the site.

Andrew Mickleburgh queried the number of parking spaces, including for trucks. Judy Kelly indicated that the site usage would be a mixture of B1, C, B2 and B8 which had different parking standards. However, parking standards would be met. There were no specific parking places for trucks on site.

With regards to control of noise, Angus Ross questioned whether conditions were strong enough to control any future occupants and usages. Jeanette Davey indicated that usage should not exceed prevailing background noise at the nearest residential property or noise sensitive property.

Malcolm Richards queried the machinery operating times beginning at 7am. Jeanette Davey commented that the applicant had originally requested longer working hours. Condition 21 covered the adjoining residents.

Malcolm Richards stated that the lithium-ion battery store would be vulnerable to fire risk. Jeanette Davey emphasised that Royal Berkshire Fire and Rescue Service had indicated that the relevant fire legislation would need to be complied with in order for a Fire Safety Order to be achieved.

Simon Weeks commented that there was currently encroachment on the wildlife as a result of the tipping of waste. Douglas Bond confirmed that this would be addressed.

RESOLVED: That application 201573 be approved subject to completion of a legal agreement and conditions and informatives as set out in pages 76 to 89, and revised conditions 4 and 16 as detailed in the Members' Update.

At this point in the meeting, 9.45pm, Gary Cowan left the meeting.

47. APPLICATION NO.200700 - 1, BARKHAM ROAD, WOKINGHAM, RG41 2XR

Proposal: Full application for the proposed erection of 14 flats, 6 no. one bedroom flats, 8 no. two bedroom flats, car parking and landscaping, following the demolition of existing buildings.

Applicant: Cleanslate Ltd

The Committee received and reviewed a report about this application as set out in agenda pages 121 to 156.

The Committee was advised that the Members' Update included the following:

- Additional condition around hours of work.

A statement was read out on behalf of Brian Norton, applicant, in support of the application. The applicant had held a Pre-Application process and met with Planning Officers and Landscaping Officers. They had had a constructive dialogue which had resulted in a number of modifications to the proposals; namely reducing its scale, improving external design features to meet the local vernacular and bolstering landscaping given its location on a Green Route. In addition, plants that bolstered biodiversity, had been selected. Although the Heritage Officer had, early in the process, expressed concern around the existing building's history, very little of the original features remained and redevelopment was the only practical option. There were some significant viability challenges due to technical requirements for remediation and poor ground conditions leading to a potential need for piling. Despite these challenges several shared ownership flats were offered as part of the development.

Imogen Shepherd-DuBey, Ward Member, spoke in objection to the application. She commented that on the whole she welcomed the application. However, she felt that the inclusion of a lift would make the building more accessible for all. Imogen Shepherd-DuBey questioned why solar panels were not part of the application. In addition, she felt that more visitor parking was required and that at least one more flat should be affordable housing.

Pauline Jorgensen sought clarification regarding the private open space in front of the flats and queried whether the landscaping was adequate. Janette Davey indicated that originally it had been proposed that the space in front of the flats would be communal open space. It was now private space for five of the flats. There was a proposal for a brick wall and planting against the brick wall which would offer a degree of privacy. Officers were keen that the proposals complied with the government's provisions for building a healthy life, seeking to achieve outdoor space in some form for as many residents as possible. This had been achieved in eleven of the fourteen dwellings. With regards to the private garden space, Officers had felt it appropriate in the location.

Malcolm Richards asked whether there was sufficient space for refuse vehicles to access. Judy Kelly indicated that larger vehicles would reverse in and then come back out on to the road in a forward manner.

Malcolm Richards commented that a lift would be useful. Janette Davey clarified that this was not something which could be required under planning legislation. Rachelle Shepherd-Dubey questioned there could be an informative regarding encouraging the inclusion of a lift.

Rachelle Shepherd-DuBey asked how the contaminated land would be treated and was informed that this was addressed in conditions five to eight.

Stephen Conway commented that there was a lack of amenity space and that the majority of amenity space was provided as balconies. Jeanette Davey stated that eleven out of fourteen dwellings had some form of outside space. There was also nearby outdoor space in the form of allotments, a play park and Elms Field.

A number of Members were of the opinion that the affordable homes provision was insufficient. The requirement for the location was 20%, however only two dwellings would be affordable housing. It was noted that the viability study had indicated that two as opposed to three was acceptable.

Andrew Mickleburgh queried how density compared to other developments. Jeanette Davey confirmed that the application complied with policy requirements.

Andrew Mickleburgh expressed concern regarding parking and questioned the impact on the surrounding area. Judy Kelly responded that the parking would be unallocated. There was a requirement for thirteen spaces and the applicant would be providing fourteen parking spaces. The area in the vicinity had heavy parking restrictions so she would not anticipate overspill into these areas. Visitors could make use of nearby car parks such as at the station.

Andrew Mickleburgh queried the access point on to the already congested Barkham Road and questioned whether access on to Oxford Road would be more appropriate. He was informed that there was existing access on to Barkham Road and the application would actually reduce the intensification of access. There were one-way restrictions on Oxford Road which may have made it less desirable to the applicant and future residents.

Abdul Loyes asked what the minimum requirements for the gross internal area were. Janette Davey commented that policy TBO7 had standards for one-bedroom flats occupied by two people and two-bedroom flats occupied by three people. A one-bedroom flat should be a minimum 50m² and a two bedroom flat, a minimum of 61m².

RESOLVED: That application 200700 be approved subject to the completion of a legal agreement pursuant to section 106 of the Town and Country Planning Act 1990 (as amended) to secure provision of affordable housing and an Employment Skills Plan, conditions and informatives as set out in agenda pages 122 to 129, the proposed additional condition set out in the Members' Update and the additional informative discussed at the meeting.

48. CONTINUATION OF MEETING

At this point in the meeting 10.25pm, in accordance with Procedure Rule 8.2.9, the Committee considered a Motion to continue the meeting beyond 10.30pm, up to no later than 11pm, to enable further business on the Agenda to be transacted. This was proposed by Chris Bowring and seconded by Simon Weeks.

RESOLVED That the meeting be extended past 10.30pm, up to no later than 11pm, to enable further business on the Agenda to be transacted.

49. APPLICATION NO.192884 - LAND AT STANBURY HOUSE, BASINGSTOKE ROAD, SPENCERS WOOD

Proposal: Full application for the proposed change of use of agricultural land to recreational use (D2 Use class) (to provide a Suitable Alternative Natural Green Space) with associated access, car park, footpaths and landscaping works.

Applicant: Cooper Estates Strategic Land Limited

The Committee received and reviewed a report about this application as set out in agenda pages 157 to 182.

The Committee was advised that the Members' Update included the following:

- Additional consultation response from WBC Green Infrastructure;
- Additional condition around hours of work.

In response to question from Angus Ross, Graham Vaughn confirmed that condition 10 referred to the provision of a footpath link between the footpath network within the site and Shinfield Byway 25, Woodcock Lane.

RESOLVED: That application 192884 be approved subject to completion of a legal agreement to secure ownership of the land for recreational use as a SANG; its maintenance in perpetuity by the Local Authority; and, a commuted sum towards maintenance, conditions and informatives as set out in agenda pages 158 to 163, and additional condition as set out in the Members' Update.

50. APPLICATION NO 200711 BARTLETTS FARM, SWALLOWFIELD ROAD, ARBORFIELD

Carl Doran did not participate in discussions or vote on the application, having not been present at the previous Committee meeting where the item had been previously discussed.

This application was considered first during the meeting.

Proposal: Full planning application for Installation of a Solar park to include 40000 solar photovoltaic panels, 11 inverter/transformer cabins, a single control building and associated works to include vehicle access and fencing with Environmental Statement.

Applicant: Wessex Solar Energy

The Committee received and reviewed a report about this application as set out in agenda pages 5 to 74 of the Supplementary Agenda.

Harry Cannon, resident, spoke in objection the application. He commented that with regards to alternative sites, he heard that the Council was giving notice to tenant farmers to vacate land for the purposes of using it for solar. This suggested that Bartletts Farm was not the only site available. Harry Cannon stated that additional drawings and information provided did not show an elevation from the north, which would be 50ft tall and difficult to screen. He felt that this would have a negative visual impact.

Richard Wearmouth, Applicant, spoke in support of the application. He commented that their Landscape Architect and the Council's Landscape Officer had agreed that the application had no unacceptable impacts. Once existing screening by vegetation and topography was considered, views from the surrounding area, were limited. There would

be additional buffer zones, new hedgerows, trees, and planting. With regards to land quality, Natural England as a statutory consultee, had not raised any concerns. The landowner had reported low crop yields despite the application of lime and fertiliser. During the life of the solar farm the land would not be lost, and the quality of the land would be improved as more intensive agricultural practices were temporarily moved away from and regenerative farming practices were introduced. He reminded Members that the land would eventually return to its current use. Richard Wearmouth asked that the application be considered on its own merits. A Site Alternative Assessment had been provided in the application, which had set out the applicant's systematic approach to site identification. Richard Wearmouth added that solar parks needed to be able to connect to the Grid, to be accessible from the road network and for a landowner to be willing to accommodate it on their land. This site met these requirements. The site was free from National and Policy designations and Rights of Way, had an onsite Grid connection and had been made available by the landowner for use.

Stuart Munro, adjoining Ward Member spoke in objection to the application. He commented that the solar farm at Sheepbridge Court had been in place for six years and that the hedging provided, provided insufficient screening.

Gary Cowan questioned whether the site was the most appropriate location. He commented that it was one of the first times the Council had dealt with an application of that size and that it was a learning curve for Members and Officers. He noted that other Councils, such as Renfrewshire, had professional standards, and that the Council did not appear to have these. Mark Croucher responded that the Council did have a policy regarding renewal energy provision, CCA5, which the application accorded with. He went on to highlight a solar farm application which had been granted at Committee stage in 2014.

Gary Cowan expressed concern around the lack of drawings or photos of potential before and after, and site elevations and on-site vertical structures. Mark Croucher commented that Members had been on a site visit to visualise how it may look, and that the applicant had submitted further information to assist. A Landscape Visual Impact Assessment had been submitted by the applicant. Catherine Brimble, Landscape Officer, believed whilst there would be some limited harm, it would not be unacceptable.

Gary Cowan commented that BRE provided guidance on the development of large-scale ground mounted solar and PV systems. He asked whether this guidance had been followed. The preference was for land used to be either 3B, 4 or 5, which was not the case in this application. Mark Croucher indicated that Officers had looked at Planning Practice guidance in detail.

Gary Cowan queried the potential impact of glare from the solar panels. He expressed concern regarding the fact that the Aviation Authority had not been consulted given the proximity of Heathrow Airports, Blackbushe and Farnborough Airports. Mark Croucher indicated that the site was not within an aerodrome safeguarding zone and therefore there was no requirement to consult.

Gary Cowan commented that the Ministry of Housing, Communication and Local Government policies on light pollution and low carbon energy 2015 stated that Councils should develop a renewable and low carbon energy policy. However, this had not been the case.

In response to a question from Andrew Mickleburgh regarding alternative sites, Mark Croucher commented that the applicant had provided significant information as to how the site had been selected. There were several factors to be considered such as land quality, willingness of the landowner and access to the Grid. Twenty landowners had been contacted and only two had responded. Officers felt that the application accorded with the Development Plan and should be approved on its own merits.

Andrew Mickleburgh queried the height of the solar panels. He questioned whether the hedgerows would be able to ensure full screening from all distant locations, and if not, how many locations would be able to easily see the panels. Mark Croucher confirmed that the panels would be 2.43m. With regards to visibility, Catherine Brimble stated that the Landscape and Visual Impact Assessment submitted with the application was very thorough. From long distance and medium distance views, there would not be views of the site because of the topography surrounding the site and the vegetation cover. Andrew Mickleburgh queried whether all statutory consultees had responded and at what stage in the process had residents of Farley Hill been informed of the public consultation. Simon Weeks stated that 89 letters of objection had been received, which suggested that the application had been known about. Mark Croucher commented that the statutory consultees had all been consulted but he was unsure as to whether they had all responded. With regards to the public consultation there was only the obligation to consult the immediate properties. However, a second round of wider consultation had been carried out. Residents would have had approximately five months in which to submit a consultation response.

Andrew Mickleburgh asked about the restoration of the land to its existing condition after 40 years, and examples of this being done elsewhere to prove that it was possible. Mark Croucher stated that solar energy was relatively new so there were few examples of decommissioning.

In response to a question from Rachelle Shepherd-Dubey, Mark Croucher confirmed that having a view was not a right of ownership.

Stephen Conway commented that he and many other Members had voted in favour of the Climate Emergency Action Plan, but that he was unsure whether the proposed site was suitable and sustainable. He still had concerns regarding the harm to the landscape and felt that renewable energy sources should not damage the landscape.

Abdul Loyes asked whether 33KvA was the highest capacity and was informed that it was the minimum that could be tapped into.

Angus Ross commented that he still regretted the loss of agricultural land.

Gary Cowan proposed that the application be refused on the grounds of inadequate plans and drawings. This proposal was not supported.

RESOLVED: That application 200711 be approved subject to prior completion of a legal agreement pursuant to section 106 of the Town and Country Planning Act 1990 (as amended) to secure an Employment Skills Plan, and conditions and informatives as set in pages 3 to 8 of the Supplementary Agenda.

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Agenda Item 54.

Application Number	Expiry Date	Parish	Ward
N/A			Emmbrook;

Applicant	Andrew Fletcher
Site Address	Land between Jubilee Avenue and Martins Drive, Wokingham, RG41 1NY
Proposal Type	Path Creation Order
PS Category	1
Officer	Andrew Fletcher
Reason for determination by committee	Scheme of delegation

FOR CONSIDERATION BY	Planning Committee on Wednesday, 10 February 2021
REPORT PREPARED BY	Andrew Fletcher, Green Infrastructure Manager

SUMMARY

A new section of path is required along unregistered land in order to construct the planned route of Greenway Route B at Jubilee Avenue, Wokingham. Part of the land required is unregistered and the Council has been unable to establish any land owner, and therefore has been unable to obtain the route by agreement. As a solution to this issue the committee is asked to consider using the Council's powers to create a new path across the land by a Path Creation Order, under Section 26 Highways Act 1980.

This enables the path to be created without specific land owner consent. The order process will advertise the making of the order in newspaper publications and on site, and provides for any objections to be received. If no objections are received or are subsequently withdrawn the Council can confirm the order. If any objections are not withdrawn the Council may send the order to the Secretary of State for confirmation.

PLANNING STATUS

Legal Framework for the Decision: Orders for the creation of footpaths, bridleways or restricted byways may be made under section 26 of the Highways Act (1980), if the local authority is satisfied that, having regard to the extent to which the path or way would add to the convenience or enjoyment of a substantial section of the public and the effect which the creation of the path would have on the rights of the persons interested in the land, it is expedient that the path or way should be created.

Defra Rights of way Circular 01/09 is also relevant.

RECOMMENDATION

That the committee authorise the making of the PATH CREATION ORDER:

1. That authorisation is given to the making of an order under s.26 Highways Act 1980 Act to create a bridleway as shown in Appendix A to this report, on the basis that the creation would add significantly to the convenience and enjoyment of the public using the Greenway in this area;

2. If no objections to the order are received or any such objections are withdrawn, that the order may be confirmed;
3. If objections are received and sustained, the order may be sent to the Secretary of State for confirmation.

CONSULTATION RESPONSES

Executive Member for Environment & Leisure	No comments received
Ward Members	No comments received
Wokingham Town Council	No objection
Loddon Valley Ramblers	No comments
British Horse Society	No comments
Mid & West Berks Local Access Forum	No comments received

BACKGROUND

1. The Wokingham Greenways project is aimed at linking the major development sites within the borough with each other and also to the existing communities and places of interest/employment. Greenways will provide non-car accessible commuting journeys to work and school as well as new leisure opportunities to get out into the countryside and greenspaces.
2. The project will link with and align to additional greenways and paths proposed to improve the non-vehicular connectivity within the Borough and to adjoining boroughs. This internal network of Greenways will utilise and extend beyond the public existing rights of way and highways network to public green space and point of interests.
3. The Council has included Greenways as part of the Wokingham Borough Council Core Strategy adopted in January 2010 and are captured in policy CP10 items 20 and 21 and SDL policies CP18-21. The implementation of the Greenways is included as one of the main objectives in the Wokingham Borough Council Rights of Way Improvement Plan 2020, adopted by the Council in April 2020.
4. The first route was constructed at California Country Park in 2017, with the final sections of the route completed in 2020. This is known as California Way.
5. The Council is currently progressing towards the implementation of Greenways Route B, which will run from Cantley Park to Arborfield Cross, linking Woosehill Meadows and Foxhill.

This route is split into 4 separate sections:

Section 1: Cantley Park to Woosehill Meadows via Matthewsgreen

Section 2.1: Woosehill Meadows

Section 2.2: Woosehill Meadows to Bearwood Road via Foxhill

Section 3: Bearwood Road to Arborfield Cross via Coombes Lane and Coles Lane.

Maps showing the different comprising sections of the route are included as Appendix B to this report.

6. Initial public consultation for the whole route was undertaken between March-May 2019. The consultation responses were 64% in support, 12% objection and 24% neither supporting nor objecting. The consultation report for the initial public consultation is included as Appendix C to this report.
7. The initial proposed route included a path through Joel Park, however subsequent discussions with the landowner (Wokingham Town Council) highlighted significant concerns and objections from the Town Council to the use of Joel Park as part of the route and permission was denied. The alternative route adjacent to Jubilee Avenue was subsequently identified in consultation and agreement with Wokingham Town Council.
8. As a result of this change, an additional localised public consultation was undertaken for Section 1 between July - September 2020. The consultation responses were 26% in support, 26% objecting and 48% neither supporting nor objecting. The consultation report for the localised public consultation is included as Appendix D to this report.
9. Part of the planned route of Section 1 will utilise land adjacent to Jubilee Avenue to link from Cantley Park to the entrance of Martins Drive. This additional section provides an extension of the off-road route, before joining with Clifton Road and Brook Close to provide the final link to the Greenway at Woosehill Meadow. This allows users to avoid the need to negotiate the busier section of Clifton Road where it meets with Holt Lane, and avoids the parking area adjacent to Joel Park.
10. Part of this land is owned by Wokingham Borough Council, and part is owned by Wokingham Town Council, and there exists a middle section of the land which is unregistered.
11. Wokingham Town Council have agreed in principle to allow a long-term permitted path across their land to facilitate the Greenway, but a landowner for the unregistered section of land has not been identified.
12. The unregistered area of land is identified in the Council's records as public open space maintained by Wokingham Borough Council. To protect the ongoing highway status of the unregistered section of the land required for the greenway it is considered that the most expedient and appropriate option will be to make a Path Creation Order under Section 26 of the Highways Act to create the path over the land.
13. Section 26 of the Highways Act provides that where it appears to a local authority that there is a need for a footpath, bridleway or restricted byway over land in their area and they are satisfied that it is expedient that the path or way should be created having regard to (a) the extent to which the path or way would add to the convenience or enjoyment of a substantial section of the public or to the convenience of persons resident in the area, and (b) the effect which the creation of the path or way would have on the rights of persons interested in the land, account being taken of the provisions as to compensation contained in Section 28 of the 1980 Act, the authority may by order made by them ...create a footpath, bridleway or restricted byway over the land.

14. Having regard to the benefits to the public as set out in paragraph 8 above, the Committee is therefore requested to consider the making of a Path Creation Order to create a bridleway across the land shown in Appendix A to this report.
15. Once a Path Creation Order is made it is subject to a statutory 6 week public consultation. This will be advertised in local newspapers and any land owner that has an interest in the land must be notified. As a landowner is not able to be identified a dispensation from the Secretary of State will be sought to notify via site notice. If no objections are received to the Order the Council is able to confirm the order. If objections are received which are not subsequently withdrawn the order will need to be sent to the Secretary of State for confirmation.
16. Section 27 of the Act confers a duty on the authority to carry out the works necessary to physically create the new path. Section 28 confers a duty on the authority to pay compensation when a claim is made and depreciation or damage is shown to exist. In such a case, compensation would payable equal to the amount of the depreciation or damage, however Officers do not consider that the impact of the proposed Order on the value of this land or the nearby land would be significant given the proximity of the land to Martins Drive, Holt Lane and Clifton Road.

PLANNING POLICY		
Adopted Core Strategy DPD 2010	CP10	Improvements to the Strategic Transport Network
	CP20	North Wokingham Strategic Development Location

The Public Sector Equality Duty (Equality Act 2010)
<p>In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief.</p> <p>There is no indication or evidence (including from consultation on the proposal) that there will be any adverse impacts upon protected groups as a result of the Path Creation Order.</p> <p>HUMAN RIGHTS</p> <p>If the making of the order is approved, the following principles of the Human Rights Act 1998 may be engaged:</p> <ul style="list-style-type: none"> • The First Protocol Article 1 (<i>Every person is entitled to the peaceful enjoyment of his possessions. No one shall be deprived of his possessions except in the public interest and subject to the conditions provided for by law</i>). The Highways Act 1980 provides the legal basis for controlling the use of property in accordance with the general interest. There must be a demonstrable “general interest” for an imposed public right of way creation such as for example a need identified in a ROWIP which is the case in relation to greenways scheme. It is intended that the authority will

take all steps reasonably necessary to ensure that any interference will be no greater than necessary although it should be noted that there is no identifiable landowner in respect of the land in question and the land is already used by the public given its inclusion in the Council's public open spaces maintenance regime.

- Schedule 1 Part 1 Article 6 (*In the determination of his civil rights and obligations everyone is entitled to a fair and public hearing within a reasonable time by an independent and impartial tribunal established by law*) The public path order process provides for any person having a valid objection to the making and confirmation of a creation order to be heard before an independent inspector at a public inquiry or a local hearing.
- Schedule 1 Part 1 Article 8 (*everyone has the right to respect for his private and family life, his home and correspondence. There shall be no interference by a public authority with the exercise of this right except such as is in accordance with the law*). The Highways Act 1980 provides the legal basis for the interference with this right. It should be noted that there is no identified landowner for the affected land but in any event all reasonable steps will be taken when carrying out the works to ensure that there will be minimal disturbance to the land in question.

CONCLUSION

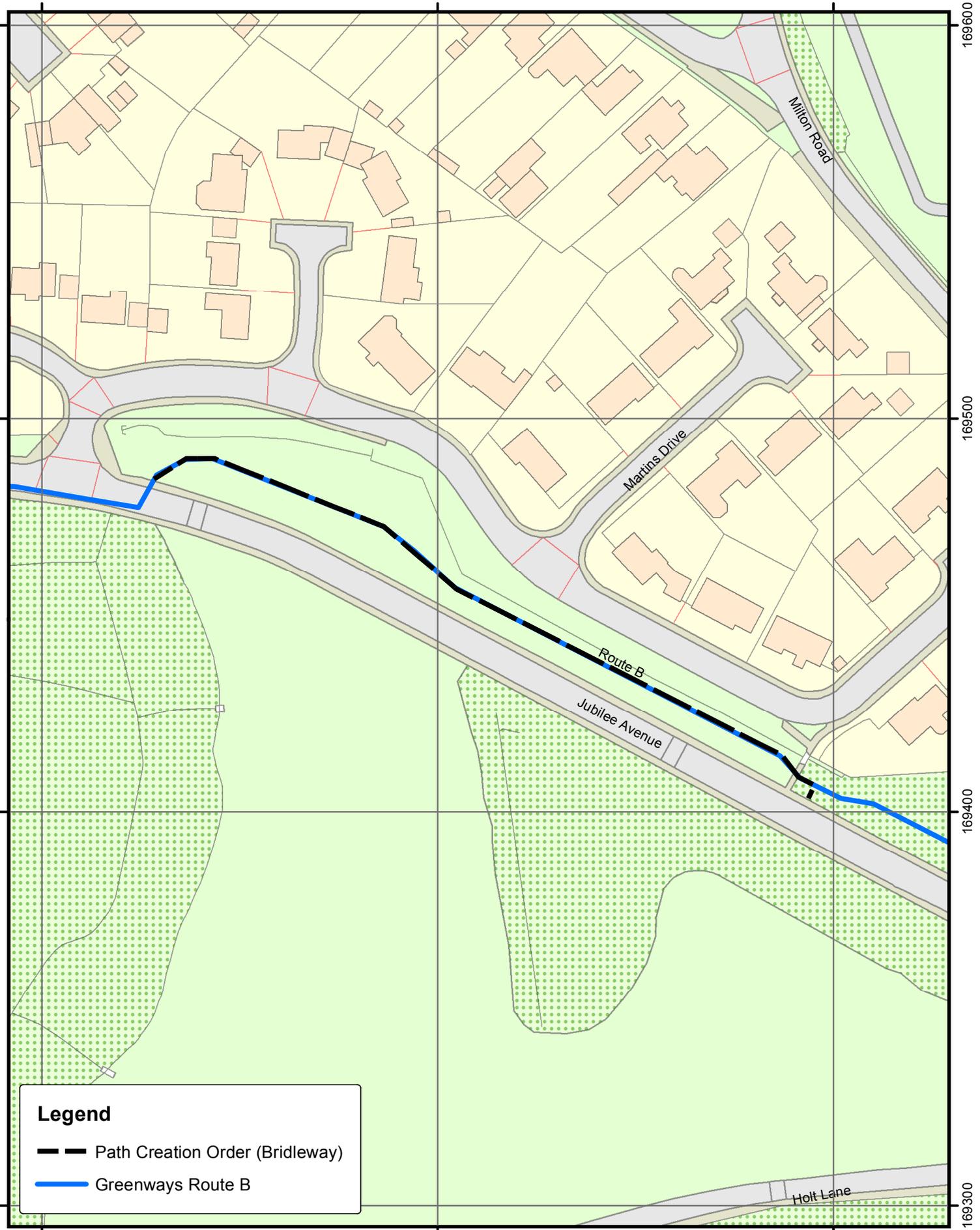
The proposed Greenway route along this section will provide a safe, off-road route for cyclists looking to pass to and from Cantley Park along the Greenway. This has been a long term strategic objective of the Council, and contributes to its climate change and sustainable transport objectives.

This would add significantly to the convenience or enjoyment of a substantial section of the public. The unregistered section of land is a small section of woodland verge adjacent to Jubilee avenue with limited prospects for improvement or development. It is therefore considered that the proposed path would have minimal effect on the rights of any persons interested in the land. As such it is considered that the tests of Section 26 Highways Act 1980 have been met, that it is expedient that the path of way should be created.

It is therefore recommended that the order is made.

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APPENDIX A
GREENWAYS ROUTE B
PATH CREATION ORDER - SECTION 26 HIGHWAYS ACT 1980



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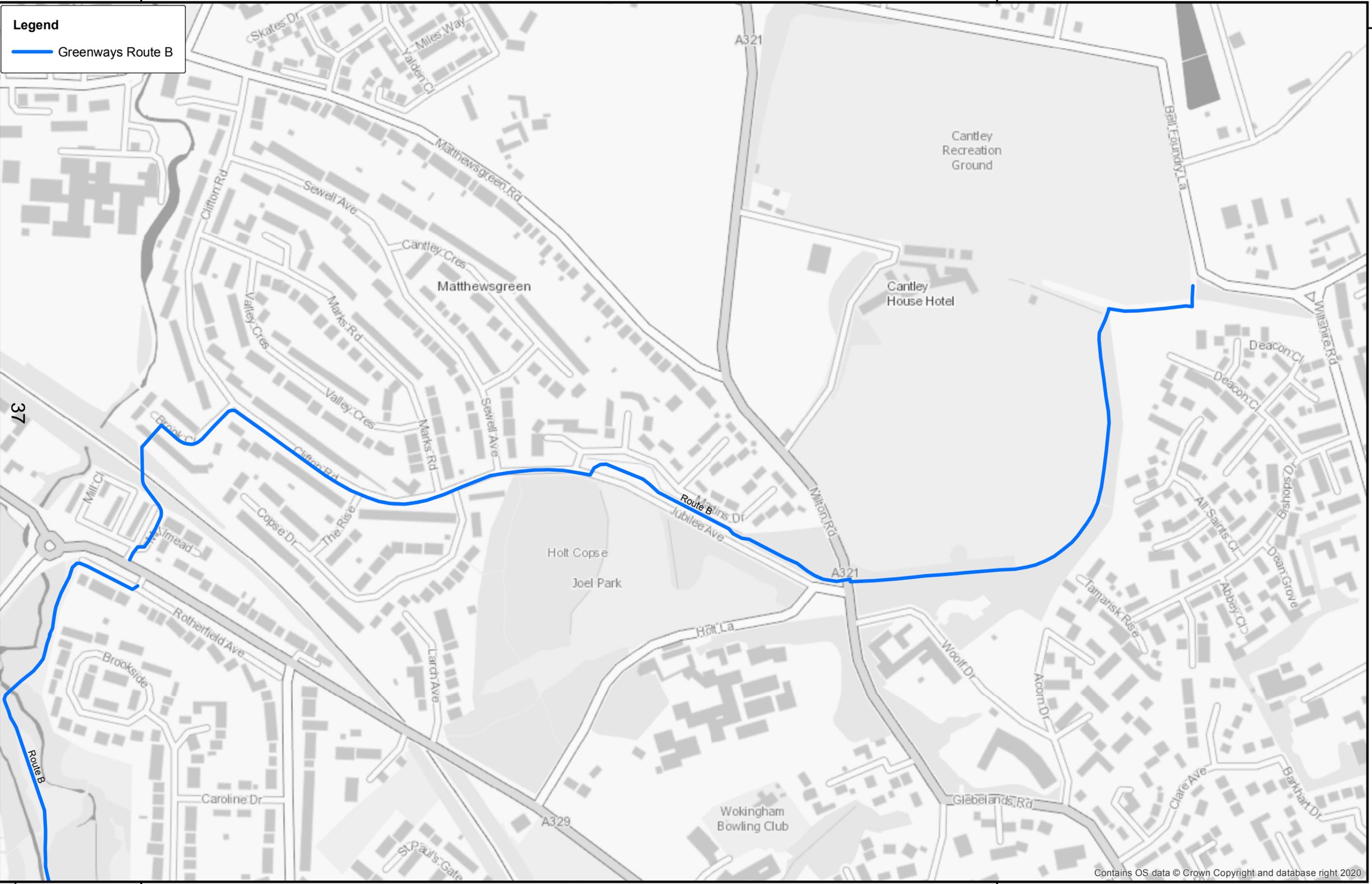
GREENWAYS ROUTE B SECTION 1

170000

170000

Legend

— Greenways Route B



37

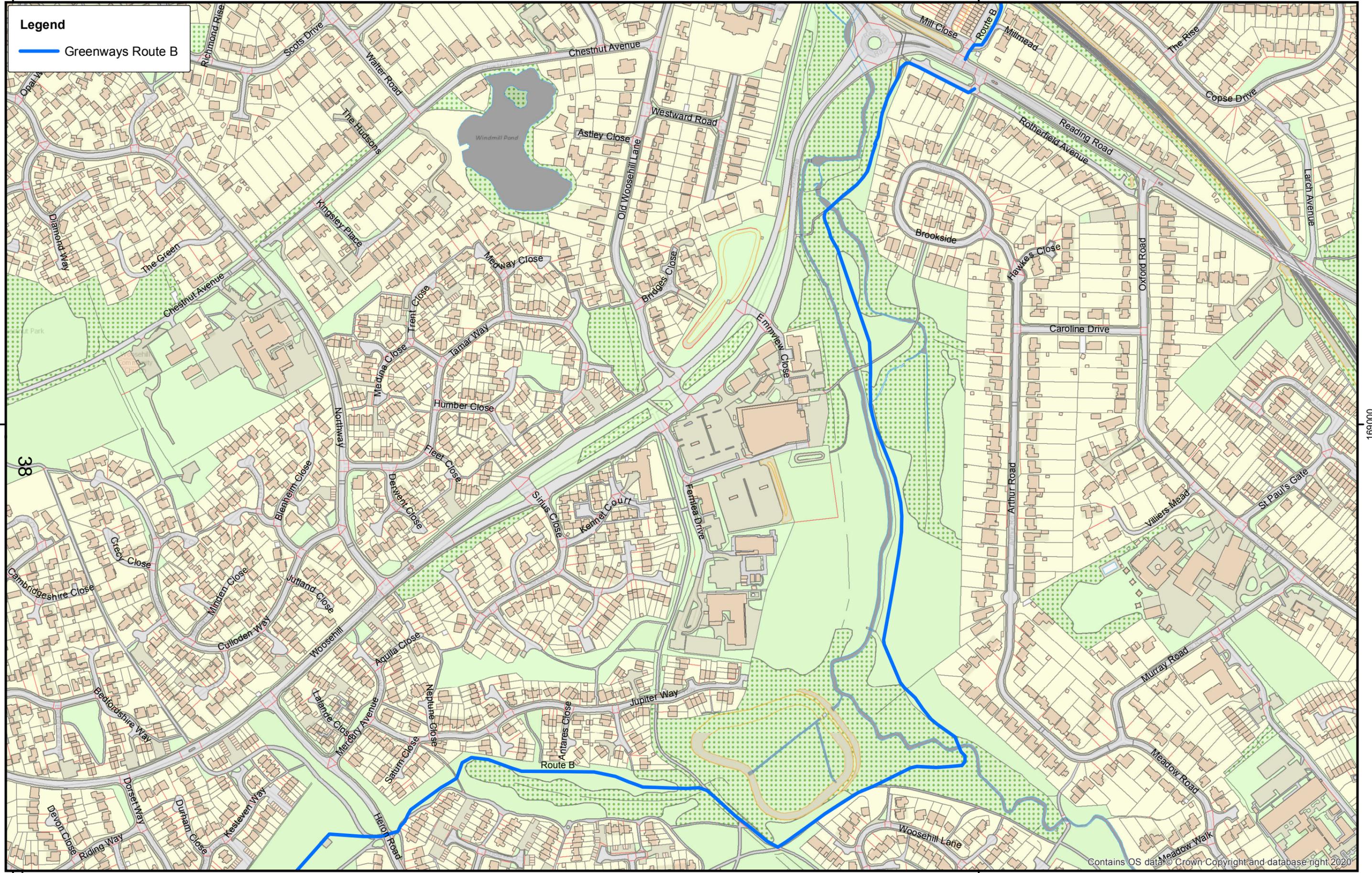


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481000

GREENWAYS ROUTE B SECTION 2.1



169000

169000

38

47900

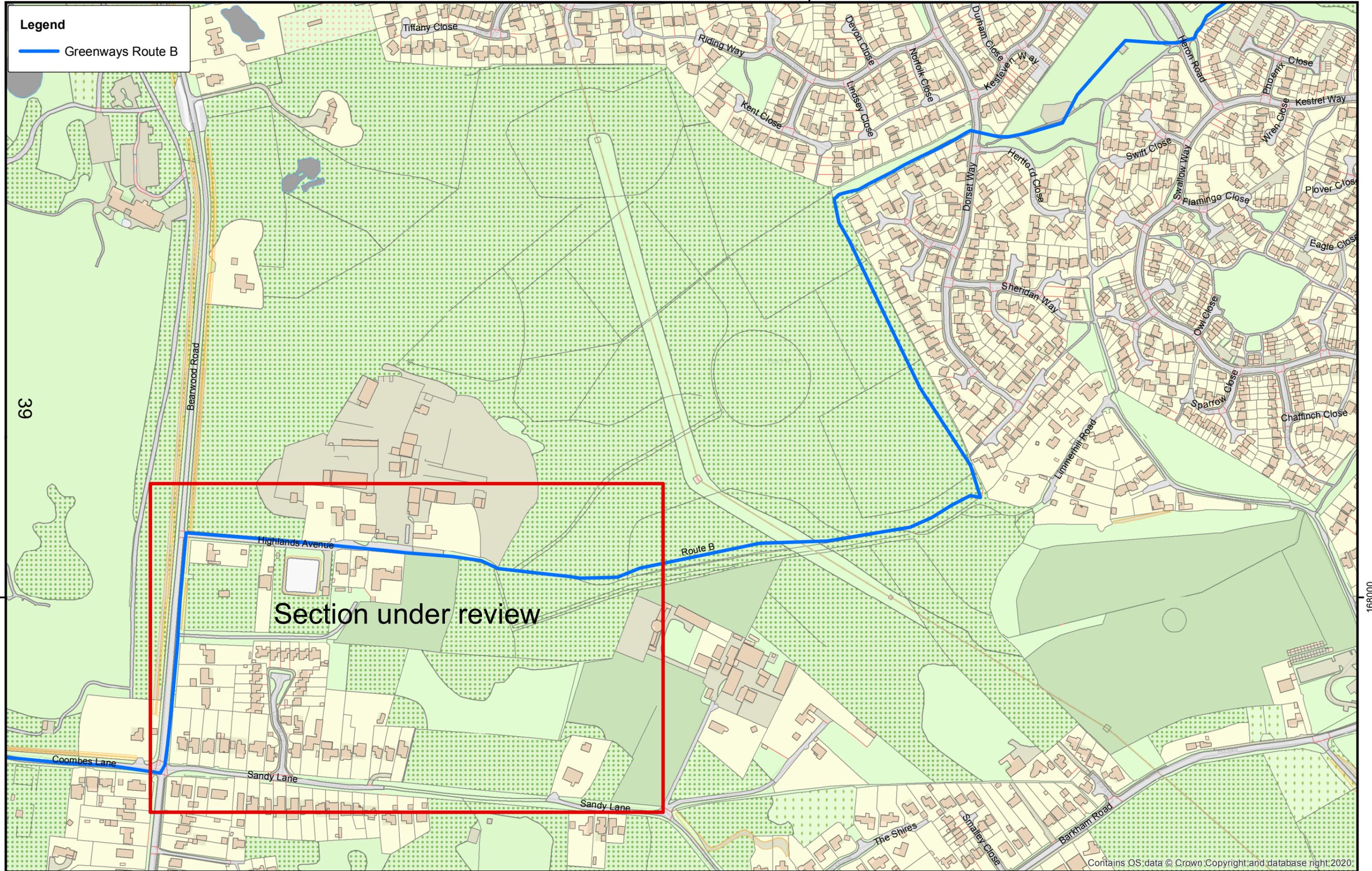
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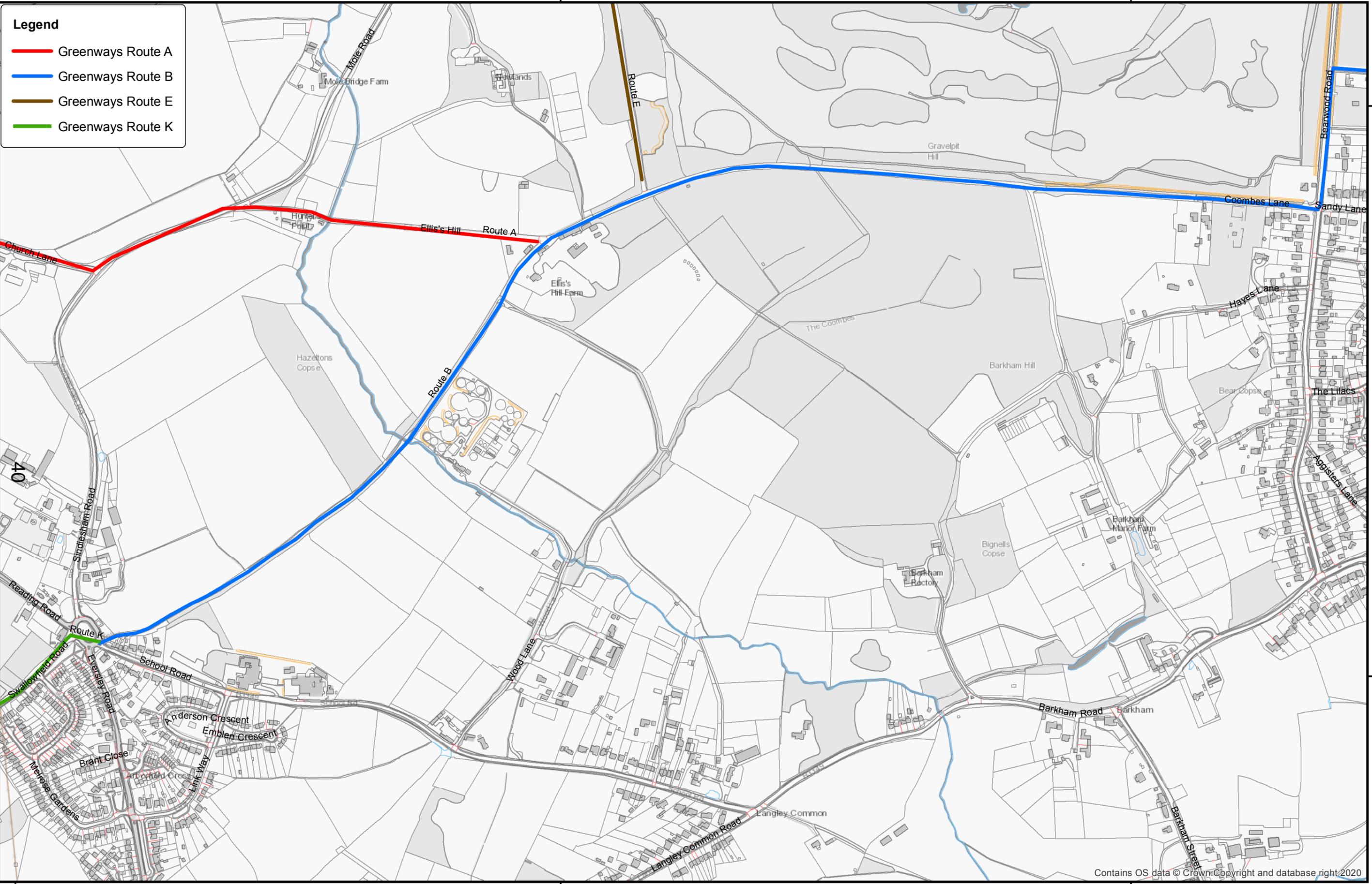
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GREENWAYS ROUTE B SECTION 2.2



GREENWAYS ROUTE B SECTION 3



Legend

- Greenways Route A
- Greenways Route B
- Greenways Route E
- Greenways Route K

168000

168000

167000

167000

40

477000

478000

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WOKINGHAM
BOROUGH COUNCIL

Wokingham Borough Council

Greenways Project

Phase 2 Consultation Report

Introduction and Background

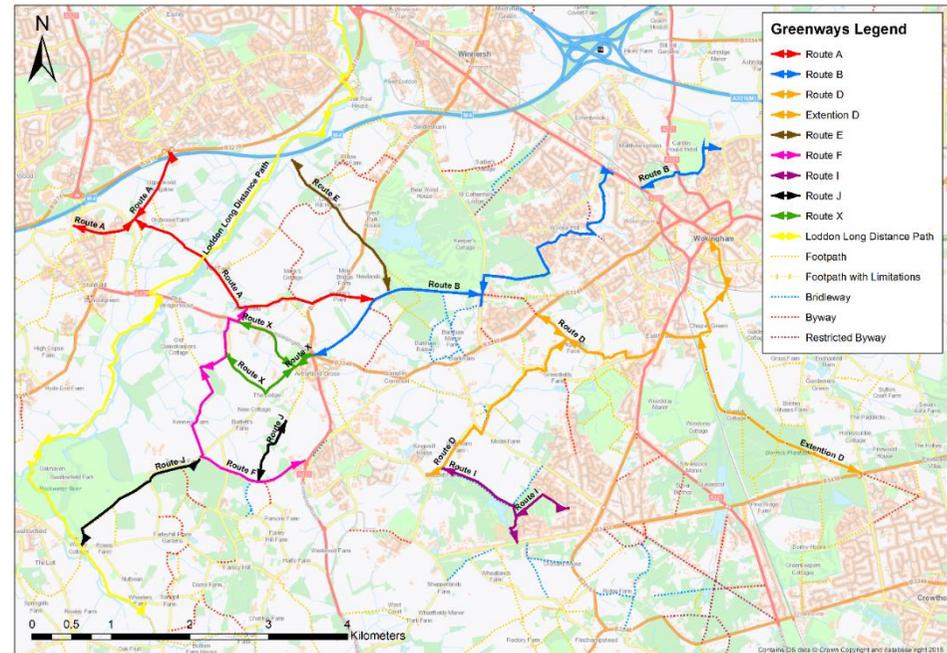
Wokingham Greenways Project is aimed at linking the major developments coming to Wokingham borough with each other and also to the existing communities and places of interest/employment. Greenways will provide non-car accessible commuting journeys to work and school as well as new leisure opportunities to get out into the countryside and greenspaces.

Greenways route network is an SDL-focused network to satisfy the requirements of the current Core Strategy to deliver the traffic free commuting and leisure routes connecting the existing and new settlements and point of interest. The Project will link with and align to additional greenways and paths proposed to improve the connectivity within the Borough and to adjoining boroughs. This internal network of Greenways to extend beyond the Public footpaths to public green space, and point of interest in Wokingham borough.

The Greenways Project also includes River Loddon Long Distance Path (LDP) that links the Thames Valley Path in the north of the borough in Wargrave to the Blackwater Valley Path in the South of the borough in Swallowfield.

Strategic Context

Greenways are referred to in Appendix 7 of the Core Strategy and are captured in policy CP10 items 20 and 21 and SDL policies CP18-21, and the council will include reference to greenways in the Local Transport Plan update due for adoption in 2019. It is intended that the greenways also be included in the scheduled update of the Rights of Way Implementation Plan (ROWIP) as well



Appendix 1 shows the Greenways Route Map across the borough including River Loddon Long distance path.

as the Local Cycling and Walking infrastructure Plan. Both ROWIP and LCWIP will be consulted later this year. The feedback received will be adopted for future greenways, cycleway and right of ways across the borough.

Greenways Phase 1

Greenways Phase 1 called California Way, which links Arborfield Green with the FBC Centre via California Country Park was consulted in 2017 and has already been built.

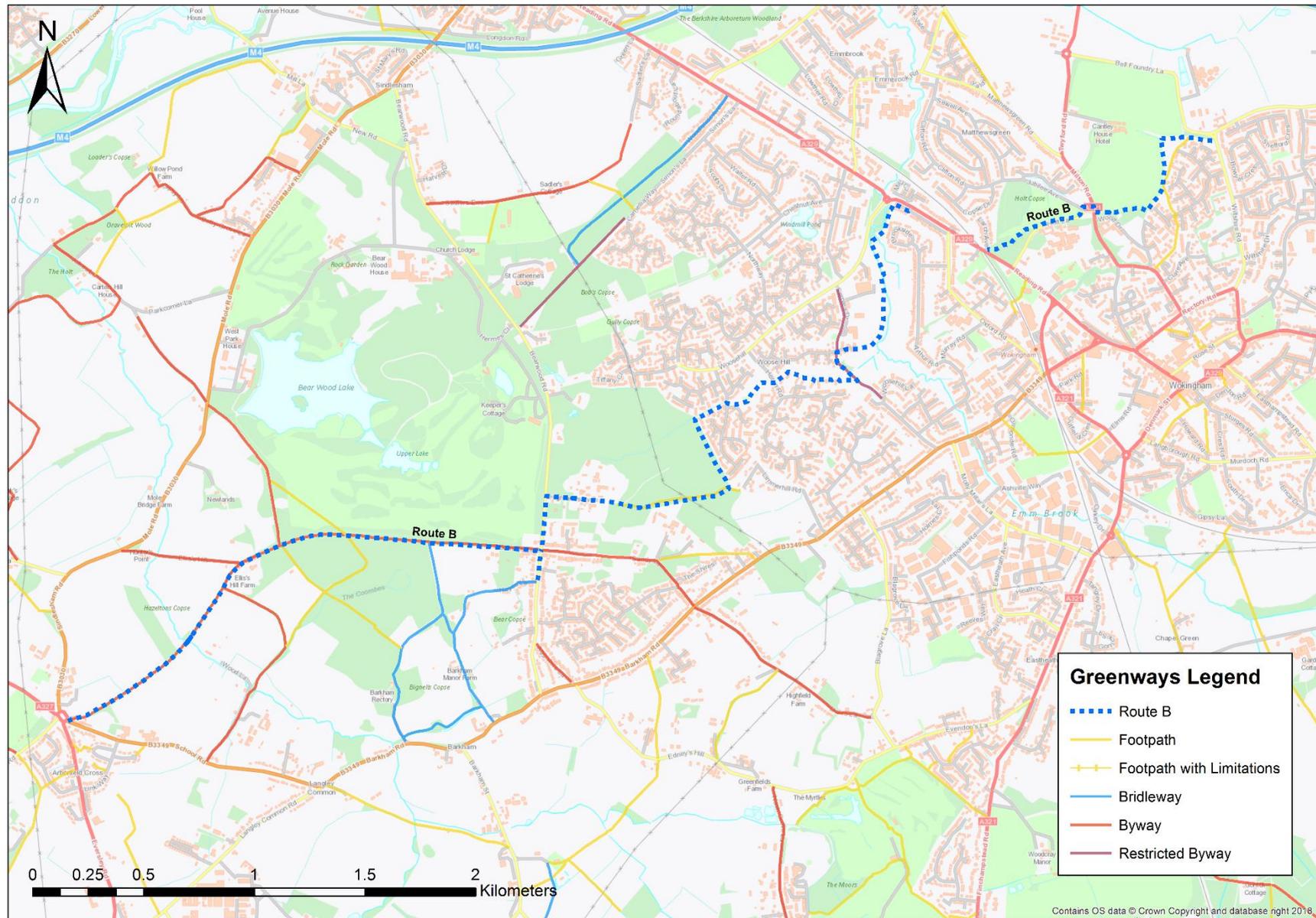


Greenways Phase 1: linking Arborfield Green Development to Finchampstead

Greenways Phase 2

Wokingham Borough Council is starting phase 2 of its greenways project. The greenways project will create a network of traffic free multi-user routes which will connect the new major developments at Arborfield Garrison (Arborfield Green) and Shinfield Parish to Wokingham town and the new major developments in North and South Wokingham. The Greenways Project will add to the borough's existing sustainable route network to create an alternative way to get around the borough.

Wokingham Borough Council carried out consultations in March/April 2019 to seek views on phase 2 which will link Arborfield Green to the new development at North Wokingham via Woosehill. The route for this sustainable link is largely fixed but the feedback from this consultation will help the Council refine the final plans for Route B as well as in detailed planning and shape final decisions on the route design.



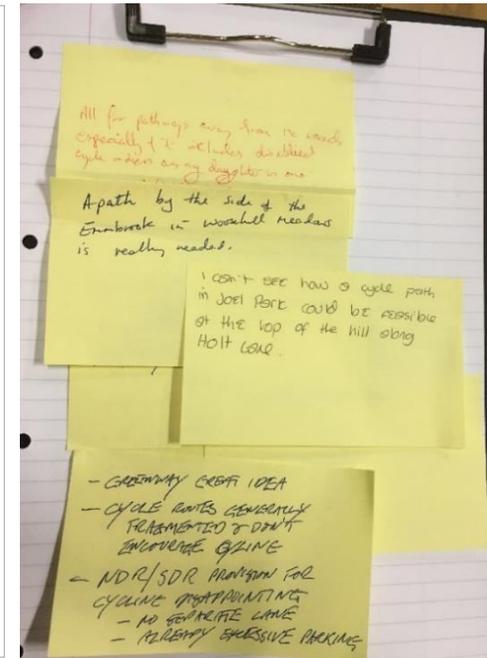
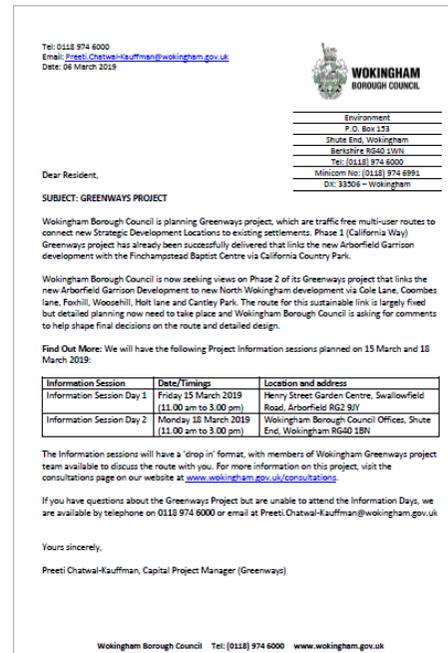
Appendix 2: Greenways Route B

Consultation and publicity undertaken

This consultation was carried out to support the creation of the Greenways Project, which begins with the strategic network of multi-user paths that connecting the major developments coming to the Wokingham borough to each other and also to the existing communities and places of interest/employment.

A six week public consultation was undertaken from 06 March 2019 to 10 May 2019. Letters were posted to the adjacent properties owners and ward members, Parish/Town councils and Greenways user groups were notified to invite feedback. This was followed by attendance at community forums, Drop-in sessions at various locations in the catchment area of the Greenways Phase 2, inviting comments through Wokingham Council's website, Social media channels and press release.

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Pictures above: various consultation tools used

A summary of how the consultation and publicity was undertaken is provided below:

Letter drop/ Emails: Letters were sent to the Landowners and people with properties located next to the proposed Greenways to make them aware of the project and seek their feedback and comments. Emails were sent to notify the ward members, parish and town councils, Greenways user groups, Local access forums and key stakeholders.

Online Information on Website and Social Media: The consultation was advertised and the Greenways Route Map and Consultation document were made available to view on the Council's website as well as shared on the social media.

Attendance at the community forums and Drop-in sessions: Greenways Project information was made available at a number of consultation events with Council officers on hand to discuss this at a number of consultation events (list of events contained in table). The drop-in sessions were arranged at various different locations and at various timings including evenings and weekends in order to reach the maximum people.

News Release: A news release advertising the consultation was posted on the Council's website and social media pages and was sent to the local media.

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Meetings with Local Access Forum and Town/ Parish Council: Wokingham Borough Council held meetings with the Local access forums and the Parish/ Town councils to discuss the routes and to share thoughts on how the greenways project could be taken forward. The Local Access forums and Parish/Town Councils further sent out letters informed the stakeholders. The aim of these meetings was to discuss the local issues and as well as for coordination so that we can could move towards a common plan of action with wide support.



Pictures (from left): North Wokingham forum; Henry Street Garden Centre (Arborfield); Woosehill Community centre)

Table 1: Community Forums, Drop-in-sessions and meetings

Project Information Sessions	Date/Timings	Location and address	Area
Information Session Day 1	Friday 15 March 2019 11.00 am -3.00 pm	Henry Street Garden Centre	Arborfield
Information Session Day 2	Monday 18 March 2019 11.00 am-3.00 pm	Shute End Offices	Wokingham
Information Session Day 3	Saturday 23 March 2019 11.00 am-2.00 pm	Woosehill Community Hall	Woosehill
Information Session Day 4	Saturday 30 March 2019 11.00 am-2.00 pm	Woosehill Community Hall	Woosehill
Information Session Day 5	Thursday 4 April 2019 5.00 pm-7 pm	Shute End Offices	Wokingham
North Wokingham Community Forum	Wednesday 6 March 2019 7pm -9 pm	Emmbrook Senior School	Wokingham
Annual Parish Forum Arborfield	Thursday 4th April, 7.30pm	Arborfield Village Hall	Arborfield
Barkham Council meeting	Wednesday 10 th April, 2019, 11am	Barkham Council office	Barkham
Wokingham Town Council meeting	Wednesday 19 th June, 2019 2pm	Shute End Offices	Wokingham

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Council officers held meetings with various stakeholders to discuss the Greenways Route

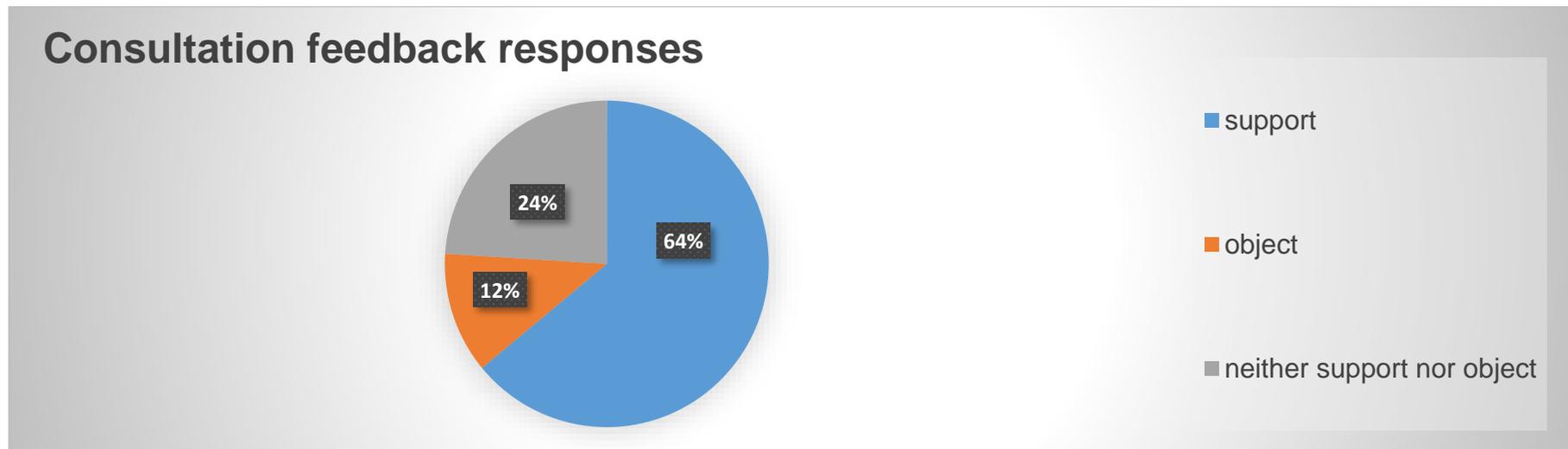
Summary of Consultation Responses

In total, more than 300 people visited us at the Community forums and the drop-in sessions, about 100 respondents left their queries and wished to be informed with final greenways proposals. 50 formal responses were received during the consultation, showing a general support and enthusiasm for the Greenways. These early consultations also provide insight into the specific area requirements and challenges.

A number of themes emerged from these responses which are analysed in the report. As well as analysing the formal responses to the consultation, the report summarises feedback gathered informally during the consultation on social media and face-to-face discussions at the drop-in sessions and community forums. All emails and responses received are included as appendices in the report.

The Table 2 below summarises all comments made during the consultation period. The Council's response to these comments are within this table, and where stated, the final route plans will incorporate some of these suggested changes in accordance with these comments. We did receive some feedback before and after the consultation period which has also been included in the summary table. (Personally identifiable information has been withheld for Data Protection and GDPR reasons).

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Key Themes on the basis of the feedback received:

A number of key themes emerged from the responses, including:

Greenways Theme 1: Access for All - Providing Inclusive and Accessible path network for people of all abilities.

Greenways Theme 2: Safe Traffic crossings – Providing safe traffic crossings on all junctions especially on Bearwood road.

Greenways Theme 3: Equestrian access - Improving the existing bridleway network and enhancing equestrian access to provide a cohesive network for all users to enjoy.

Greenways Theme 4: Connecting neighbourhoods to Schools and train station- this will encourage more people to use sustainable network for everyday commuting

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Greenways Theme 5: No segregation of users on Greenways- the Council's cycle network and Greenways do not include segregation for off-carriage way routes for various reasons:

- The white line / segregation is not enforceable and often ignored;
- In rural areas, non-segregated paths are better for family groups, including walkers and young children (we hope this route is used as a route to school for local primary schools);
- Research by Sustrans indicates that disabled users prefer the whole path to be available so that they have maximum flexibility in its use

Greenways Theme 6: Access for landowners on Coombes Lane and Cole Lane – The access to the landowners, residents and farmers will be retained. They will be consulted as part of statutory process for implementing any traffic regulatory orders on these byways.

Table 2: Formal Responses:

No.	Letter/ Email from	Received	Supports/ Objects/ Comment	Response summary	Wokingham Borough Council Officer response
1	Steve Vale, Loddon Valley Ramblers	18/03/19	Supports/ comments	<p>Loddon Valley Ramblers welcomes the proposals for Phase 2 of the Greenways Project, are generally supportive of the route identified and of the proposed surfacing, landscaping etc. We believe that the Project will enhance a number of routes, particularly at the Arborfield end of the route and through the Coombes.</p> <p>Few comments:</p> <ol style="list-style-type: none"> 1. Linkage of Phase2 with Phase 1? Linking via FP14 and F17 may be an option. 2. The junction of the Greenway with Bearwood road is potentially difficult, although we support linking Coombes lane to Highlands Avenue via Bearwood road. We would like to see the highest standard of crossing on the Bearwood Road at the end of Coombes Lane (Barkham Byway 1), which takes full account of the additional hazards created by the road gradients at this point; <ul style="list-style-type: none"> • substantial widening of the footway along the E side of Bearwood Road from Sandy Lane to Highlands Avenue, with a physical barrier between the Greenway and the road; • the creation of a Greenway-standard link on the west side of the Bearwood Road between Hayes Lane (Barkham Bridleway 5) and Coombes Lane, so that users of the former can make a safe and easy transition on to the new Greenway, and can cross Bearwood Road using the new crossing, rather than doing so at the east end of the Bridleway 5. 3. Access from Dorset Way down the East side of Foxhill needs to be of adequate width, to allow for buggies, walkers and cyclists to pass. 4. Conversion of Wokingham Footpath 1 to multiuser path 5. Conflict of traffic from A1 metal recycling with Highland Avenue. 6. The impact of the Greenway on parking and traffic movements at the west end of Sandy Lane needs to be taken into account. 7. Fast traffic on the Bearwood road and at the junction with Highland Avenue. 8. Sandy Lane-Coombes Lane-Arborfield is a well-established recreational route for horse-riders, off-road motor bikes, 4WDs as well as cyclists and walkers and there could be wide-spread opposition to an all-weather surface. This 	<p>The Greenways project has been phased for ease of delivery. The Greenways is intended to provide continuous link to the new developments with existing.</p> <p>Traffic Audits will be carried out as per statutory guidance and the traffic junctions and crossings will be designed confirming to the standards.</p> <p>The footway will be designed and widened as much as possible.</p> <p>The Public rights on Greenways Wokingham</p>

				<p>byway also suffers from very muddy conditions in the winter at its Eastern suggesting serious maintenance implications for this stretch. The issues regarding existing vehicular rights also needs to be clear, in terms of both existing rights and how these will impact on/be affected by the Greenway. There could be segregation issues, and gradients and access for the disabled may again be an issue. There is a ford to be negotiated - presumably a bridge is proposed.</p>	<p>Footpath1 and Highland Avenue will be increased to for cycling and horse-riding.</p>
2	Mid & West Berkshire Local access forum	15/04/19	Support/ Comments	<p>The M&WBLAF are generally supportive of the Greenways Plan, and wish to see it reflect more fully the ROWIP route already widely consulted upon by WBC with this LAF over a number of years. General concerns of this proposal are:</p> <ol style="list-style-type: none"> 1. Whilst it would be wonderful to address the problems of Byway 9, which is often impassable in winter, the engineering challenges of providing a route here must be carefully assessed before work commences. One section leading to Arborfield Cross is on a very steep incline with considerable leaf-fall in autumn and another at the top of the hill in the Coombes Woodland is extremely boggy year round and would seem to require a good deal of drainage, with ongoing maintenance to ensure that it is suitable for vulnerable users. 2. We understand that in the past it has been mooted by WBC in ROWIP discussions that Byway 9 could be given 'restricted byway' status? It should be noted that mid-way along the route is a business which operates (unlawfully??) a storage business for Lorries. Unless this becomes a Restricted Byway this will require passing places for where vulnerable users, of which there are many, meet Lorries. It should be noted that this is a popular route for horses and dog walkers and easy connections to the Woosehill Estate created by this Phase of the Greenway will increase demand for cyclists, walkers and riders. All vulnerable users. 3. Poor visibility and vegetation growth at the junction of Byway 9 with Bearwood Road would demand that a safe crossing of some kind is installed, and its position seriously considered. This LAF notes that a section of the Rights of Way Improvement Plan (N/S route from Simons La to Coombes La) would 	<p>The Greenways route will be designed to be all weather and inclusive path.</p> <p>Statutory consultation will be taken ahead of changing any PROW status or applying any traffic regulatory orders.</p> <p>Safe crossing will be designed on the basis of the road safety surveys and traffic Audits.</p>

				<p>provide a safer off-road route through land owned by Bearwood Lakes Golf Club. A user activated crossing could then be positioned in Bearwood Road at the junction of Highlands Avenue. The emerging Arborfield and Barkham Neighbourhood Plan supports this route, by encouraging safer crossings and off-road routes and environmental protection for the Rhododendron Avenue on Bearwood Road (see notes below). Please note that the Footpath section of Highlands Avenue will need to be upgraded to Bridleway or Restricted Byway status, but it seems to be an existing route used by vehicle traffic despite its Footpath status. A further section of the ROWIP near to the Woosehill Housing estate could be used to allow cyclists off-road access the centre of Wokingham (and its Railway Station) from Barkham, Arborfield and Woosehill. The other end of the Coombes Lane route emerges at the infamous 'Mole Road crossing' of Byway 4/Road used as Footpath 6, and so logic would state that a crossing be installed here at the same time to further the aims of providing 'circular routes' and to allow off-road cycle access to the centre of Wokingham from areas of Arborfield.</p> <p>4. It should be noted that the Rights of Way Improvement Plan also covers the upgrading of Barkham Footpaths 8, 9 & 10 to Bridleway and these are in the vicinity of this Greenways route. It would be a missed opportunity if these upgrades are not included in this phase of the Greenways Plan as they are routes which could not only improve off-road connectivity between the Arborfield SDL and Wokingham Railway Station, but between the various SANGs being created as mitigation for the Thames Basin SPA.</p> <p>5. The promotion for this Consultation includes a section which questions whether equestrian use will be possible. I was informed at the drop-in session this is an error. Can we have written confirmation of this please?</p> <p>Specific equestrian concerns are;</p> <p>6. The Foxhill to Woosehill section, retaining existing equestrian use. 7. Safety concerns about the junction of Sandy Lane and Bearwood Road. 8. Conflicting uses of Byway 9 and safety issues arising from new surfacing. 9. Issues for riders around the ford crossing on byway 9.</p>	<p>All these routes will be reviewed as part of Revised Public Right of Way Improvement plan.</p> <p>The existing equestrian access will be retained and rights on new routes will be provided as much as possible.</p>
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			<p>10. A more detailed submission specifically concerning equestrian issues will be available after discussion by the LAF at our meeting on 22nd May.</p> <p>11. In the meantime, it should be noted by WBC that SANGs in the area are designed specifically to mitigate against use of the Thames Basin SPA, and that in Natural England's document "SANGs Guidance" there is evidence that the SPA is used by horse riders;</p> <p>12. "The majority of the people using most of the SPA sites come to walk, with or without dogs. At two or three sites there were also a significant number of cyclists and joggers. A small amount of horse riding also occurs at some sites."</p> <p>13. Therefore, consideration for equestrian needs in the local area is vital, especially in this case, in relation to the network of existing PROWs and ROWIP which should aim to connect the SANGs being provided in this area.</p> <p>14. The LAF has worked extensively with WBC's Countryside team on the ROWIP, and now would like to highlight the subject of a network of routes for a range of users connecting a number of SANGs, as Natural England's Guidance, Network of Sites section also states;</p> <p>15. "The provision of longer routes within larger SANGS is important in determining the effectiveness of the authorities' network of SANGS as mitigation, because a large proportion of visitors to the SPA have long walks or run or bicycle rides. The design of routes within sites smaller than about 40 ha will be critical to providing routes of sufficient length and attractiveness for mitigation purposes.</p> <p>16. Where long routes cannot be accommodated within individual SANGS it may be possible to provide them through a network of sites." And,</p> <p>17. "The findings suggest that SANGS should aim to supply a choice of routes of around 2.5km in length with both shorter and longer routes of at least 5km as part of the choice, where space permits. The fact that a considerable proportion of visitors were walking up to 5km and beyond suggests the</p>	<p>The Greenways are designed to be multi-user routes safe for walking, cycling and horse-riding.</p> <p>The feedback from LAF regarding SANGs is useful and will be considered while designing Greenways Routes through SANG. The proposed Greenway routes do not pass through a SANG.</p>
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				<p>provision of longer routes should be regarded as a standard, either on-site or through the connection of sites along green corridors.”</p> <p>Relevant Policies in the emerging Arborfield and Barkham Neighbourhood Plan;</p> <p>18. POLICY GA2: IMPLEMENT LOCAL NETWORK OF GREEN ROUTES FOR NON-MOTORISED USERS 1. Development proposals must retain and protect and take available opportunities to improve and extend the networks of Greenways and other PROWs. Proposals should support:</p> <ul style="list-style-type: none"> a) The achievement of a network of all-weather routes. b) The provision of safe crossings for the benefit of non-motorised users, wherever PROWs meet primary routes c) Better connectivity throughout the plan area, including the new developments proposed at Arborfield Green. d) Creation of opportunities for sustainable transport including safe routes to schools. e) Provision of better access to the countryside and surrounding destinations for amenity purposes. <p>3. Refer also to the Arborfield and Barkham Greenways Report (Annex XII)</p> <p>19. POLICY IRS3: PROTECTION AND ENHANCEMENT OF THE NATURAL ENVIRONMENT AND GREEN SPACES 3. Non-Designated Sites of Natural Interest have been identified within the plan as follows (see Map K): a) The Coombes woodland and adjacent areas b) The Holt woodland c) Rhododendron avenue along Bearwood Road</p>	<p>Arborfield and Barkham Neighbourhood Plan has been referred to while designing and planning Greenways.</p>
3	Barkham Parish Council	01/04/19	Supports/ Comments	<p>In principle the Parish Council fully support the Greenway concept. It would be a very popular amenity and would permit some local journeys by foot or cycle. There are a number of riding stables in the area and the riders would make good use of at least a part of the route.</p> <p>Phase 2 is intended to run from Arborfield Cross, along Cole Lane and Coombes Lane and then via Fox Hill to Woosehill and eventually continuing to north Wokingham. This would have an all-weather surface suitable for walking, cycling and horses.</p>	

			<p>Phase 1, which runs along the southern boundary of Barkham from Finchampstead to Commonfield Lane has undoubtedly been a success. The 3m wide resin surface functions well, providing an all-weather surface. The greenway is well used, particularly where it is near to the populated part of Finchampstead. Also, it is used by a large number of pupils to access Bohunt School.</p> <p>BPC's priorities would be:</p> <ol style="list-style-type: none"> 1. A greenway linking Sandy Lane (near the Junipers Park) to Woosehill. This would connect with the network of cycleways in Woosehill through to Smiths Walk, effectively creating a cycle route from Barkham to the railway station without the need to use Barkham Road. 2. A greenway linking from Edney's Hill towards Barkham Church and onwards to Arborfield Green. This would have the benefit of providing a route to Bohunt School and could become part of a wider network of amenity routes linking the Barkham Hill part of the parish with Rooks Nest Park and Hazebrouck Meadows, both of which have been provided with good footpaths. It would also create an effective link between the older established parts of the parish around Barkham Hill to the new part in Arborfield Green. <p>The proposed Phase 2 has limited benefit for Barkham residents as it could improve conditions along Coombes Lane. Although it passes through the parish, however, it has the serious disadvantage that it does not connect properly with the parts of Barkham where the majority of people live, notably around Elizabeth Park and Barkham Road.</p> <p>Assuming WBC wishes to progress the Arborfield Cross to North Wokingham route, there are a number of issues that need to be resolved: I think it would be desirable to point out that the sort of surface which would make it attractive as a green route would be likely to break down quickly if used by vehicular traffic;</p> <ol style="list-style-type: none"> a) The section through The Coombes is currently classified as a byway which is open to all traffic. It is being increasingly used by vehicles; some are 4 x 4s, others smaller cars, vans, quad bikes and occasionally a tractor. Often vehicles are getting stuck at a particularly wet part causing a deep hole and ruts. Pedestrians constantly have to use self-created paths on each side of the lane, particularly on the north side. Providing a decent surface will encourage all 	<p>The linking of Sandy lane to Woosehill has been explored, but due to the private landholdings as well as steep level difference, this is not feasible.</p> <p>Future Greenways route connects Barkham to Wokingham. Please refer to the Attached map (<i>Appendix 1</i>)</p> <p>The Greenways are Traffic free routes. Any traffic restrictions on the Byways will be consulted as part</p>
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				<p>types of traffic to use the route. Also, it is likely that the surface used would break down quickly if used by vehicular traffic. Use of the byway by additional traffic is a particularly sensitive point in view of the various unauthorised activities taking place in The Coombes. Therefore, it needs to have gates so that only authorised vehicles are able to use it, thus downgrading it from byway status.</p> <p>b) The crossing at Bearwood Road needs to be improved. It is difficult enough already for pedestrians and horses to cross and is further complicated by vehicles turning in and out of Sandy Lane. At the very least there needs to be improved warning signs and a reduction in the speed limit to 30mph on Bearwood Road at this point. Ideally there should be some kind of warning lights. (There is an example at Lambourn which could offer a prototype.)</p> <p>c) The current proposal is for the greenway to continue along the east side of Bearwood Road and then Highland Av. Bearwood Road is not particularly pleasant for non-motorised users and there is the added problem of a large ditch adjacent to the existing footway. Highland Av has the additional issue of HGVs manoeuvring near the entrance of the A1 scrap yard. A better solution would be a link from Sandy Lane north towards the existing right of way towards Limmerhill. Besides being a pleasanter route, this would be much more useful for Barkham people. This would involve some land acquisition, but there are at least three different options which could be investigated.</p> <p>An alternative to c) would be to make use of a short length of the existing track within the Bearwood Lakes Golf Club from Coombes Lane to Bearwood Road (in the vicinity of Highland Avenue). Obviously raises the question of land ownership so some sort of arrangement with the golf club would be necessary. This alternative route would facilitate a safer crossing point of Bearwood Road, though it would not eliminate the need for improvements at the Coombes Lane-Sandy Lane crossing, as many residents and horse riders would still cross at this point. Refer also to the Arborfield and Barkham Greenways report which is part of the Neighbourhood Plan project.</p> <p>https://www.arbarplan.com/html/documents.html</p>	<p>of the statutory process.</p> <p>Safe crossing will be designed on the basis of the road safety surveys and traffic Audits.</p> <p>The suggested routes will be explored to achieve the best possible and safest route for Greenways.</p>
4	Arborfield & Newland Parish Council	09/05/19	Supports/ Comments	<p>In principle the Greenways Project is a great addition to the current network of footpaths, byways and cycle paths. We very much look forward to seeing it completed, and in the future extended to include other areas.</p> <p>Comments specific to Phase 2:</p>	<p>The California Way Greenways is intended to be a circular route. The</p>

				<p>1) The Greenway offers continuous connection but might it also offer circular routes? If I am out involved in recreational walking, running, cycling etc. I often wish to return to my starting point.</p> <p>2) The Greenway offers great "access to nature" but this often comes with some associated problems such as littering and fly tipping. For example Gravel Pit Hill Lane is regularly used to dump rubbish and is connected to the route through the Coombes. How will this issue be dealt with?</p> <p>3) "Access for all" might involve travel to and from the Greenway route by public transport or private car. Will there be any provision for parking or connecting to the bus network?</p> <p>4) Equestrian use is not currently catered for as part of the Greenway, yet there are many stables, liveries and riders in the Arborfield area. Horses are having to cross and use busy roads at potential danger to themselves and inconvenience to other road users. Is it intended to put Pegasus crossings and dedicated horse tracks in later as part of the network? It would make sense to include them into the planning, perhaps as part of phase 3 if it is not possible to fit this into phase 2.</p> <p>5) Parts of the Coombes are currently under threat of development, some of it illegal. How can this area be best conserved and protected going forward, so that it is available for everyone to enjoy?</p>	<p>other routes will also be explored.</p> <p>The Greenways are traffic free routes so there are no plans to increase parking. But linkages to Public transport will be looked at.</p> <p>Greenways are multi-user routes. The equestrian access on the new routes will be explored and consulted.</p> <p>Wokingham Development control is looking into this matter.</p>
5	The British Horse Society	18/03/19	Support/ Comments	<p>Arborfield and Barkham are rural parishes located to the West of Wokingham town centre. The footpath network in these parishes is extensive and allows walkers to enjoy many circular routes. However, the bridleway network in these parishes is disjointed with horse riders and cyclists having to use busy roads to link bridleways together and create circular rides.</p> <p>The Greenways network is a fantastic initiative and WBC should be praised for their decision to implement this scheme. The Greenways offer a unique opportunity to improve the existing bridleway network and provide a cohesive network for all users to enjoy.</p>	

			<p>Notes on Phase Two Greenway Consultation map showing current and potential usage. Please include horse riders in the Arborfield to Woosehill Greenway as follows:</p> <p>1. Make allowance for continued access along Arborfield Byway 9 and Barkham Byway 1 (red dotted line). The byway survey (appendix I) shows that there is sufficient width for the greenway but not enough width to divide the track to separate the users; motorised and non-motorised. Reclassification of these routes as restricted Byway would be desirable to increase safety for all users, including children on bicycles, disabled and elderly users. Also to protect a new Greenway surface from damage by 4x4's and other traffic that may choose to use it once it's been improved. As a minimum I would suggest that a seasonal TRO to close the Greenways through the Winter months would help protect the route and reduce the maintenance costs. Lorries do use Byway 6 and part of Byway 9 to access Wysipigg Farm, consideration should be given to how safe passage for Greenway users can be achieved here. Perhaps a couple of overtaking places could be put in here so people, children, bicycles and horses have safe places back from the byway to stand while they pass.</p> <p>2. The ford on Arborfield Byway 9 has been crossed safely by horse riders, the ford bottom is a firm gravel base, I'm sure horse riders will be happy to continue doing so. If a bridge is to be placed here instead, please ensure it is a bridleway bridge with sufficient width and closed in sides. The existing foot bridge is not safe, it is very narrow at 85cm, slippery and has steps on and off it so not accessible by disabled users. There is also no lower hand rail for children and the sides are completely open so it would be easy to fall into the water. If the footbridge is replaced with a footpath/cycleway bridge not intended for use by horse riders, to avoid unnecessary injury to horse and/or rider, it is essential that a barrier or signage is placed either side to make sure they don't try to use it.</p> <p>3. Bearwood Road is a fast road, the footway on the East side of the road is very narrow and there appears to be no space to make this wider without filling in the ditch. There is also very limited space to put in a pavement on the West side of it without removing the Rhododendrons. I believe it is the residents wish that through the Neighbourhood plan process, these are protected.</p> <p>Alternative 1 Consider and have discussions with Bearwood Lakes about use of their trackway that runs parallel to Bearwood Road (marked in green), just as far as a crossing</p>	<p>We are detailing out the route and statutory consultations will be carried out for implementing any traffic regulatory orders.</p> <p>In the current proposals, the ford will be replaced with a bridge designed suitably for Greenways.</p> <p>A suitable traffic crossing will be designed on Greenways junction with Bearwood road.</p>
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			<p>point opposite Highlands Avenue - this route is on the ROWIP. If use of this track is not possible, perhaps the Greenway could be put in through the fenced strip of woodland (not dense) that is between Bearwood Road and the Lakes track itself, approx. 30-40m wide. This off road alternative route would be much safer and more enjoyable to use for all users, not just horse riders.</p> <p>Alternative 2 Taking the greenway along the route marked with a turquoise dotted line is another good alternative to using Bearwood Road, please see map above. Crossing Bearwood Road is difficult at the moment and would not be possible for disabled or elderly users. Altering the line of the Greenway to cross at Coombes Lane/Sandy Lane would be possible if an assisted crossing was viable and combined with traffic lights on this junction. Visibility for those crossing is good on the brow of the hill but Coombes Lane and Sandy Lane seem to carry a surprisingly large number of vehicles so safety for all must be considered carefully if the Greenway is to cross here.</p> <p>4. A frightened horse will react very quickly and will not always move in the direction that could be anticipated. Provision of both a pavement and cycleway along Bearwood Road would make the road unsafe for horse riders to use. Horses are stabled in Highlands Avenue itself, it is essential that they have continued use of Bearwood Road. Horses are usually happy for pedestrians to pass them on one side whilst a car passes on the other, but are very frightened by a bicycle and car doing the same. If a cycleway is placed on the road for horse riders and bicycles to share this also presents a potential danger as drivers with a full width roadway to use then have no motive to slow down to pass the horses. Point 2 above is the best solution to keep all users safe.</p> <p>5. Wokingham Footpath 1, marked with a purple dotted line has been used by horse riders and cyclists for a number of decades. Continued use of this is essential to allow horse riders stabled in Barkham Road, Blagrove Lane, Doles Lane and Edneys Hill continued access along Limmerhill Road and through Foxhill to get to the Coombes. This route helps to form a circular route with the shortest routes on busy, main roads. Highlands Avenue and Limmerhill Road are on the list of streets, Wokingham footpath 1 links the two together, it is a fair assumption that</p>	<p>The suggested routes in Alternative 1 & 2 are on private land and is subjected to discussions.</p> <p>Thank you for the valuable feedback, we will take this into account while designing Greenways.</p> <p>The Wokingham Footpath 1 will be a part of Greenway route and based on the consultation feedback received,</p>
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			<p>historically this would have been a thoroughfare, especially as there has been a pub located at the end of Limmerhill Road since the 17th Century! As such it should be given restricted byway status and allow use by horse and cart as well as all other non-motorised users. As part of this application please give Highlands Avenue, footpath 1 and Limmerhill Road restricted byway status or bridleway status as a minimum. Please see footpath survey (appendix i) carried out April 19 which provides evidence that there is more than enough space here to include all users, including horse riders.</p> <p>6. The section of unclassified pathway in Foxhill, marked with an orange dotted line, has been used by horse riders and cyclists for a number of decades (living memory). Please allow continued access along here.</p> <p>7. Horse riders and cyclists have also used the pathways through Foxhill that are marked with pink dotted lines. To acknowledge this use, bridleway status should be given to these pathways. When combined, these routes offer horse riders a short circular route - there is only one circular bridleway route in the Wokingham Borough which is in the Coombes, made up using bridleways 2, 6 and 20. I am aware that a large chunk of the Foxhill land has been advertised for sale as potential SANG. It is currently showing as under offer, if the new bridleways were fenced, the rest of the land could be used as SANG. The nearest example of where Natural England has allowed this is in the Church Crookham - Queen Elizabeth Barracks development, Fleet where the Taylor Wimpy development included a new 2km bridleway through 400acres of SANG, this was opened November 2013.</p> <p>8. Bearwood Road is a straight, fast, busy road with a 50mph limit, it drops to 40mph just before Highlands Avenue. Provision of an assisted crossing on this road would be an advantage for all users. Without assisted crossing elderly and disabled users may not have enough time to cross safely? Should the land at A1 be developed and children use the Greenway to get to the Bohunt school at Arborfield, an assisted crossing would be much safer for them too. Highlands Avenue is a safe distance from the brow of the hill at the Sandy Lane/Coombes Lane so will allow time for drivers to see the crossing and slow accordingly.</p>	<p>the rights will be enhanced to cycling and horse-riding.</p> <p>We are in discussions with the private landowners for Greenways on Highland Avenue.</p>
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			<p>9. The black dotted line follows the half of this Greenway that travels through Woosehill itself, I do not believe horse riders will have any need to use this section as there are no stables in this housing development or in the town centre.</p> <p>10. Suggested locations for seasonal TRO gates are shown on the map above. These would prevent damage being done by 4x4's and other all-terrain vehicles, saving the council money in maintenance costs. This would also increase safety for all users along the majority of the Greenway should restricted byway status not be possible for Arborfield 9 and Barkham 1. Byway 6 aka Gravel Pitt Lane, seems to be the main access used by Wysipigg Farm, if the gates were located as shown above, this would still allow them unrestricted use of this access. If they or other landowners have a right of access along any gated tracks, perhaps they could have keys or codes for the gates. Gravel Pitt Lane is on the Greenways wish list to provide access from the phase two greenway to the River Loddon way. Byway 6 is a wide track, it may be wide enough to allow a fenced greenway to be put along one side of the route to affording safety for its users and allow unhindered access for Wysipigg Farm.</p> <p>If the 'Arborfield to Woosehill' Greenway is put in as above, including one of the off road alternatives, (shown in green or turquoise) it can help solve some of the issues raised above, also provide benefits for the wider community.</p> <ol style="list-style-type: none"> 1. Classifying Barkham footpath 1 as restricted byway or bridleway, along with the other pathways in Foxhill, provides a short circular route for all users including horse riders approaching from Arborfield, Barkham and Wokingham. 2. Provides a safer route for Pony Club Children riding to the Pony Cub field in Coombes Lane, by removing the need for them to use Bearwood Road. 3. If an on-road cycle lane is added to Bearwood Road, inclusion of one of the off road alternatives (shown in green and turquoise) would provide a safer route for horse riders and walkers between Highlands Avenue and Coombes Lane, also protect the Rhododendrons. 4. If land at A1, Highlands Avenue is developed as indicated on the potential development sites map above, this development would benefit from being having the Greenway running very close by. 	<p>Traffic regulatory orders will follow the statutory process and consult stakeholders.</p>
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			<p>The Flexi-pave surface at a 50/50 ratio of stone to rubber, as seen at the WBC trial site between Smiths Walk and Meadow Road, Wokingham, is secure and comfortable to use for horse riding, walking and cycling.</p> <p>If barriers are to be used, the one created at the western entrance at Commonfield Lane/Arborfield Green on the California Way is a great example.</p> <p>Conclusion The increased traffic from an additional 13,000 homes in Wokingham will make riding a horse or a bicycle on the roads significantly more unpleasant and considerably more dangerous than it is currently. The evidence above provides more than enough reason to allow horse rider access along the Arborfield to Woosehill Greenway, as far as the edge of the pathway in Foxhill between houses to the Heron Park playground(as indicated above), fulfilling the need within the neighbourhood.</p> <p>I appreciate that some areas are desirable to retain as pedestrian only, however with segregated paths set aside for horse riders and cyclists they too can be included in public open spaces and country parks. In rural areas, especially those that will see new housing development, improved connectivity via greenways is essential to help everyone access the new facilities, schools etc. not just those buying the new houses. Inclusion of horse riders in these routes will help to provide improved safer links for them to access existing bridleways and byways. It is my opinion that Greenways could help to retain the equestrian businesses within the Borough and potentially enhance economic prospects in the rural parishes, as well as encouraging active travel to work and school. Many equestrian businesses are concerned that without improved links to and between bridleways, horse owners will choose to livery outside the Borough, reducing their business opportunity and risking possible closure. If the routes above are fully inclusive, I believe a great deal more support for your development plans will be gained from these local equestrian businesses and riders/horse owners. Historically Rights of Way were for transport from A to B but as travel has changed so has the use of these off road pathways. Rights of Way are now fulfilling a more recreational, leisure purpose. Whilst connectivity is important, creating multiple links for forming circular routes is now equally as important.</p>	<p>Thank you for the feedback. The Greenways consultation along with the Revised Right of Way Improvement plan (ROWIP) and Local Cycling and Walking Infrastructure Improvement plan (LCWIP) will help planning for the future Greenways route in the borough.</p>
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			<p>Horse riders must be included in promotion of any form of physical, health enhancing activity, it is discriminatory not to include them. Horse riding has similar benefits to both walking and cycling, it appeals to a wide range of people from small children to elderly and those with disabilities. Approximately 200 calories can be burned by someone when either walking or horse riding for approx 1 hour. All forms of exercise are helpful in combating obesity.</p> <p>It is also my opinion that the only way to improve actual and perceived safety for all road users, including vulnerable users, is to make the Greenway network as extensive as possible. Inclusion of ALL non-motorised users in as much of, if not all of these new multi-user routes is essential. Horse riders are the most vulnerable users on the roads today, the accident statistics above confirm this. Moving forward into the future, traffic volume is certain to increase as it has done over the last 30+ years. Increased volume of traffic is also likely to increase waiting times at junctions and for some drivers, potentially increase their sense of urgency to get to their destination. If their tolerance of slower moving cyclists and horse riders is also then decreased, the risk of accidents to all road users will increase.</p> <p>Enhancing the off road network will provide safer off road routes for all. I believe it would be wrong for WBC to put in any new routes, other than on road cycleways in urban areas that exclude horse riders. It would go against your standing in promoting physical activity to combat obesity. Bridleways/Greenway multi user paths are essential to provide equality amongst users. All new bridleways/ multi-user Greenways that are integrated with the existing bridleway network must allow use by horse riders and be given definitive bridleway classification so they can remain in perpetuity.</p> <p>You are in a unique position with the power to provide the 'Arborfield to Woosehill' Greenway as a fully inclusive route for future generations to enjoy. I urge you to reconsider your design and include one of the off road alternatives to Bearwood road and include horse riders use along the full length of it, from Arborfield cross, to the edge of the pathway into the Woosehill housing development by Heron Park playground. Also to give definitive restricted byway or bridleway classification to Highlands Avenue, Wokingham footpath 1 and Limmerhill Road. Lastly, protection for Foxhill for the whole community, giving bridleway classification for the pathways used by horse riders and cyclists as part of the creation of this greenway.</p>	
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6	Local Resident	21/02/19	Support/ comment	The Greenways project sounds promising and I will look out for further updates on the Greenways routes. Also, it seems the only safe route for walking/cycling from Arborfield to Farley hill school is via a footpath which runs from Poperinghe Way off the A327 and joins Castle Hill road. Unfortunately this path is not really suitable for a daily school run. A path like the one running from Commonfield Lane to the FBC centre is what is required. Please let me know what the council's position on this is. Surely I can't be the only person to raise this issue?	Greenways Route F will link to Arborfield via Route J and will also link to the greenways along the proposed Arborfield relief road.
7	Local Resident	06/03/19	Support	All for pathways away from the roads especially if it includes provision for disabled cycle riders.	The Greenways will be designed as inclusive paths.
8	Local Resident	12/03/19	Support	Having looked through the pdf for the proposed Greenway I am fully in support of the proposal for a hard standing surface to be laid on the largely extant tracks and pathways to allow road bike access on a largely car-free route across the town. My suggestions would be to utilise the bridge across the railway behind St Paul's church to avoid cycling on the busy A329.	Subjected to approval from adjacent landowners.
9	Local Resident	06/03/19	Object/ comment	I can't see how a cycle path in Joel Park could be feasible at the top of the hill along Holt lane.	We are currently working on the detailed plans.
10	Local Resident	06/03/19	support	Greenways - Great Idea Cycle routes generally fragmented and don't encourage cycling NDR/SDR provision for cycling disappointing No separate lane Already excessive parking	Proposed Wokingham's Local Cycling and Walking infrastructure Improvement plan (LCWIP) is looking into this.
11	Barkham Parish Council/ Residents	03/04/19	Support/ comment	While discussing various concerns on traffic etc. in the parish, the use of the new greenway was discussed. The residents highlighted the issues with the kissing gates especially with the cycles with child trailers and wheelchair access.	All gates will be replaced with bollards for better accessibility.

12	Local Resident	11/03/19	Support	<p>I am an equestrian and I am encouraged by the commitment made by Wokingham to include riders where possible on your multi user routes, after all as non-motorised users we are as vulnerable on the roads as the other non-motorised users and do contribute significantly to the local economy, spending three to four thousand pounds a year on keeping and looking after our horses. We are desperate to exercise our horses locally, meet friends and be healthy but we take our lives in our hands on the roads these days and need our councils to think of us when thinking about walkers and cyclists.</p> <p>We have shared bridleways and minor roads with walkers, cyclists and cars and any conflict occurs when one user group feels they have rights over another. Calling the routes greenways is an excellent solution as it gives the route no specific status and is truly a shared space. In my experience users can co-exist making these routes good value for public money whilst encouraging more people to be more active more often.</p> <p>I fully support the plans proposed and hope you will involve the equestrian community in any decisions alongside any other advice you seek from walkers and cyclists.</p>	<p>Thank you for your valuable feedback. The Greenways are designed to be multi-user routes. We will try to increase equestrian access wherever possible, subjected to consultations with the adjacent property owners.</p>
13	Local Resident	11/03/19	Support/ comment	<p>Please consider putting bridleways into this new development as you are building on all our green space and the increased housing is making riding on the roads more and more dangerous.</p>	
14	Local Resident	11/03/19	Support/ comment	<p>I applaud the initiative to introduce traffic free routes to the borough, especially if the surface standards are the same as the greenway in the FBC/California Country Park area. Some comments:</p> <ul style="list-style-type: none"> • The new routes must connect with the first phase of the greenway to make it useful, especially for people wishing to travel traffic free from Finchampstead Baptist Church to Wokingham. • If it would be possible to add a spur to connect Finchampstead Baptist Church to the route more directly, that would be of great benefit for encouraging people from Finchampstead to ditch their cars when travelling to avoid traffic; generally it should be quicker to cycle for people to choose a bike over a car. By forcing people to ride via Arborfield, it makes the route quite indirect. Nashville lane, to Doles Lane to Sandy Lane could do this make the route much more direct for Finchampstead residents 	<p>All Greenways routes are linked with each other. Though these will be constructed in phases.</p>

				<ul style="list-style-type: none"> • From California country park experience, please do not introduce gates that force people to dismount, or do not allow wheelchair access. The gate at the end of common field lane is particularly obstructive to a smooth journey. • The London Cycle Design Standards (see page 73), recognised as the highest quality standards in the UK, recommends against the use of all barriers, because of the accessibility issues they create. • The latest cycle design guidance for Highways England (who are responsible for trunk routes, and cycleways associated with them) goes further and requires that barriers, specifically K type barriers as favoured, shall not be used (paragraph 2.3.8). https://www.cyclesheffield.org.uk/2017/01/04/barriers-on-cycle-paths/ • Where possible, the routes should take into account the use of "desire lines" http://www.copenhagenize.com/2015/05/the-desire-lines-of-cyclists-global.html • Please do design junctions sensibly to encourage the use of cycling and give priority to cyclists. A particular example of what not to do can be seen at molly millers lane outside intersurgical (see attached) and most importantly, I note the comment of "generally" traffic free routes. • The routes must be safe enough for an unaccompanied child to ride without fear of traffic. If this is not the case, they will likely not be used practically, or we will continue to see families driving to the routes to use them for leisure at the weekend and not actually use them instead of cars. This is something I regularly see at Finchampstead Baptist Church; families arriving in cars and removing their children's bikes from the boot. • Children and traffic do not mix. 	<p>All gates will be replaced with bollards to safeguard against motor traffic.</p> <p>Wokingham's My Journey Team works with people of all age groups and abilities to encourage cycling and walking.</p>
15	Local Resident	12/03/19	Support/comment	<p>I've been looking at the new greenway proposals. We are in desperate need of a safe cycling route from Finchampstead North / Cali Crossroads to Wokingham centre and station. This has become especially urgent with all the extra traffic coming from the new developments surrounding us. Finchampstead road is gridlocked after 7:15 in the morning. The regeneration seems to have done nothing to make cycling easier. Ditto for the Cricket Green development near the station, or the planned carnival pool update. Can you tell me if there are any safe cycling routes planned (aside from the greenways) by WBC?</p>	<p>Proposed Wokingham's Local Cycling and Walking infrastructure Improvement plan (LCWIP) is looking into this.</p>

16	Local Resident	12/03/19	support	<p>The new Arborfield garrison Greenway has been a great benefit to the area. What would be a great next step is to add more greenways to allow safe routes to rail stations from Arborfield. There is an existing footpath from church lane that links to the unused motorway bridge at Lower Earley. With similar mods / surfacing and a few crossings this could link to existing reading infrastructure.</p> <p>Alternative could be using the A327 verge as a cycle path to link to Shinfield. Also could a way be found to cross coombes woods to get to Winnersh station?</p>	ROWIP and LCWIP consultations will pave way for more walking and cycling routes in the borough.
17	Local Resident	14/03/19	support	<p>I would like to ask you to take in consideration all the horse riders around the area that the Greenways Project is affecting.</p> <p>As a horse rider we cannot afford to lose any more bridleways and are in desperate need of safer routes due to car drivers becoming increasingly impatient and dangerous around horse and riders.</p> <p>We have to use roads more and more to access bridleways that are becoming harder to find as we are never given a thought about, hopefully Wokingham Greenways Project will set a fantastic example by helping us stay safe and lessen the accidents and fatalities that all horse riders are experiencing more and more frequently.</p>	The equestrian access will remain on the bridleways which are being made part of the Greenways.
18	Local Resident	17/03/19	object	<p>We object to it on the grounds of:</p> <ul style="list-style-type: none"> • The project seems to be destroying yet more local green space • It will add unnecessary noise to a quiet residential area. • The Foxhill area has flooding issues and this path network behind our houses would undoubtedly make the situation worse, however permeable the surface is. Just look at what has happened with the widened path leading from the bridge below Arthur Road alongside the Emmbrook towards Falcon Way, a route which is now often unpassable in wet weather. • Destruction of Natural Habitat: Foxhill Woods are a beautiful natural resource that the residents of Wokingham enjoy and which is home to diverse wildlife. Cutting a large swathe of concrete through these woodlands would ruin this environmental habitat forever and destroy the wildlife with this addition of further creeping and un-necessary urbanisation. • Alternative paths for recreation already exist. Why destroy Ancient Woodland for yet more? 	The Greenways Project is a sustainable project increasing accessibility to Green spaces. Most of the routes through Green spaces are on existing path. Stakeholder consultations will be carried out for new routes.

			<ul style="list-style-type: none"> • Light pollution: We are lucky to live in a close which is almost semi-rural in nature with little light pollution. The additional of a light path behind my house would significantly impact the quality of my dwelling at night. • Disturbance of local residents. The woods are already a place where local teenagers congregate, particular in the summer and cause disturbance late into the night. The addition of a path behind many dwellings and on the edge of the woods will only make this worse. • Alternative commuter routes already exist. Good paths exist throughout Woosehill linked to Limmerhill Road, which is mostly traffic free. This in turn leads to footpaths on Barkham Road that connects to Arborfield. There are better routes. Existing hard surfaces already exist through Woosehill linked to an exit at the top of Limmerhill Road where it turns right toward Foxhill. If the woods must be destroyed for the sake of a dubious benefit, this would be a much better route to connect to Highlands Avenue. • Tree Preservation Orders (TPOs): Part of the route will be through woodland with trees that are the subject of Tree Preservation Orders (Woodland from the bottom of Limmerhill Road to Highland Avenue). • Impact on Local Wildlife – Local Wildlife Sites (LSWs): The route of the path through Foxhill is not designated as a Local Wildlife Site. However Foxhill Woods is immediately adjacent to sites which are designated as Local Wildlife Sites. The woodland surrounding St Catherine’s Lodge and extending parallel to Simmon’s Way, on the opposite side of Bearwood Road at Bearwood Lakes, and The Coombes is all land designated as LSWs. Together with other adjoining land, there is a contiguous area of natural woodland inhabited by wildlife including deer, foxes, smaller mammals and a wide range of birdlife. Further, this area forms a wildlife corridor, which extends through to Arborfield and beyond, and which is recognised as being important in preserving a wildlife population. We frequently see deer in the woods, on our front drive at night, or signs of them from deer droppings left on our drive in Chaucer Way. Reducing the size of this contiguous woodland area by placing a substantial development through it would undoubtedly impact the sustainability of the wildlife population. • Thames Basin Heath Special Protection Area Buffer Zone: The path through Foxhill is in the 7km Thames Basin Heath Protection 7km buffer zone. Any 	<p>The routes will be planned without cutting any trees. Formal path network will make areas accessible and reduce anti-social behaviour.</p>
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				<p>proposals for development within the 7km buffer zone must demonstrate that adverse impacts to the Special Protection Area are avoided and mitigated.</p> <ul style="list-style-type: none"> Wokingham District Landscape Character Assessment Foxhill Woods are identified in "Part 2 Wokingham District Landscape Character Assessment" as "The Bearwood Wooded Sand and Gravel Hills (L1)". The area is evaluated (para 16.26) as "...being a landscape of high quality". Para 16.27 identifies the Landscape strategy as follows: "This overall strategy is to conserve and strengthen existing character. The key characteristic to be conserved through ongoing management, is the woodland and the features associated with the historic landscape." I do not see how the development of a substantial path of highly urban character would be consistent with the landscape strategy. Separation of Settlements and Setting of Settlements Foxhill Woods are identified in Wokingham District Council's June 2006 report "Assessment of Gaps and Green Wedges" as being the "Wokingham-Winnersh-Barkham Hill – Sindlesham Gap/Green Wedge (No.5B)" Para 8.3.1 says that "...On the basis of the above survey and analysis, it is considered that the existing gap fulfils its essential function in the physical and visual separation of the settlements and, as such, meets the criteria for retaining the gap designation." Para 8.3.2 says that "... Any built development in the relatively enclosed central and southern parts of the gap would potentially impact on the continuity of the woodland and its removal would potentially open up views between settlements." Again, I do not see how the development of a substantial path of highly urban character would be consistent with the strategy identified in the Assessment of Gaps and Green Wedges. To build such a path would significantly degrade the Gap/Green Wedge, and would be highly detrimental to the physical and visual separation of the settlements they partition. 	
19	Local Resident	17/03/19	Support/ comment	<p>I understand you are seeking views on the proposals for Phase 2 of the Greenways Project. I am a regular user of the Phase 1 project and think this is an excellent facility. I fully support WBC in its drive to provide traffic-free routes into and around Wokingham for walkers and cyclists.</p> <p>I think Phase 2 is an admirable project and look forward to using it in future, probably mostly by cycle. Some particular comments are as follows:</p>	

			<p>It is regrettable that there is no link to central Wokingham or the station, although I suppose the route along Reading Road from Woosehill roundabout could be used but this would be rather indirect. Is there a potential link along Woosehill Lane and Barkham Road?</p> <p>Section 2 – Woosehill to Bearwood Road</p> <p>1. You say the route is pretty much fixed, but would it not be possible to move the route from Highland Avenue to Sandy Lane. This would then allow a more direct link with Doles Lane and be more useful for residents of Barkham and north Finchampstead.</p> <p>2. What is proposed for the crossing of Bearwood Road? I would suggest that a pedestrian crossing with traffic lights would be desirable here for safety reasons.</p> <p>Section 3 – Sandy lane to Arborfield</p> <p>1. This section is entirely along a byway, which at present is subject to antisocial use by four wheel drive vehicles, which churn up the surface in wet weather, as well as presumably some legitimate access by agricultural vehicles. Cars of dog walkers and vans of dog walking businesses also use it. I think it is essential that casual vehicular traffic be banned from this section (with locked gates or barriers to allow for any legitimate vehicular access). Perhaps some provision should be made for parking for dog walkers at the eastern end.</p> <p>As to future phases, I look forward to a traffic-free alternative to Finchampstead Road as a route into Wokingham for residents of Finchampstead! I regularly cycle into Wokingham from north Finchampstead using a route along Nashgrove Lane, Evendons Lane, Blagrove Lane and Oaklands Drive which is relatively quiet, although Evendons Lane is now increasingly busy as something of a rat-run. I wonder if parts of this route could be adopted and improved for a future Greenway phase.</p>	<p>We will explore these routes, subjected to consent from adjacent landowners.</p>
20	Local Resident	17/03/19	<p>Object</p> <p>I am writing to you to inform you why I think there should not be a public footpath through Foxhill Woods. I think this is the wrong thing to do for several reasons. Firstly, it will cause destruction of the habitat and will ruin the lovely feel of the natural environment. I am 12 years old, and have lived here all my life. I enjoy going in my garden backing on to the woods and it being peaceful. That peace will</p>	<p>The Foxhill Woods is already used by local residents and commuters. Greenways will increase</p>

				<p>be destroyed. I also enjoy going for many walks in the wild woods, and it will not be the same after the massive path is built.</p> <p>The new path will deeply affect the residents of Woosehill that back on to the woods, because the noise of the cyclists coming past every day will be very disturbing.</p> <p>On another point, homeowners backing on to the woods already have flooding in their gardens. The path could make this much worse. How would you feel if you were one of those home owners?</p> <p>Finally, I would ask, is this path really necessary? I cannot see where it leads to and from.</p> <p>I am sure you will take all of my points into consideration as part of the consultation.</p>	<p>accessibility in the area.</p> <p>The stakeholders will be consulted before final plans are drawn.</p>
21	Local Resident	18/03/19	comment	<p>Can you please advise me on the above project's impact on Highland Avenue which is a private road?</p> <p>Specifically how do you intend to pass our property where the road narrows considerably and a vast quantity of juggernauts use the road daily? Are there any plans in place to require the A1 company to relocate their entrance?</p>	<p>We are in discussions with the landowners for Greenways access.</p>
22	Local Resident	18/03/19	comment	<p>How will the proposal affects the adjacent properties? How close the route will be to the adjacent properties?</p>	<p>In the current proposal, most of the Greenways is on WBC land. The adjacent property owners will be consulted and informed before commencement of the route.</p>
23	Local Resident	18/03/19	Support/ comment	<p>Barkham Village Residents association has brought the following to my attention and I would like the following comments to be considered.</p> <p>"This would have an all-weather surface suitable for walking, cycling and horses."</p> <p>As a horse rider, a cyclist and dog walker I feel that I am typical of the person that may use this route.</p> <p>Sections 2 and 3 pass though Barkham.</p>	<p>The proposals will be discussed with statutory consultees before a final plan is developed for</p>

				<p>In principle I think this is a great idea and fully support the concept. However there are a number of issues which need to be resolved.</p> <p>a. The section through The Coombes is currently classified as a byway which is open to all traffic. Therefore it needs to have gates so that only authorised vehicles are able to use it. Otherwise providing a decent surface will encourage all sorts of traffic to use the route. This is a particularly sensitive point in view of the various unauthorised activities taking place in The Coombes. Consideration should be given to those wishing to use a horse and carriage. Especially considering the heritage in Arborfield of the working horse.</p> <p>b. The crossing at Bearwood Road needs to be improved. It is difficult enough already for pedestrians and horses to cross and for traffic emerging from Sandy Lane. At the very least there needs to be improved warning signs and a reduction in the speed limit on Bearwood Road at this point to 30mph. Ideally there should be some kind of warning lights. The 30mph zone needs to be extended to beyond the A1 metal recycling turning.</p> <p>c. The current proposal is for the Greenway to continue along the east side of Bearwood Road and then Highland Avenue. Bearwood Road is not particularly pleasant for non-motorised users and Highland Avenue has the additional issue of HGVs manoeuvring near the entrance of the A1 scrap yard. A better solution would be a link from Sandy Lane north towards the existing right of way towards Limmerhill. Besides being a pleasanter route, this would be much more useful for Barkham people. This would involve some land acquisition, but there are at least three different options which could be considered.</p>	Coombes Lane/ Cole Lane.
24	Local resident	18/03/2018	Object/ comment	<p>I am a resident of Woosehill and I have some serious concerns about the proposed Greenway through Foxhill Woods. As I'm sure you are aware the residents of Woosehill have discovered that parts of the woods are up for sale. The vendors are hoping by all accounts to gain planning permission for various developments on this site including houses, hotels and other industrial units which many of us are against. At the moment the council have been non-committal about whether it will grant planning or not and this proposal does nothing to allay my concerns.</p> <p>If future planning is not going to be granted why on earth do we need a Greenway that links one end of Wokingham to the Barkham Road and beyond? Please don't</p>	The proposed Greenway Route is the most feasible traffic free route connecting the new developments in Arborfield, Shinfield and North Wokingham

				<p>tell me it's so people can cycle etc from there into Wokingham as few people live on that part of Barkham Road, Woosehill residents would only go in the other direction and anyone coming from Arborfield would not want to go that way to get to town. In my opinion this is nothing to do with making paths better for dog walkers etc. and just smacks of putting in the groundwork so when any planning application comes before the council you can say that the infrastructure is already there. I would therefore appreciate it if I am wrong about the reasons for this project if you could explain beyond what's written on the website how you justify it from a planning and cost perspective.</p>	to each other and to the existing neighbourhoods.
25	Local resident	19/03/2019	Support/ comment	<p>In principle we think this is a great idea and fully support the concept. However there are a number of issues which need to be resolved.</p> <ol style="list-style-type: none"> 1. The section through The Coombes is currently classified as a byway which is open to all traffic. Therefore it needs to have gates so that only authorised vehicles are able to use it. Otherwise providing a decent surface will encourage all sorts of traffic to use the route. This is a particularly sensitive point in view of the various unauthorised activities taking place in The Coombes. 2. The crossing at Bearwood Road needs to be improved. It is difficult enough already for pedestrians and horses to cross and for traffic emerging from Sandy Lane. At the very least there needs to be improved warning signs and a reduction in the speed limit on Bearwood Road at this point to 30mph. This could be a natural extension of the current 30mph limit that ends at the Barkham Post Office. Ideally there should be some kind of speed alert/ warning lights – or better still active speed cameras. 3. The current proposal is for the Greenway to continue along the east side of Bearwood Road and then Highland Avenue. Bearwood Road is not particularly pleasant for non-motorised users and Highland Avenue has the additional issue of HGVs manoeuvring near the entrance of the A1 scrap yard. the traffic route, all the way along Highland Avenue, from Bearwood Road to the A1 yard entrance, is totally unsuitable for the mixing of significant numbers of Greenway users, (pedestrians, runners, cyclists, wheelchairs, invalid trolleys and horses), with HGVs and non-HGV traffic to & from the yard. Highland Avenue narrows, in several places, to not much more than an HGV's width, (certainly not sufficient width for an HGV and an adjacent Greenway path). 	<p>Any changes to the Byway status or final plans for Greenways through Coombes lane/ Coles lane will go through statutory consultations with the stakeholders.</p> <p>The traffic survey and audits will be carried out to design the traffic junctions on Greenways.</p>

				<p>4. A better solution would be a link from Sandy Lane north towards the existing right of way towards Limmerhill. Besides being a pleasanter route, this would be much more useful for Barkham people. This would involve some land acquisition, but there are at least three different options which could be considered</p> <p>5. Along Sandy Lane and across onto the Coombes there are a number of regular motorcycle scramblers. They drive too fast and without due care to other road users. They should be deterred from using any new Greenway route through the placement of stiles or barriers along the route.</p>	The Greenways routes on private land is subjected to landowners consent.
26	Local resident	20/03/2019	Comment	<p>Thank you for the opportunity to comment; I would like to do this and ask questions about the areas which I know well: Coles Lane and Coombes Lane. The resin-based Greenway already installed from Biggs Lane to Finchampstead Baptist church is about three metres across, well-lit and as far as I can see a success and well-used, as I do myself. It is however along a Bridleway and four-by-four vehicles and tractors do not use it, although on occasion. Electricity company, California Country Park and WBC vehicles do, but not regularly. The width and surface seems fine for cycles, pedestrians and horses. There is a wooden bar at the Commonfield Lane end which acts as a control in any case. However my main questions are these: is the new second Greenway going to be the same width (about 3 metres) as the first? And how will they be positioned along wider RoWs We really do need to have a greater depth of information, please?</p> <p>I am very interested to know what the Greenway plan is for Coombes Lane in particular, a By-way, which has been severely damaged every winter, but particularly this winter; have you seen it in the last month? The same comment would apply to Coles Lane although this seems to be less used by vehicles, probably because of the ford.</p> <p>Coombes Lane is being increasingly used by vehicles. Some are 4 x 4s, others smaller and occasionally a tractor; the former two are getting stuck at a particularly wet part causing a deep hole and ruts.</p> <p>Pedestrians constantly have to use self-created paths on each side of the lane, particularly on the north side. Horse-riders have to take pot-luck. Who is the new Greenway for? Is it for pedestrians and cycles? How will it be positioned? Is to be created at the side of the track used by vehicles, between the ditch nearer the golf-course fence and the edge of the bank beside the track so that vehicles</p>	<p>The Greenways are multi-user routes intended to be 3m wide and have been planned to connect with existing Public Right of ways wherever possible.</p> <p>Greenways are traffic free routes. We are currently detailing the plans through Coombes Lane/ Cole lane and stakeholders</p>

				cannot use it? Or what is the plan? Otherwise I am concerned that the amount of money used to create the greenway along this lane in particular will be completely wasted as it will be rather swiftly ruined by vehicles.	will consulted before these are finalised.
27	Local Resident	20/03/2019	Support/ Comment	<p>I wish to state on the Barkham Development Watch Alert which highlighted the proposed Greenways routes, that not once has it been mentioned accessibility for disabled people. I was disgusted a few years ago when a route that I took on many occasions had been blocked by a beam allowing horses to step over, but not accessible for mothers with pushchairs or people like myself in wheelchairs or mobility scooters through the Coombes. Also there was the steps that magically appeared with no alert sign for anyone in a seated position, who would not have a clear view that the slight gradient had been made into dangerous steps for wheels. This new venture sounds all well and good apart from gates in the Coombes or any other path unless they are staggered gates accessible to all types of pedestrians.</p> <p>I also requested many years ago if a footpath could be provided down Bearwood Road from Highland Avenue to St Catherine's Lodge, (Electricity Buildings) due to the amount of joggers and cyclist using this dangerous route on a regular basis. I myself also would use this route in my buggy to get to Nirvana, Dinton & Sainsburys if there was a path and I know many mother wish also for a path as their children would have access to their schools without having to rely on their parents cars. Instead it was decided to have a cycle, and pedestrian path on Lower Early road. To this day after using that road to get to Reading I can count on one hand how many people I've seen on the path since it was built.</p>	The Greenways are designed to be inclusive path network.
28	Local Resident/ Landowner	17/03/2019	comment	We use all of the byways including Coombes Lane for vehicular access and so we are particularly concerned when you suggest that the Greenways Project provides a traffic free route between the new developments.	Greenways are traffic free routes with no access to motor vehicles.
29	Local Resident	17/03/2019	comment	What type of crossing is being planned for Coombes Lane/ Mole Road junction?	The traffic crossings will be designed on the basis of traffic survey and audits.

30	Local Resident	21/03/2019	comment	<p>As you are probably aware there are a number of residential properties that all need vehicular access from Coombes Lane, please confirm access will be maintained.</p> <p>The track at the end of Coombes Lane all the way to Arborfield is used extensively by 4X4 enthusiasts, which leads to the track being heavily rutted and in some places providing access into the woodlands of the Coombes. Will the new Greenway prevent vehicular access for the majority of the route? Hope you will agree, it is everyone's interest to maintain the present relative tranquillity of this area and to avoid Coombes Lane becoming a short cut to avoid congested roads as did Sandy Lane a number of years ago.</p>	The access to the landowners and residents will be maintained on the Byways. We need to design the surface and access accordingly.
31	Local Resident	22/03/2019	comment	<p>My concern is to how you intend to surface Coombes Lane between Bearwood Road and Alexion House, as your Multi-Use Paths do not appear to be suitable for cars, delivery and service vehicles etc. (although the present condition of the Lane is also hardly suitable for those uses and certainly not for persons with mobility impairments and those in wheelchairs).</p> <p>I would also be interested to know if you are intending to restrict access to motor vehicles going down to the Coombes, as I do not think your Multi-Use Paths will withstand the 4 x 4s and motorcycles which regularly use this route.</p>	The access to the landowners and residents will be maintained on the Byways. We need to design the surface and access accordingly.
32	Local Resident	23/03/2019	Support/comment	<p>I think the provision of traffic-free routes is an excellent idea to encourage more sustainable and healthier ways of travel, and welcome this proposal. However, I have a few concerns which I have outlined below.</p> <p>The plan proposes that the greenway will be routed through Coombes Lane, then along the east side of Bearwood Road, then onto Highland Avenue. Given the volume and speed of motor traffic on Bearwood Road (it being a main route from Barkham/Wokingham to Earley and Winnersh) I would be strongly in favour of a lowered speed limit of 30mph, and a formal pedestrian/cycle crossing with traffic lights.</p> <p>I would also very much prefer a route which avoids following Bearwood Road as it is unpleasant with the volume of traffic, and the existing roadside path is very narrow and difficult to widen due to trees. Large HGV vehicles associated with the entrance of the A1 scrap yard along Highlands Avenue could also be a danger to pedestrians and cyclists. I suggest a much nicer route of Sandy Lane to avoid both of these. There are a number of options to link up from Sandy Lane to the</p>	<p>The traffic crossings will be designed on the basis of traffic survey and audits.</p> <p>We are in discussions with the landowners and it is subjected to consent.</p> <p>The Byways will be closed to through traffic only</p>

				<p>current suggested route at Limmerhill. The Coombes section is classified as a byway at present, and is open to all traffic. If a high-quality hard surface is laid, this could encourage an increase in traffic along the route, which will cause safety problems with pedestrians and cyclists. I would be in favour of either revoking motor traffic completely along this route or installing gates to ensure only authorised and necessary traffic can use it (e.g. farm and Thames Water maintenance vehicles).</p> <p>I would also question how this would fully link up with the new Arborfield Green development. At the moment, path users would need to follow the extremely busy A327 along Eversley road to the Arborfield Cross roundabout before joining Coombes Lane, or have to follow Langley Common road to School Road (B3349). There are footpaths along both these routes, but these are very narrow in places and not suitable for cyclists, particularly the section between the Rickman Close roundabout and the Arborfield Legion. Is there any plan to improve (e.g. widen, reduce speed limit to 30mph) these access routes to enable the new Arborfield Green community to easily access the new greenway route?</p> <p>I would also be interested to know whether there are any plans in place to combine the new foot/cycle path following the Arborfield Cross Relief road with a foot/cycle path alongside the A327 into Shinfield. If not this is a major omission for encouraging walking/cycling routes to/from Shinfield and Reading, given the volume of traffic and danger to pedestrians and cyclists on this major road. Thank you for the opportunity to comment on the Greenways proposal. I hope my feedback will prove useful.</p>	<p>giving access to landowners. This will go through statutory consultation process before any changes are made.</p> <p>Arborfield Relief road has a Greenways which will be connected the proposed Greenways routes.</p>
33	Local Resident	24/03/2019	Support / comment	<p>Please can we have access for horse riders too in the plans as we are losing all our safe places to ride due to all the houses being built?</p>	
34	Local Horse owner and rider.	27/03/2019	Support / comment	<p>I have heard of the consultation for the phase 2 of the Green ways project, and would like to request that horse riders are given access to this route. I ride in the Wokingham area as that is where I keep my horse, but the routes we can access are becoming detached and sparse.</p> <p>The Greenways project has been progressive and innovative in their approach to make access, enjoyment of the outdoors and safety a priority for many users. There is a lot here that other councils can learn from and Wokingham can be proud of.</p>	<p>Existing Equestrian access will remain and we will try to provide access on Greenways subjected to consultations with</p>

				<p>Please if I can request the consideration of the following:-</p> <ol style="list-style-type: none"> 1. Continued access along Arborfield Byway 9/Barkham 1 and through Foxhill on Wokingham Footpath 1, either on the new greenway surface or on a separately fenced section of the route. Footpath 1 and other paths in Foxhill have been used by horse riders for more than 30 years. 2. Continued access from Wokingham Footpath 1 onto Limmerhill Road. This is especially important for those who live in Blagrove Lane, Doles Lane and Edney's Hill. 3. A cycleway along the edge of Bearwood Road, between Coombes Lane and Highlands Avenue would be not be safe for horse riders to use as it would allow traffic to pass too fast, also potentially cyclists and cars to pass a horse simultaneously. Please negotiate with the landowner for use of the track which runs parallel to Bearwood Road from Coombes Lane to provide a safer, off road route. Please provide safe crossing for all users at this point. 4. Please re classify Byways 9 and 1 as restricted Byway to make the Greenway safer for all non-motorised traffic. Especially the most vulnerable; children, elderly, disabled, cyclists and horse riders. If these byways are to remain as such, please make sure the Greenway itself is safe to use and not accessible by fast moving all-terrain vehicles and lorries. 5. Please recognise the historic use of Footpath 1 by horse riders and cyclists and re classify as bridleway as part of the delivery of this Greenway. 6. Please ensure safe crossing of the ford for horse riders is retained. If this is not possible and all users will use a bridge, please make sure the bridge used has a non-slip surface and is safe for use by horse riders. 	adjacent landowners.
35	Local Resident	27/03/2019	comment	Maps which also show the existing cycle routes close to the proposed Greenway route that would be most useful.	Proposed Greenways Route Map attached in Appendix 1.
36	Local Resident	27/03/2019	support	<p>I am writing to voice my support for the Greenways Project to include horse riders on these multi user paths.</p> <p>We take our life in our hands when we ride on the roads. This is an incredibly busy area and as the number of vehicles on the road is set to increase exponentially due to the amount of development in Wokingham, it will only become worse.</p>	

				This is an incredibly popular area for horse riding which brings a lot of money and jobs to the local community and it is vital we do not lose this link to the rural nature of the region. Because riding horses on the roads is so dangerous, you don't see enough riders to realise we are out there and I don't think Wokingham Council is aware quite how many local people own and ride horses. We are the invisible sport. I look forward to your support of the local riders.	
37	Local Resident	27/03/2019	comment	I am a resident of Shinfield parish and have a very active interest in the housing development of our parish and how it connects with Wokingham. WDC are focusing a lot of development upon our parish but there appears no improvement on how we and the new residents will connect to Wokingham. Can a phase 3 proposal be presented if it doesn't already exist? How can I help get this on WDCs plans and agenda?	Proposed Greenways Route Map attached in Appendix 1.
38	Local Resident	27/03/2019	comment	With regards to the new section of this project, will the design remain the same as given in the previous drawings? This gives 3m wide path with a 1m edge etc. Will this same design fit as it passes through the Woosehill Estate or are you going to make reductions in the width of said design to suit some tight spots adjacent to houses and trees? In saying this, the very rough plan shows the route following the paths that are already in situ, or is this not correct?	The Greenways are designed to be 3m wide inclusive path network. We have included the existing path network wherever possible.
39	Local Resident	29/03/2019	Support / comment	Have been reading the next phase of the greenway plans and was shocked to see there was no provision for equestrian access? I am wondering why this is the case and the equestrian communities needs are being ignored? Riding horses on the roads is extremely dangerous and has been getting more so year on year as the numbers of cars in use increases. The area in which we live has a very high proportion of horse riders and therefore making more safe off-road routes for them should be a priority. This greenways project is the perfect opportunity to make more safe shared off-road tracks for all, including equestrians please, please take this into account.	Existing Equestrian access will remain and we will try to provide access on Greenways subjected to consultations with adjacent landowners.

40	Local Resident	30/03/2019	Support / comment	<p>How does the Coombes Lane section in Arborfield link to the Arborfield Green/FBC section of the Greenway? How are you intending that walkers/riders will get from Commonfield Lane to Coombes Lane?</p> <p>I would like to lodge the following response to the greenways consultation: It is an excellent idea to convert the proposed route through the Coombes to a greenway. However, this is currently a byway which means it can be used by motorised vehicles. If it stays as a byway and has the proposed hard surface, it will turn into a road, and then will be unusable by walkers and riders. It is already terrifying when cars tear up there when you are walking. This is an ideal opportunity to remove the vehicular access.</p> <p>I can't see the maps in detail that you have published as they appear to be quite low resolution, but it seems the planned route for the byway is up from the Arborfield Cross roundabout. There is a ford there which is an important part of the Arborfield natural landscape. How would this be tackled?</p> <p>The documentation all says that it is linking the greenways to the new Arborfield development - it does NOT. It links to Arborfield village, then there is a huge gap which has no bike or horse access to the next greenway. This must be addressed. It would probably make more sense to run the Coombes greenway down to School Road (rather than to the Cross) then across the fields to the new development. This must be agreed BEFORE this new greenway plan is signed off. Can you please let me know that this has been included in the consultation response?</p>	The California Way Greenways will be a circular route connecting to Arborfield development via common field lane as well as via Nine Mile Ride.
41	Local Resident	31/03/2019	Support/ comment	<p>Overall we full support the idea to the Greenways project and joining up the different parts of Wokingham is a good idea.</p> <p>Backing onto the woods at Fox Hill, I am happy with the routing as long as it doesn't come any nearer to the back of the properties on Dorset way than the existing well-trodden path.</p> <p>I think it is important that the section through the woods at Fox Hill is not lit.</p>	There will be a green buffer zone between Greenways and existing houses. This is still under planning and will be consulted.
42	Local Resident	31/03/2019	Support/ comment	<p>There are a number of horse riders, including myself, that regularly use the same routes noted in Phase 2, especially parts 2 & 3, and it is essential that we continue to have access to these routes. If the proposed plans effectively eliminate</p>	Existing Equestrian access will remain and we will try to provide

				<p>equestrian access to these routes, we will have lost our ability to ride safely off-road.</p> <p>These are the only routes we currently have that do not require us to ride on the main road.</p> <p>Most of the roads near where I ride are A roads with 50 mph speed limits with some blind curves. It is incredibly dangerous to have to ride on these roads. I have had a few near misses and I know a rider who suffered back injuries when her horse spooked on one of these A roads.</p> <p>For horse riders in the Arborfield and Wokingham area, we have lost so many places to ride as the area has been developed over the years. This is literally our last safe set of bridleways so it is a matter of personal safety that our current access isn't restricted and equestrian use is fully added to the planned greenways.</p>	<p>access on Greenways subjected to consultations with adjacent landowners.</p>
43	Local Resident	01/04/2019	Support / comment	<p>I write in reference to the Greenways Development. I am a horse owner in Barkham and regularly use The Coombes and Foxhills for riding. I have to use Bearwood Road which is becoming very dangerous with the increased volumes of traffic – most people are considerate but around 1 in 10 cars passes far too fast and / or too close and I do believe that unfortunately there is an accident waiting to happen in this area. I would very much welcome more off-road access to our local bridleways. I particular please can I request due consideration be given to the following which will help to ensure increased safety for all road users and particularly vulnerable horses and their riders in our very busy environment:</p> <p>Please change the usage of the footpath at the end of Highlands Avenue into Foxhills from footpath status to bridleway. This will allow the bridleways and byways behind St Catherines to be accessible legally without having to ride down the length of Bearwood Road. I understand that this is called Barkham 1.</p> <p>Please provide a safe path for horses along the section of Bearwood Road from the Coombes Lane entrance to Highlands Avenue, if possible a track on the other side of the hedges to allow riders to not have to use the road at all. A cycleway unfortunately will not work well for horses; I have found from experience that taking a horse on such tracks means that cars pass far too fast as they assume that the horse is safely out of the way. Also if a bike is on the track at the same time there is a space issue! A safe crossing over to Highlands Avenue would be appreciated; perhaps a horse crossing.</p>	<p>The Greenways from Foxhill to Coombes Lane via Highland Avenue/ Bearwood road will have equestrian access subjected to consultation with existing landowners.</p>

				<p>Please provide continued access from Wokingham Footpath 1 onto Limmerhill Road, and recognise the historic use of the same by riders and cyclists and reclassify it as a bridleway. It's the best and safest way to get to the pub on a horse from Barkham.</p> <p>Thank you very much for your time to read this email and I look forwards to seeing the Greenways development take shape to ensure our roads are safer for horses, cyclists, pedestrians and car drivers alike.</p>	
44	Local Resident	01/04/2019	Support / comment	<p>Thank you for the opportunity to take part in the consultation on the Greenways. I appreciated your time on Saturday afternoon to talk through some of my questions.</p> <p>I broadly support the Greenways Project and making natural areas more accessible for the Wokingham community.</p> <p>My areas of consideration are focused on Fox Hill and the Combes which I use frequently for walking and running with friends and family.</p> <p>In Fox Hill I would like the following to be taken into consideration:</p> <ul style="list-style-type: none"> - To maintain the natural feel of the woodland, the visual amenity, and minimise the impact on the habitat and species - A path that is as narrow as possible. The 1.5m width that you mentioned would be appropriate for the area - A meandering path rather a straight path - Minimising the trees that are removed, preferably none <p>In the Combes I appreciate that access needs to be maintained for land, but would prefer that the use of a tarmac road be minimised as much as possible to maintain the visual amenity and the walking surface. I would use that track less if it was tarmac as it would not be as pleasant to walk or run on, and generally not as visually attractive to walk on. We currently walk on that track with family, friends, and with our local Ramblers group.</p>	This part of the route is still under planning and will be consulted.
45	Local Resident	04/04/2019	Support / comment	<p>I have a couple of comments to take into account regarding the greenways project.</p> <p>1. Please can you make the route family friendly? By that I mean so that families who want to cycle can. At the moment phase 1 of the greenway does not allow for an adult to tow either a bike trailer for a baby or toddler or a second bike with a young child attached to a parent's bike because of the barrier just before</p>	The Greenways are inclusive, all weather path network providing access to all age groups and ability.

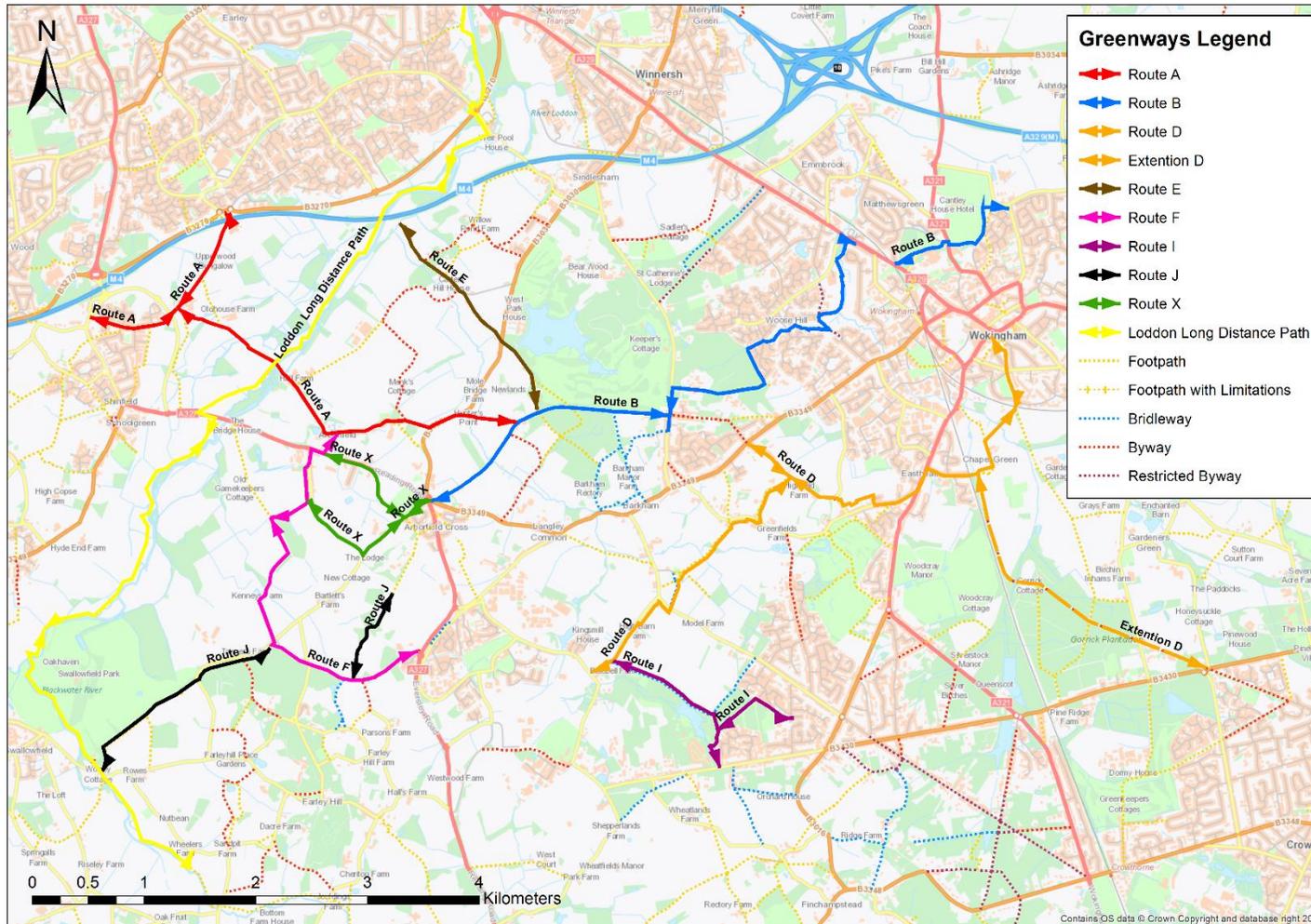
				<p>Commonfield Lane. The swing gate is not wide enough to get an adult bike with a trailer through and it is dangerous to lift a bike with a trailer or bike seat over the metal barrier. It is simply not convenient to get your child out of the trailer / bike seat for them to wander off while you detach the trailer and lift the whole thing over the metal barrier. If you really want to encourage families to cycle for the nursery / school run please make the route accessible to all.</p> <p>2. Are there any plans to link South Wokingham to the Finchampstead and on to Arborfield? A cycle path is desperately needed down the Finchampstead Road from the Tesco roundabout to the Ford garage especially with a Primary School just off of Evendons Lane and to allow the secondary aged children in the area to cycle to school as there is no secondary school provision there. Both primary school children and secondary school children cycle on the pavement along that stretch of road because the road is too fast and dangerous for children to cycle on. The path is far wider than the shared cycle path / footpath on the Finchampstead Rd from the Ford Garage to Finchampstead so it should be feasible.</p>	<p>The gates will be replaced with bollards.</p> <p>The South Wokingham Development will be linked with Greenway routes. Please see proposed route Map (Appendix 1).</p>
46	Friends of Foxhill	05/04/2019	comment	<p>We met at the information session at Woosehill Community Centre last Saturday. I think you mentioned meeting and walking the route through the woods with some of us? Please let me know if this would be possible - hopefully before the closing date for comments.</p>	<p>We will contact you to arrange a meeting.</p>
47	Local Resident	06/04/2019	Support / comment	<p>I have been interested in the greenway surfaces since the original test strip was laid and have contributed to Rights of Way issues for many years. I recognize you have to deal with the conflicting interests of different types of users and have to do that within budget. Please consider these comments:</p> <p>1. The section from Arborfield to A329 in particular is in an area with a high number of horse riders who have no wish to ride on roads to the annoyance of motorists and to significant danger to themselves. All sections of the route should be available to equestrians. Equestrians actually are a safer mix with pedestrians than cyclists with pedestrians (my experience is that the majority of cyclists are totally irresponsible, zooming silently past startled pedestrians with no warning) whereas equestrians can be heard and are almost all responsible when in the vicinity of pedestrians.</p>	<p>Existing Equestrian access will remain and we will try to provide access on Greenways subjected to consultations with adjacent landowners.</p>

				<p>2. Every effort should be made to purchase/rent land alongside sections where your plan indicates there will be need to travel on a road.</p> <p>3. On this project and generally throughout the Borough, where existing rights of way currently classified as footpaths are relatively wide, these should be re-classified as bridleways or restricted byways. This will cost very little, other than time, and will formalise their existing use by equestrians and cyclists. Greenway Phase 1 is an example, as I understand it, where an opportunity to get equestrians off the busy Nine Mile Ride area roads has been missed.</p>	
48	Local Resident	11/04/2019	Object	<p>It would not be appropriate to run the cycle route through Linnet Walk, currently the bike route through Smiths Walk causes many major issues with bikes going too fast amongst pedestrians, and the bikes coming from Woosehill use the designated walkways to get to Smiths Walk, the accidents and issues this already causes makes this too dangerous for Linnet Walk to become a major cycle route, and the same applies for the footpath along the side of Heron Park. Children here run onto the footpath, and already have issues with cyclists heading for the Spine Road or Smiths Walk and travelling too fast.</p> <p>The green space adjacent to Linnet Walk was designated in 1973 as the recreation area for Woosehill and the Birds estate, and for the Planets estate for the other side of the Linnet Walk green space.</p> <p>I believe it cannot be used as a through cycle path as it has already been designated as recreational space, as has the space adjacent to the Willow Trees in the picnic area on Smiths Walk.</p> <p>It would be appropriate to re-route this Green Route from Smiths Walk down through Jupiter way – clearly away from a walkway/footpath that has been in use by children and pedestrians for over 33 years. I do not believe you can change the designation.</p>	We have taken the feedback and will consult you with an updated route plan.
49	Local Resident	15/04/2019	comments	<p>I live on School Road in Arborfield and access my garage and drive from Coles lane, if the greenway is built how will my access be affected?</p> <p>There is a tree felling business that also use Coles lane, how will they be impacted, will the surface be sufficient for their vehicles?</p> <p>Will the ford have a wider bridge? We do get the odd off road vehicle down the lane which is not an issue but if cars can go over the ford it may increase the traffic which is a concern?</p>	The Greenways are traffic free routes but the access to residents and landowners will remain. Byways

				<p>Will the route be lit? If so light pollution would be an issue.</p> <p>The information states that horses are not be catered for at this stage but many riders use Coles Lane and I think this should be taken into consideration.</p>	<p>on Combes lane/Coles lane will be designed accordingly.</p>
50	Local Resident	15/04/2019	object	<p>Thank you for your time the other day, explaining the new Greenway project through Woosehill. As discussed, our house is 21 Chaucer Way, the garden and side aspect of which, backs onto the path leading from Dorset Way into the woods.</p> <p>We are opposed to the Greenway going through the woods unless the following can be addressed:</p> <ol style="list-style-type: none"> 1. Security: There will be an increased risk in crime as the smooth path will allow quick access between main roads. The reason why we chose to live in Chaucer Way is because it is a cul-de-sac and less likely to have opportunistic crime taking place. The current path does not lend itself for a getaway through the woods for a burglar or thief - the proposed new path certainly will. 2. Noise Levels: The area is lovely and quiet. The increased foot fall including skaters and potentially moped riders will become a noise nuisance. We enjoy our garden, but with a constant footfall of people walking past, close to our fence will detract from our enjoyment. We are also very concerned that the path will be used by anti-social moped / motor cross riders, particularly at night. The area is very quiet and would be easily disturbed by mopeds and motor cross bikes, riding up and down the pathways - especially as this path will be used as a cut through from Bearwood Road, into Woosehill and beyond. 3. Privacy: The greenway users would be able to see directly into the windows of our house, which could also increase the risk of crime. The greater the footfall, the more people looking into our bedroom window. We shouldn't have to have our curtains drawn closed due to the number of people looking in. 4. Flooding: Our garden and patio has been flooded due to surface water runoff from the path and ground around the path. We appreciate that the new path will be permeable and all measures will be undertaken to reduce any effect of flooding. The path and grounds surrounding the path should be reduced in height, which will also help with the privacy of people looking over the fence and also any surface water remains on the council owned land and not directed into people's gardens. 	<p>Foxhill is a Public open space accessible to all users. We understand your concern and need a further discussion with you.</p> <p>The Greenways are traffic free routes and bollards will be in place to restrict any motorised users.</p> <p>The Greenways has permeable surface. The path network through flooding zones will be designed keeping historic flooding data.</p>

				<p>If the above concerns could be addressed, then we would be supportive of the project. We discussed the council planting high hedges along the fence line which could address the majority of the points above.</p> <p>However, we have not seen a satisfactory solution to help prevent anti-social behaviour, particularly with the risk of mopeds and motor cross riders using the path at night.</p>	<p>Green buffer zone will be designed between houses and Greenways but this is subjected to discussion as it maintenance cost implications.</p>
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Appendix 1: Greenways Route Map across the borough including River Loddon Long distance path.



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WOKINGHAM
BOROUGH COUNCIL

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Wokingham Borough Council

Greenways Project

Greenways Route B - Section 1 Consultation Report

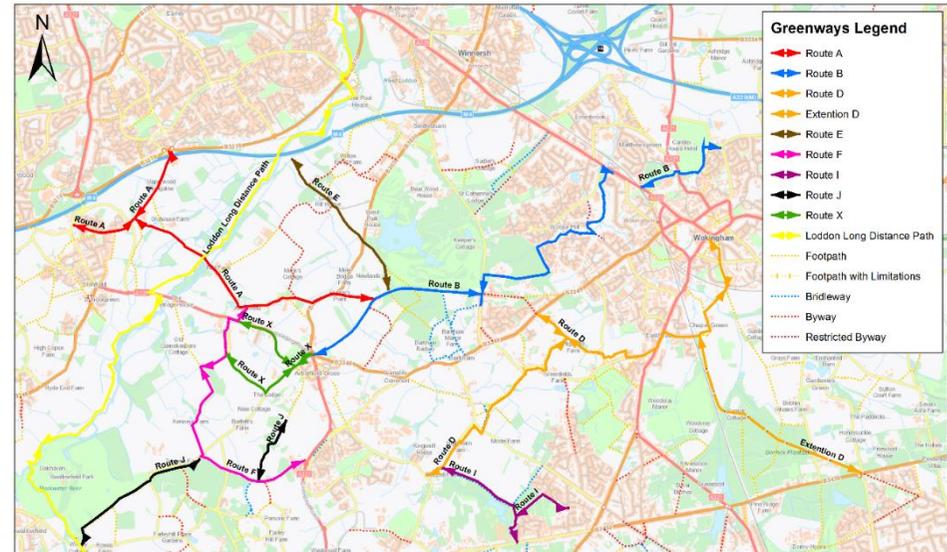
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Appendix 4: Full responses received during the public consultation	

Introduction and Background

The Wokingham Greenways project is aimed at linking the major developments within the borough with each other and also to the existing communities and places of interest/employment. Greenways will provide non-car accessible commuting journeys to work and school as well as new leisure opportunities to get out into the countryside and greenspaces.

The project will link with and align to additional greenways and paths proposed to improve the non-vehicular connectivity within the Borough and to adjoining boroughs. This internal network of Greenways will utilise and extend beyond the public existing rights of way and highways network to public green space and point of interests.

The Greenways project also includes the River Loddon Long Distance Path (LDP) that links the Thames Valley Path in the north of the borough in Wargrave to the Blackwater Valley Path in the South of the borough in Swallowfield.



Appendix 1 shows the Greenways Route Map across the borough including River Loddon Long distance path.

Strategic Context

The completion of the Greenways project is included in or links to the following corporate plans and policies:

- Rights of Way Improvement Plan 2020-2030
 - Statement of Action 2.1(a): Develop and implement the Greenways network
 - Appendix 5: Greenways Strategy and Implementation Plan.
- WBC Core Strategy
 - Policies CP10, CP18-21
 - Appendix 7
- Local Transport Plan 2011-2026
 - Active Travel goals Policy AT1, AT2 & AT3
- Joint Strategic Needs Assessment 2018-2021
 - Aim 1: Promoting more active travel
- Draft Local Plan Update 2020
 - Policies C8, SS6 and SS12

Greenways Route B

Wokingham Borough Council is progressing with phase 2 of its greenways project, which is to implement Greenways Route B. The project will create a network of traffic free multi-user routes which will connect the new major developments at Arborfield Garrison (Arborfield Green) and Shinfield Parish to Wokingham and the new major developments in North Wokingham. The Greenways Project will add to the borough's existing sustainable route network to create an alternative way to get around the borough.

The route has been separated into 3 sub-sections:

1. Cantley Park to Woosehill, through Matthewsgreen
2. Woosehill Meadows to Bearwood Road, through Foxhill
3. Bearwood Road to Arborfield Cross, through Coombes Lane and Coles Lane.

A map detailing the proposed line of Route B is included as Appendix 2 to this report.

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This report summarises the responses to the localised public consultation conducted for Section 1 of the route. The full responses to the consultation have been included in Appendix 4 to this report.

Consultation and publicity undertaken

First phase (March – May 2019)

A six week public consultation for the whole of Route B was undertaken from 06 March 2019 to 10 May 2019. Letters were posted to the adjacent properties owners and ward members, Parish/Town councils and Greenways user groups were notified to invite feedback. This was followed by attendance at community forums, Drop-in sessions at various locations in the catchment area of the Greenways Phase 2, inviting comments through Wokingham Council's website, Social media channels and press release.

The consultation received over 300 visitors to the various drop-in sessions and community forums that were held. The consultation responses were 64% in support, 12% objection and 24% neither supporting nor objecting.

Second phase (July – Sept 2020)

Following on from the initial public consultation, an additional localised public consultation was undertaken for Section 1.

94 A summary of how the consultation and publicity was undertaken is provided below:

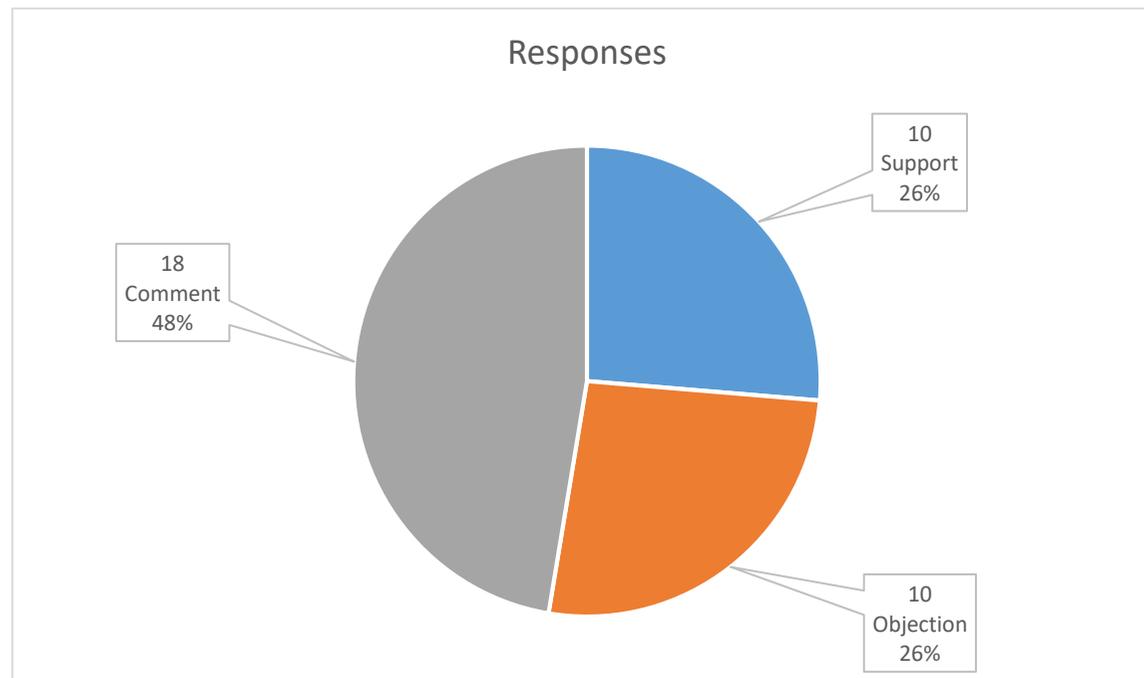
Letter drop/ Emails: Letters were sent to the Landowners and people with properties located next to Section 1 to make them aware of the project and seek their feedback and comments. Emails were sent to notify the ward members, Wokingham Town Council, known user groups and interest groups, the Mid & West Berks Local Access Forum and other key stakeholders.

Online Information on Website and Social Media: The consultation was advertised and the Greenways Route Map and Consultation document were made available to view on the Council's website as well as shared on the social media.

News Release: A news release advertising the consultation was posted on the Council's website and social media pages and was sent to the local media.

Summary of Consultation Responses

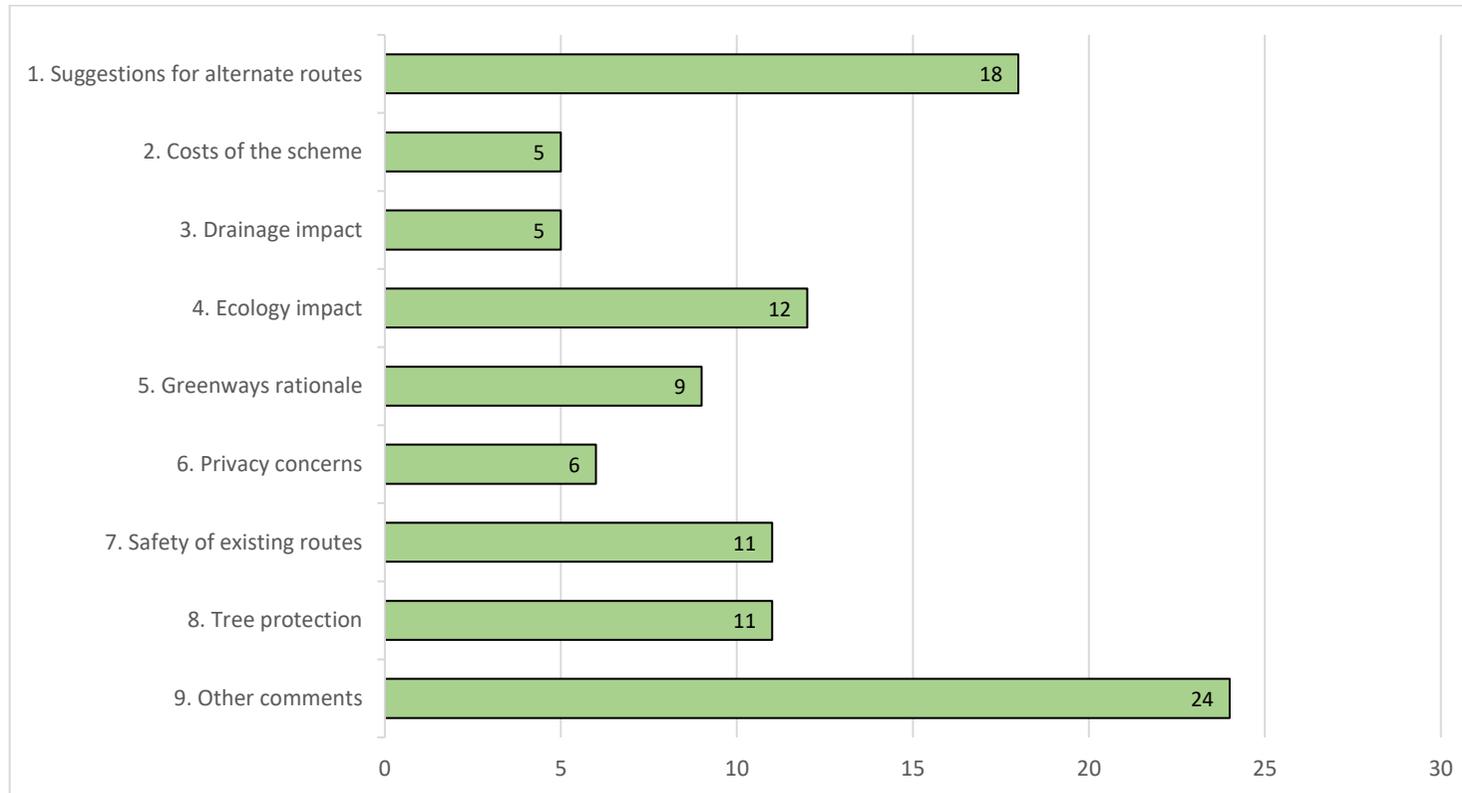
In total, 38 formal responses were received during the consultation, as shown in the graph below. In addition to this a petition was received from the residents of Martins Drive objecting to the proposed route of the Greenway. This petition had approximately 39 signatures but did not contain address details. Some of the signatures included in the petition have also supplied formal responses to the consultation.



The responses are summarised below. The responses have been categorised into various themes as detailed below. An analysis of the feedback received, and any subsequent changes to the final plan are detailed under each heading.

Key Themes from the feedback received:

The comments and representations fell into 9 broad categories



Theme 1: Suggestions for alternate routes

Feedback from the consultation contained a number of suggestions for alternate routes of the Greenway. These suggestions, along with the Councils' responses, are detailed in the table below.

Suggestion	Council response
Use the existing multi-user path that has been constructed along Toutley Road that links to Emmbrook Road	Whilst the path alongside Toutley Road is a good on-road cycling route, the aims of the Greenway is to create an off-road route as much as possible. The suggested option of using the route along Toutley Road goes significantly in the opposite direction that we are seeking to link between the Strategic Development Locations and would require a significant amount of use on busy roads.
Connect into Cantley Park via a new entrance opposite Eldridge Avenue, with a path leading up to the park's amenities.	At the present time there is no plans to connect the Greenways through to the north of Cantley Park to be opposite Eldridge Avenue, the aim is to utilise the existing path network, however the request for additional access to the site from this location will be considered as an improvement project separate to the Greenways scheme.
Suggestion to modify the route of the Greenway at Arborfield to use Biggs Lane in Arborfield Green, down Langley Common Road to the new Barkham Bridge, and then into the Coombs. Alternatively cutting through a small portion of farmland to utilise the Wood Lane Entrance into the Coombs	This suggestion relates to Section 3 of the Greenway. The suggestion is noted, we will take this into account for the design at Section 3.
Suggestion for an East/West route between Twyford and Woodley, or between Charvil/Twyford Station	The Loddon Long Distance Path project is a sister project to link Wargrave to Swallowfield through Twyford, Charvil, Woodley and Winnersh. This project has commenced during 2020 and is being actively pursued in conjunction with the Greenways project.
Suggestion for a direct link using Holt Lane, St Pauls Churchyard, the existing pedestrian rail bridge and Murray Road rather than the longer route via Woosehill.	One of the main strategic objectives for Greenways is to create off-road routes, avoiding the need to mix with vehicular traffic as much as possible. The route as described would be require Greenway users to travel along roads rather than off-road routes.

<p>Suggestion to simply add signage for the area along Jubilee Avenue, Clifton Road and Brook Close rather than creating a new route.</p>	<p>The aim for the section of path adjacent to Jubilee Avenue is to avoid the existing traffic issues that cyclists and pedestrians need to deal with along Jubilee Avenue, which requires the negotiation of two roundabouts amongst significant traffic moving through to Holt Lane; cyclists also need to pass a number of parked vehicles along this section of Jubilee Avenue from Milton Road to the entrance of Martins Drive which means spending a significant amount of time cycling in the centre of the road.</p> <p>Whilst it is a relatively small section of new path, it allows users to avoid the particularly acute issues at Jubilee Avenue. Once this section has been passed the remainder of Clifton Road and Brook Close is a quieter residential section of road.</p>
<p>The end of the signage of the route stops at a major road junction between Bell Foundry Lane, Wiltshire Road and Warren House Road. I would suggest you connect the signage down the new path along the edge of Cantley Park that runs parallel with Bell Foundry Lane and into the Eldridge Park SANG created recently. The North Wokingham SANGs should (hopefully) join up when they are all complete, so it would be an obvious continuation to the greenway for cyclists.</p>	<p>The potential to connect into the new cycleway along Bell Foundry Road has been investigated and the route of the Greenway has been adjusted to meet with this existing cycleway and the section that meets with Wiltshire Road will be removed.</p>
<p>Suggestion to widen the footpath that runs along Milton Road towards the town centre</p>	<p>The widening of the existing footway to join into the Town Centre is outside the scope of the Greenways project.</p>
<p>Suggestion to resurface the pavements on the Joel Park estate instead of creating the Greenway</p>	<p>The proposal is to create a new section of path through land north of Jubilee Avenue, rather than create a route through Joel Park. The existing footpaths are for pedestrian use only, whereas the Greenway is intended to be a multi-user route.</p>
<p>Make Holt Lane a one way road (going North from the Reading Road).</p>	<p>The use of Holt Lane is not considered to be appropriate for a multi-user route, as it would require users to mix with significant vehicular traffic.</p>

<p>Widen the existing pavement on Jubilee Avenue using the adjacent verge.</p>	<p>The widening of the along Jubilee Avenue was considered but it not the preferred option. The creation of new routes within Joel Park was refused by the landowner (detailed below), and a widening of the path adjacent to Joel Park would require using extra land within the park itself, and through environmentally sensitive parts of the park.</p> <p>On the northern side, there is some room, however there is significant level differences between the existing footway and the adjacent land. There is a risk that widening the footway at this point would impact on the root system of the trees.</p> <p>In addition to this, the creation of on-road shared cycle tracks / footways runs against the Greenways rationale, which aims to create, as far as possible, easy to use off-road multi-user routes.</p>
<p>Suggestions for a number of routes through Joel Park:</p> <ul style="list-style-type: none"> • Run the Greenway through Joel Park parallel to the existing pavement • Create the Greenway alongside the lower edge of Joel Park running between the existing avenue of trees that run parallel with Jubilee Avenue 	<p>A number of potential routes through Joel Park were discussed with Wokingham Town Council, who are the landowners of Joel Park. The Town Council considered the proposals but refused permission for any routes through Joel Park.</p>

Theme 2: Costs of the scheme

Feedback from the consultation raised concerns about the cost of the Greenways scheme. As detailed above, the creation of the Greenways network is one of the key strategic focuses of the Council to deliver high quality traffic free commuting and leisure routes connecting the existing and new settlements and points of interest. In light of these aims, the Council has set aside sufficient funds for the creation of the Greenways.

Theme 3: Drainage impact

Feedback from the consultation has raised concerns regarding the existing drainage system within the land to the north of Jubilee Avenue. The detailed design for this section of the Greenway is in the process of being developed, and topographical surveys have been commissioned that will inform the detailed design. The route of the proposed path will be designed with the existing drainage system taken into account. The surfacing of the Greenways is also designed to be fully permeable. The Council's Drainage team will be consulted on any proposals that would affect the drainage system on the land.

Theme 4: Ecology impact

Feedback from the consultation has raised concerns regarding the impact on the local wildlife and ecology of the area. The detailed design for this section of the Greenway is in the process of being developed, and ecological surveys have been commissioned that will inform the detailed design. The route of the proposed path will be designed to minimise the ecological impact of the scheme, including modifying the design as necessary and incorporating any recommended mitigation measures. The Council's Ecology Officer will be consulted on any such proposals.

Theme 5: Greenways rationale

There were a number of comments and questions raised in the consultation questioning the rationale behind the Greenways project, what the Greenways project means and why the Council is seeking to create the Greenways network. As detailed above, the creation of the Greenways network is one of the key strategic focuses of the Council to deliver high quality traffic free commuting and leisure routes connecting the existing and new settlements and points of interest. The implementation of the Greenways project is expected to bring the following benefits:

- People already cycling along these routes may experience reduced accident risk as the paths are safer and more separated from cars.
- People who are enticed to cycling or to cycling more as a result of the project are likely to experience health and wellbeing benefits from greater physical activity.
- Additional transport infrastructure in an area can increase the attractiveness of living in a place and potentially drive up land values.
- It can promote local business and other development in the area. Cycling infrastructure is more likely to drive business development than roads, as cyclists tend to travel more slowly and notice their surroundings in detail.
- Reducing parking pressures on local streets, transport hubs and streets close to shops.

- Reducing the need for parking to be included in new developments.
- Reducing the number of cars used to transport children to and from schools on a daily basis.
- Reducing wear and tear on roads.
- Reductions for vehicle owners in private car costs, such as for petrol and maintenance.
- Environmental benefits including reduction in carbon emissions, a reduction in polluted surface water run-off from vehicles using the highway and a reduction in the urban heat island effect.
- A greenway route as part of place making can have social benefits such as creation of a sense of place or community. There are potentially opportunities for investment in public art forms and advertising along the route.
- The route is also likely to attract casual users, particularly on weekends, with people likely to use the route and then access the various upgrades being made to Cantley Park, including the Destination Play Area, the sports pitches and the new café, producing financial benefits for the Council and local businesses.
- The greenway can also be used as an important educational resource for schools, encouraging cycling and walking as well as biodiversity and ecology.

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While there are a number of benefits that are likely to be generated from this scheme, it remains difficult to financially quantify the sum of these. However from an environmental and traffic congestion perspective, it is becoming increasingly important for a scheme of this nature to be delivered in the area and it is clear that the benefits far out way the initial costs to deliver the scheme.

Theme 6: Privacy concerns

Significant objections and comments were received as a result of the consultation raising concerns about the impact that the proposed scheme will have on the residents of Martins Drive. Some of the comments were concerned about the amount of vegetation that would need to be removed, which would enable the residents of Martins Drive to see the public using the Greenway. In addition to this, a separate petition was received from the residents of Martins Drive, reiterating the concerns that have been raised in the public consultation, and requesting that the scheme be cancelled.

Whilst the Council recognises the concerns of the residents regarding the scheme, it does not consider that the proposed route would significantly impact on the privacy of the residents, for the following reasons:

- The majority of the houses along the road are on the opposite side of the road to the wooded area. The closest property boundary is 8.15 metres of the path, and the property itself over 23 metres away. Outside of this the closest property boundary is 27 metres away from the proposed route of the path.

- Martins Drive itself, along with the adjoining footpath bridge link to Jubilee Avenue, is a public highway maintained by the Council. Members of the public have an existing right to pass and re-pass along this road and adjoining footways at a much closer distance to the residents' houses to where the proposed Greenways route will be situated. In addition to this the woodland area is marked on the Council's public open space records as land that the Council maintains.
- The existing use of the footway adjacent to Jubilee Avenue is already visible from Martin's Drive.
- The exact route of the Greenways is being developed as part of the detailed design, however the Council is aiming to keep the path as close to Jubilee Avenue as possible, leaving plenty of land between the path and Martins Drive.

Further planting of this area will be investigated as part of the design to increase the screening between the Greenway and Martins Drive.

In addition to the privacy concerns above, comments were received regarding the privacy of residents at Millmead, seeking reassurance that they do not lose any privacy that they currently have. With respect to this area, there are no plans for any significant changes to the route as existing, the existing fence lines will remain and there are no plans to remove the existing trees in this area.

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Theme 7: Safety of existing routes / Road crossings

A number of respondents have raised concerns about the road crossings, in particular at Milton Road and the Reading Road. These crossing points will be reviewed when the scheme advances to the detailed design stage for these sections.

The crossing at Milton Road will be reviewed once the scheme advances into Jubilee Avenue, and the Reading Road crossing will be examined as part of section 2 of the Route. All road crossing designs will comply with Highways design legislation, which specifies a three-stage safety audit of any proposal to ensure that they are safe for the public to use.

In addition to the comments regarding road crossings, comments were received regarding issues at Brook Close, including the issues of cycling access through from Brook Close, potential speed of traffic, overgrowing vegetation and surface issues. The Council will ensure that the vegetation is cut back as part of the works, and this, along with the condition of the surface will be monitored and cleared as part of the maintenance regime for the Greenway.

With regard to the speed of cycling, whilst we recognise that the gradients along the path do allow the potential for some speed to be developed, we are not proposing any particular changes to the use that is already happening along the path. As detailed below,

our experiences of the Greenway at California Country Park have not shown any particular issue for increasing the speed of cycling but we will monitor the situation and make adjustments if necessary.

Theme 8: Tree protection

Feedback from the consultation has raised concerns regarding the removal of trees within the land to the north of Jubilee Avenue. The detailed design for this section of the Greenway is in the process of being developed, however the design has been developed to ensure that no trees over 75mm in trunk diameter will need to be removed. Arboricultural and topographical surveys have been undertaken which have informed the detailed design. The route of the proposed path will be designed with the existing trees taken into account, and the design will seek to ensure that it minimises the impact on any of the existing trees in this area. The Council's Trees and Landscape team will be consulted on any proposals that would affect trees on the land.

Theme 9: Other comments

Other comments regarding the Greenways were received as follows:

Comment	Number of comments	Council response
Horse riding	4	<p>A number of comments questioned whether horse riding will be allowed along the route.</p> <p>Parts of Greenway Route B are being developed to allow horse riding where appropriate, however the section of the Greenway at Cantley Park to Woosehill through Matthewsgreen it is not intended to be for horse riding.</p>
Speed of cyclists	1	<p>We note the concerns about the potential speed of the cyclists along the route. The Council's experience of the Greenway at California Country Park has not highlighted any significant problems with the speed of cyclists, and therefore there are no plans to put in additional speed restrictions. However, we will continue to monitor the use of the Greenway once installed and can put in place intervention measures if necessary.</p>
Maintenance	2	<p>Maintenance questions were asked regarding the maintenance of the routes, highlighting the need for removal of the leaf fall.</p> <p>The Greenways will be maintained by the Council as part of its strategic active travel network. This will include periodic sweeping and clearance of the surface of the way.</p>
Comments on other sections	3	<p>There were a number of comments received about other sections of Route B. In addition to the alternate route suggestions examined under Theme 1. These will be considered during the design of those respective sections.</p>
Barriers	2	<p>A comment was received concerning the existing barrier at the entrance to Cantley Park from Wiltshire Road. The route of the Greenway has now been adjusted in light of the feedback received and will no longer connect directly to Wiltshire Road.</p> <p>Another comment was suggesting a barrier within the route to prevent potential conflict between cyclists using the Greenway at Jubilee Avenue, the need for any barrier at this point will be considered as part of the detailed design stages and the highway safety audits.</p>
Parking	1	<p>A question was raised asking if the Council plans to change the existing usage of Clifton Road, including parking.</p>

		There are no plans to change the existing use of Clifton Road as part of the Greenways project.
Street lighting	3	Concerns were raised about the implementation of increased street lighting in Cantley Park and at Jubilee Avenue. There are no plans to install street lighting along the Greenways.
Numbers of users	3	Questions were raised asking for the number of anticipated users for the Greenway. The council does not have any quantifiable number as to the numbers of users.
Bins	1	Comments were received requesting the number of increased bins that will be provided along the route to accommodate the increase of use, particularly within Cantley Park. The installation and maintenance of bins is a day-to-day land management issue and outside the scope of the Greenways project, however the land managers are closely involved with the plans and any additional bins will be arranged where they are required.
Climate change	1	Questions were raised regarding the removal of trees and the potential negative impact on the climate. As detailed above the detailed design of the scheme will seek to minimise any removal of existing trees, and seek re-planting of new trees in compensation where appropriate. In addition to this, the creation of an off-road non-vehicular commuting and leisure route has been identified in the Council's Climate Change Action Plan as a key element in reducing the existing carbon emissions.
Noise during the works	1	Concerns were raised about the noise of the works during any construction. As with all construction schemes, noise during the works is unavoidable, however any disturbance will be kept to a minimum as much as possible; the Council will be seeking "no dig" construction techniques as much as possible and through root protection areas of trees. The final surfacing of the Greenways is also laid by hand, which minimises the amount of heavy machinery use.
Signage	1	Comments were received recommending that correct signage is put in place that demonstrates where cycling is allowed. The Greenways signage includes clear diagrams and descriptions about what usage is allowed along certain sections.

Road cycling	1	<p>A comment was received highlighting that the route through Clifton Road is not ideal as it still requires cyclists to use the roads, and requested a subsequent phase that would address these issues.</p> <p>The Council does recognise that it is not ideal that road use will be necessary along the route, and has sought to reduce this as much as possible in the proposed design, using quieter roads where it becomes necessary. These issues can be addressed at a future time, however there is no second phase planned.</p>
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Agenda Item 55.

Application Number	Expiry Date	Parish	Ward
201022	12 February 2021	Twyford	Twyford

Applicant	Wokingham Borough Council
Site Address	Old School Hall, High Street, Twyford RG10 9AD
Proposal	Full application for the proposed restoration and conversion of existing building into a library, erection of a single storey lobby/link area and a two storey extension to provide multi-purpose community facilities including a café, plus associated landscaping and demolition of existing public toilets (phased)
Type	Full
Officer	Simon Taylor
Reason for determination by committee	Council application

FOR CONSIDERATION BY	Planning Committee on Wednesday 10 February 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>The Old Polehampton Boys School is a prominent Grade II listed building at the intersection of Polehampton Close and High Street, at the western end of Twyford Town Centre and Conservation Area. The building dates from c1877 and it was last approved for use as a hall or theatre space. The Polehampton Trust charity are the freehold owners of this building and the adjoining property (Polehampton School House) that is separately listed building now converted to residential flats.</p> <p>The Trust have sought to develop the rest of Old Polehampton Boys School building into a community facility and in October 2016, approval was granted for the redevelopment of the building to a library, parish council office, police office, cafe and community facility. That consent has since lapsed.</p> <p>The Council agreed in principle to support the development of a new library on the site of the Old Polehampton Boys School in Twyford at the meeting of the Executive on 25 July 2019. This would involve the relocation of the existing library on the opposite side of Polehampton Close, which has been identified as being in need of an upgrade.</p> <p>Phase 1 of the application involves the refurbishment of the listed building to be used as library space, with a modest single storey rear extension as a new entrance to the facility. Phase 2 involves a two storey rear extension extending from the phase 1 extension and comprising a café and community facility. The total increase in floorspace is 253m².</p> <p>In NPPF terms, there is some harm to the listed building but it is concluded that it is less than substantial harm (ie minor). It also involves the loss of bat roosts within the listed building and the existing toilet block and the felling of four Category B and C trees, one of which is a Sycamore that has been identified as part of this assessment as containing characteristics that would support its categorisation as a veteran tree. There have been no objections from neighbours, Twyford Parish Council or the ward members.</p> <p>Weighing the harm against the benefits, the proposal represents positive benefits to the community with the provision of improved library facilities, a new community space and ancillary café facilities in a sustainable town centre location. The architectural design is</p>

appropriate and it allows for the re-use and enhancement of a listed building. When weighing these public benefits against the identified minor harm that is mitigated or addressed via additional pre commencement and pre occupation/use information in Conditions 4-22 requiring supplementary heritage details, ecological mitigation, enhancement and bat licensing, landscape enhancement, protection and strategies, parking provision, drainage details and access details. The resulting development represents a positive outcome for the area.

PLANNING STATUS

- Major Development Location
- Twyford Town Centre
- Twyford Conservation Area
- Listed Building (Grade II)
- In the vicinity of other listed buildings
- Part WBC owned land
- Bat roost suitability
- Known bat roost
- Adjacent to priority habitat (Lowland mixed deciduous woodland)
- Flood Zone 1
- Classified road (High Street)
- Non-classified road (Polehampton Close)
- Adopted highway
- Radon affected area
- Nitrate vulnerable zone (groundwater)
- Heathrow Aerodrome wind turbine safeguarding zone
- Sand and gravel extraction consultation zone

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following conditions and informatives:

Conditions

1) Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2) Approved details

This permission is in respect of the submitted plans:

Phase 1

- a) A001 Rev A and A002 Rev A, dated 30 November 2020
- b) A002 Rev B, dated 15 December 2020
- c) A003, A004 and A005, dated 12 March 2020
- d) A010 Rev F and A011 Rev E, dated 30 November 2020

- e) A012 Rev D, dated 19 January 2021
- f) A013 Rev D, dated 19 January 2021
- g) A014 Rev D, dated 30 November 2020
- h) A030, dated 29 January 2021
- i) 2-001, 2-101 and 3-001, dated 12 March 2020
- j) 3-101 Rev B, dated 18 September 2020
- k) 4-001, dated 23 June 2020
- l) 4-002, 4-101 and 4-102, dated 12 March 2020
- m) 5-001, Rev A, dated 22 December 2020
- n) 5-101 Rev C, dated 30 November 2020
- o) 5-102, dated 22 December 2020
- p) 6-101, dated 12 March 2020
- q) 6-102
- r) 6-103 Rev C, dated 19 January 2021
- s) 7-101, dated 20 June 2020

Phase 2

- t) A020 Rev G, dated 30 November 2020
- u) A021 Rev D and A022 Rev B, dated 18 September 2020
- v) A023 Rev B, dated 18 September 2020
- w) A024 Rev C, dated 19 January 2021
- x) A025 Rev D, dated 30 November 2020

The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3) Notice of commencement

A minimum seven calendar days notice of the commencement of the development shall be given in writing to the local planning authority at development.control@wokingham.gov.uk **prior to the commencement of phase 1**, quoting the above reference number and addressed for the attention of the Planning Manager and the Conservation Officer at development.control@wokingham.gov.uk. The period of notice does not commence until receipt by the developer of an acknowledgement from the local planning authority that either the written notice or the email of notice has been received.

Reason: To allow opportunity for inspection and monitoring of the works to help ensure the satisfactory control of the work to the listed building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

4) Landscape strategy

Prior to the commencement of the development **for phase 1 and prior to the discharge of hard and soft landscaping details in Condition 5**, a landscape strategy shall be submitted to and approved in writing by the local planning

authority. The landscape strategy shall provide an understanding of how the urban form and visual quality of the buildings as part of phase 1 and 2 is to be enhanced through a planting scheme and this is to be used in guiding the design and layout of the hard and soft landscaping scheme (Condition 5) and boundary treatments (Condition 8).

Reason: In the interests of visual amenity.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

5) Hard and soft landscaping details

Prior to the commencement of the use of the **phase 1 and/or phase 2 buildings**, full details of both hard and soft landscape proposals for the relevant phase of development shall be submitted to and approved in writing by the local planning authority.

These details shall reflect the strategy approved under condition 4 and shall include, as appropriate, proposed finished floor levels or contours, means of enclosure, pedestrian access and circulation areas, hard surfacing materials and minor artefacts and structures, soft surfacing, play equipment and benches and all other structures within the children's activity area, composing facilities, green walls, refuse or other storage units, signs, lighting, external services, etc). Soft landscaping details shall include a planting plan, specifications (including cultivation and other operations associated with plant and grass establishment), schedules of plants, noting species, planting sizes and proposed numbers/densities where appropriate, and implementation timetable.

All hard and soft landscape works shall be carried out in accordance with the approved details for each phase prior to the occupation of any part of the development or in accordance with an alternative timetable approved in writing by the local planning authority. Any trees or plants which, within a period of five years after planting within the respective phase, are removed, die or become seriously damaged or defective, shall be replaced in the next planting season with others of species, size and number as originally approved and permanently retained.

Reason: In the interests of visual amenity.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

6) Tree protection details

Prior to the commencement of the development **for phase 1**, an Arboricultural Method Statement and Scheme of Works for the phase 1 works which provides for the retention and protection of trees, shrubs and hedges growing on or adjacent to the site in accordance with BS5837: 2012 shall be submitted to and approved in writing by the local planning authority. It is to include an arboricultural watching brief for the works at and around Tree 1. No development or other operations for Phase 1 shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).

Prior to the commencement of the **phase 2 development works**, a further Arboricultural Method Statement and Scheme of Works for the phase 2 works shall be submitted to and approved in writing by the local planning authority. No development or other operations for Phase 2 shall take place except in complete accordance with the details as so-approved (hereinafter referred to as the Approved Scheme).

No operations shall commence on site in connection with each phase of the development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the relevant Approved Scheme are in place on site.

No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the relevant Approved Scheme.

The fencing or other works which are part of the relevant Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works associated with the respective phase, including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

7) Landscape management details

Prior to the commencement of the use of the **phase 1 and/or phase 2 buildings**, a landscape management plan for the relevant phase, including long term design objectives, management responsibilities, timescales and maintenance schedules for all landscape areas, other than privately owned, domestic gardens, shall be submitted to and approved in writing by the local planning authority. The landscape management plan(s) shall be carried out as approved.

Reason: In order to ensure that provision is made to allow satisfactory maintenance of the landscaping hereby approved.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

8) Boundary treatment details

Prior to the commencement of the use of the **phase 1 and/or phase 2 buildings**, details of all boundary treatments for the relevant phase, including to delineate the parking space for staff purposes, shall be submitted to and approved in writing by

the local planning authority. The approved scheme for each phase shall be implemented prior to the first occupation of each phase of the development. The scheme shall be maintained in the approved form for so long as the development remains on the site.

Reason: In the interests of amenity and highway safety.

Relevant policy: Core Strategy policies CP1 and CP3,

9) Listed building method statement

Prior to the commencement of the development **in phase 1, including any stripping out, demolition and alteration works**, a method statement for works to the listed building and a timetable for the works shall be submitted to and approved in writing by the local planning authority. The works shall be implemented in accordance with the details so-approved.

Reason: In order to safeguard the special architectural or historic interest of the building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

10) Listed building details

Prior to the commencement of the development **in phase 1, including any stripping out, demolition and alteration works**, the following details (including drawings and schedules) shall be submitted to and approved in writing by the local planning authority and shall be implemented as so-approved prior to commencement of the use:

- a) The means of abutment of the proposed fixed glazed rooflight to the rear elevation of the listed building
- b) All parts of the building to be removed, dismantled or stripped out, including the removal of internal features, floors, and wall or ceiling surfaces
- c) Materials to be used in the external surfaces of the building
- d) New joinery, including full sized moulding profiles
- e) Any areas of external brickwork required to be re-instated (as matching the existing brickwork in brick colour and size, mortar colour, and pointing and bonding detail)
- f) All new works and works of making good to the retained fabric, whether internal or external (finished to match the adjacent work with regard to the methods used and to material, colour, texture and profile)
- g) The relationship of the thermal insulation for the roof and the existing guttering

Development shall not be carried out other than in accordance with the so-approved details.

Reason: In order to safeguard the special architectural or historic interest of the building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

11) Central partition details

Prior to commencement of development **in phase 1, including any stripping out, demolition and alteration works**, a scheme showing the retention of a section of the central partition so as to provide visual evidence of its function to subdivide the space into two teaching classrooms shall be submitted to and approved in writing by the local planning authority. An interpretive plaque illustrating the historic function of this building as a school shall form part of the approved scheme. The works shall thereafter be undertaken and the plaque erected in accordance with the approved scheme.

Reason: To help ensure the satisfactory control of the work to the listed building in order to safeguard its special architectural or historic interest.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

12) Bat licence

Prior to the commencement of development **for phase 1 and for phase 2**, including any demolition or refurbishment works, a licence for development works affecting bats shall be obtained from the Statutory Nature Conservation Organisation (Natural England) and a copy submitted to and acknowledgement given by the local planning authority. Thereafter, mitigation measures approved in the licence shall be maintained for each respective phase in accordance with the approved details. Should the applicant conclude that a licence for development works affecting bats is not required the applicant is to submit a report to the council detailing the reasons for this assessment and this report is to be approved in writing by the council prior to commencement of works.

Reason: The buildings host multiple bat roosts which are affected by the proposal. This condition will ensure that bats, a material consideration, are not adversely impacted upon by the proposed development, and that the Council demonstrates that the council has fulfilled its duties under the Conservation (Natural Habitats &c.) Regulations 2017.

13) Biodiversity enhancement details

Prior to the commencement of **either Phase 1 or Phase 2 of the development, including any demolition works, internal refurbishment or works to the roof**, details of biodiversity enhancements pertaining to the relevant phase of development and their locations shall be submitted to and approved in writing by the council. The biodiversity enhancements shall thereafter be installed as approved.

Reason: To incorporate biodiversity in and around developments.

Relevant policies: Paragraphs 170 and 175 of the NPPF, Policy CP7 of the Core Strategy and Policy TB23 of the Managing Development Delivery Local Plan.

14) Drainage details

Prior to the commencement of the development **for phase 1**, full details of the drainage system for phase 1 of the development shall be submitted to and approved in writing by the local planning authority. The details shall include how the site

currently drains, how it will be drained after phase 1 of the proposed development with consideration to SuDS and shall include an overarching strategy for how the drainage system for Phase 2 can be accommodated at the site in the future should the phases not be built out concurrently.

Prior to the commencement of the development **for phase 2**, full details of the drainage system for the site (inclusive of Phase 1 and Phase 2) shall be submitted to and approved in writing by the local planning authority, accounting for how the site drains at the completion and occupation of the development, with consideration to SuDS.

Reason: This is to prevent increased flood risk from surface water run-off.

Relevant policy: NPPF Section 10, Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

15) Thames Water main

Prior to the commencement of the development **for phase 2**, information detailing how the developer intends to divert the asset/align the development so as to prevent the potential for damage to subsurface potable water infrastructure shall be submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any construction must be undertaken in accordance with the terms of the approved information. No construction associated with phase 1 or 2 shall take place within 5m of the Thames Water main without the prior authorisation from Thames Water. Unrestricted access must be available at all times for the maintenance and repair of the asset during and after all construction works.

Reason: The proposed works will be in close proximity to underground Thames Water strategic water main, utility infrastructure. The works have the potential to impact on local underground water utility infrastructure.

16) Construction Method Statement

Prior to the commencement of any development **for Phase 1 including any demolition or refurbishment works**, a Construction Method Statement and Management Plan shall be submitted to and approved in writing by the local planning authority. The Statement shall provide for:

- a) The parking of vehicles of site operatives and visitors
- b) Vehicle routing
- c) Loading and unloading of plant and materials
- d) Storage of plant and materials used during construction
- e) The erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate
- f) Wheel washing facilities
- g) Excavation methods and estimated volume of material to be removed from the site and subsequent number of HGV movements to and from the site
- h) Measures to control the emission of dust, dirt, odour and noise during construction
- i) The control of rats and other vermin
- j) A scheme for recycling/disposing of waste resulting from demolition and construction works

- k) The control of surface water run-off
- l) The proposed method of piling for foundations (if any)
- m) Construction and demolition working hours
- n) Delivery hours
- o) Measures to ensure the safe operation or stability of the railway

Where revisions are proposed to the approved details as part of the demolition and construction works for Phase 2, a revised Construction Method Statement and Management Plan shall be submitted prior to the commencement of any development, including demolition, **for Phase 2**.

The approved Statement shall be adhered to throughout the demolition and construction phases of phase 1 and 2.

Reason: In the interests of highway safety and convenience and neighbour amenities.

Relevant policy: Core Strategy policies CP3 & CP6.

17) Cycle parking details

Prior to the commencement of the use of the **phase 1 and phase 2 buildings**, details of secure and covered bicycle parking for staff, occupants and visitors in relation to the relevant phase shall be submitted to and approved in writing by the local planning authority. The cycle storage/parking shall be implemented in accordance with such details as may be approved before occupation of each phase of the development, and shall be permanently retained in the approved form for the parking of bicycles.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel.

Relevant policy: NPPF Section 4 and Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

18) Electric Vehicle charging details

Prior to commencement of the development **for Phase 2**, an Electric Vehicle (EV) charging strategy shall be submitted to and approved in writing by the local planning authority. This strategy shall include details relating to electric vehicle charging infrastructure, including a plan showing electric vehicle charging provision within the public car park in line with Appendix E of the WBC Living Streets: Highways Design Guide (2019), and details of installation of charging points and future proofing of the site. The development shall be implemented in accordance with the agreed strategy thereafter.

Reason: In order to ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel.

Relevant policy: Core Strategy policies CP1, CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

19) Remediation details

Prior to the commencement of the **phase 1 development**, a scheme to investigate

potential contamination of the site and a timetable for the investigation works shall be submitted to and approved in writing by the local planning authority. The scheme shall include and be informed by a full desktop study of the entire site (including both phases of the development - to include scale map, historical information for the site and the surrounding area, geology, hydrology, hydrogeology, a risk assessment for human, buildings and environment, and a conceptual model).

Where the desk top study identifies the need for an intrusive site investigation as part of the scheme of investigation, the written report for such shall be submitted to and approved in writing by the planning authority. The report shall contain the results, interpretation, analysis, a scheme/timetable of/for the remediation works, and conclusions. The remediation works shall thereafter be carried out as agreed or unless otherwise agreed in writing by the local planning authority.

A site condition report and validation certificate must be submitted post remediation to the planning authority and approved in writing, unless it has been agreed with the local planning authority through the desk top study or intrusive site investigation that no further remediation is necessary for the proposed end use.

Reason: To protect the occupants of the development from contamination and to protect the workforce undertaking the development.

Relevant policies: National Planning Policy Framework Section 15.

20) Sustainability details

Prior to the commencement of **phase 1 or phase 2 of the development**, full details of internal and external sustainability measures to be incorporated into the respective phase of the development shall be submitted to and approved in writing by the local planning authority. The approved measures shall be implemented before the respective phases are first occupied and shall remain operational for the lifetime of the development.

Reason: To ensure developments contribute to sustainable development. Relevant policy: NPPF Section 14, Core Strategy policy CP1, Managing Development Delivery Local Plan policy CC05 and the Sustainable Design and Construction Supplementary Planning Document.

21) Bin storage

Prior to the occupation of any building within **Phase 1 and 2** respectively, details of bin storage facilities (including for food waste) for the respective phase shall be submitted to and approved in writing by the local planning authority. The facilities shall be provided in accordance with the approved details and shall be permanently so-retained and used for no purpose other than the temporary storage of refuse and recyclable materials.

Reason: In the interests of visual and neighbouring amenities and functional development.

Relevant policy: Core Strategy CP3 and Managing Development Delivery Local Plan policy CC04.

22) Noise and odour details

Prior to commencement of the **first use of the café in phase 2**, details of suitable ventilation and filtration equipment to suppress and disperse fumes and/or smell created from the cooking operations on the premises shall be submitted for approval in writing by the local planning authority. The equipment shall be effectively operated and maintained in accordance with manufacturer's instructions for as long as the proposed use continues.

The approved equipment shall be installed and be in full working order prior to the commencement of use of the cafe. Details to include outlet height, which in general should be at least 1m above ridge height of the nearest building.

Reason: To protect the amenity of local residents

Relevant policy: Core Strategy Policy CP3 and Managing Development Delivery Policy CC06.

23) Car parking

No part of the development hereby permitted **in phase 1** shall be occupied or used until the staff vehicle parking space has been provided in accordance with the approved plans of phase 1. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

No part of the development hereby permitted **in phase 2** shall be occupied or used until the revised parking arrangements are provided in accordance with the approved plans of phase 2. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity.

Relevant policy: Core Strategy policies CP3 and CP6 and Managing Development Delivery Local Plan policy CC07.

24) Storage of roof coverings

Notwithstanding the details submitted, the existing roof tiles are to be carefully removed and safely stored whilst the hereby approved works to insulate the roof are undertaken, and are then to be reinstated. Where additional replacement clay roof tiles are required these shall match in all aspects (material, size, profile(s), colour and finish) unless otherwise agreed in writing by the local planning authority prior to their use.

Reason: In order to safeguard the special architectural or historic interest of the building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

25) Windows, screens and doors

Notwithstanding the details submitted, the consent hereby given does not extend to include any alteration and/or removal of the existing windows, doors (external and

internal), door frames or the room screen (to the eastern end of the hall), other than the removal of central partition to the hall and the modification of the central window to the southern elevation of the hall as part of the doorway to the new link structure.

Reason: In order to safeguard the special architectural or historic interest of the building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

26) Compliance with the ecology survey

Development undertaken in accordance with **phases 1 and 2** is to be carried out in accordance with the mitigation strategy set out in the recommendations in Section 6 of the submitted ecological report (Ecology by Design, ref: EBD01329, dated 17 December 2020) unless otherwise agreed in writing by the local planning authority or by any approved details in other conditions of this planning permission.

Reason: To ensure that bats and reptiles, both protected and priority species (as per the NPPF), are not adversely affected by the proposals.

Relevant policies: Paragraphs 170 and 175 of the NPPF, Policy CP7 of the Core Strategy and Policy TB23 of the Managing Development Delivery Local Plan.

27) Retention of trees and shrubs

No trees, shrubs or hedges within the site which are shown as being retained on the approved plans shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area.

Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

28) Hours of construction work and deliveries

No work or deliveries relating to the construction of the development hereby approved, including works of demolition or preparation prior to building operations, shall take place other than between the hours of 8am and 6pm Monday to Friday and 8am and 1pm Saturdays and at no time on Sundays or Bank or National Holidays.

Reason: To protect the occupiers of neighbouring properties from noise and disturbance outside the permitted hours during the construction period.

Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

29) Demolition of toilet block

The development in **Phase 2** shall not be occupied until the existing toilet block shown to be demolished on the approved plan has been so demolished and the resulting materials removed from the site.

Reason: In the interests of the amenity of the area.

Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policies TB21.

30) Permeable surfacing

The hard surfacing hereby permitted shall be constructed from porous materials or provision shall be made to direct run-of water from the hard surface to a permeable or porous area within the curtilage of the development. The hard surfacing shall be thereafter so-retained.

Reason: To prevent increased flood risk from surface water run-off.

Relevant policy: NPPF Section 14 and Managing Development Delivery Local Plan policies CC09 and CC10.

31) Hours of cafe

No customer shall be permitted to be on the café premises outside of 7am-11pm, Monday to Saturday or 9am-8pm on Sundays and public holidays. No deliveries shall be taken in or dispatched from the site outside these hours.

Reason: To safeguard residential amenities.

Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

32) Permission not given

Notwithstanding the details submitted, no fixtures (i.e. flues/vents, external lighting, security cameras, satellite dish, cabling, pipework, alarm boxes, flag/hanging plant brackets etc.) other than those shown on the plans as approved shall installed and/or affixed to the exterior surfaces of the listed building without written approval from the local planning authority prior to the works being carried out.

Reason: In order to safeguard the special architectural or historic interest of the building.

Relevant policy: National Planning Policy Framework Section 16 and Managing Development Delivery Local Plan policy TB24.

Informatives

1) Phases of Development

This permission relates to two phases of development, as approved by Condition 2 and involving the conversion, renovation and change of use of the listed building and a single storey entrance extension for use as Twyford Library (Phase 1) and the

demolition of the toilet block and erection of storey rear extension for use as a community space, office space and café (Phase 2).

2) Thames Water

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by emailing trade.effluent@thameswater.co.uk by phoning 020 3577 9483. Application forms should be completed online

Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to the Thames Water website.

If you are planning on using mains water for construction purposes, it is important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.

Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

3) Network Rail

Network Rail are the landowners across the western boundary and they must be consulted on matters of fencing height, type and materials, drainage, demolition, scaffolding and cranes, access, landscaping species and the children's play area.

Any works on this land will need to be undertaken following engagement with Asset Protection to determine the interface with Network Rail assets, buried or otherwise and by entering into a Basis Asset Protection Agreement, if required, with a minimum of 3months notice before works start. Initially the outside party should contact assetprotectionwestern@networkrail.co.uk.

4) Blue badge parking

The applicant is advised to contact the Highways Authority to allow for the provision of additional blue badge parking or the relocation of the existing blue badge parking closer to the Phase 2 building.

5) Pre-commencement conditions

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may

be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

6) Demolition notice

The applicant is reminded that a Demolition Notice may be required to be served on the Council in accordance with current Building Regulations and it is recommended that the Building Control Section be contacted for further advice.

Attention is drawn to Section 8(2) of the Planning (Listed Buildings and Conservation Areas) Act 1990, the effect of which is that any demolition may not be undertaken (despite the terms of this consent) until notice of the works has been given to English Heritage, Historic Research and Conservation Support, Eastgate Court, 195-205 High Street, Guildford GU1 3EX (01483 252000) , and they have been given reasonable access to the building for at least one month following the grant of this consent, for the purpose of making a record or within that period, they have stated that they have completed their record of the building or that they do not wish to record it.

7) Changes to the approved plans

The applicant is reminded that should there be any change from the approved drawings during the build of the development this may require a fresh planning application if the changes differ materially from the approved details. Non-material changes may be formalised by way of an application under s.96A Town and Country Planning Act 1990.

8) Advertisements consent

This permission does not convey or imply any approval or consent that may be required for the display of advertisements on the site for which a separate Advertisement Consent application may be required. You should be aware that the display of advertisements without the necessary consent is a criminal offence liable to criminal prosecution proceedings through the courts.

9) Protected species

This permission does not convey or imply any approval or consent required under the Wildlife and Countryside Act 1981 for protected species. The applicant is advised to contact Natural England with regard to any protected species that may be found on the site.

10) Listed building consent

This permission does not convey or imply any approval or consent in respect of Listed Building Consent that is required for external or internal alterations to the listed building(s).

11) Discussion

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of amended plans being submitted by the applicant to overcome various concerns.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

PLANNING HISTORY

The building is Grade II listed, with the listing stated as:

Former school. c1887, erected by The Polehampton Trust. Grey Reading brick with red Reading brick pilasters, strips, panels, cornices and dressings to windows, chimneys and gables. Tiled roofs with segmental and triangular pedimented Flemish gables having terracotta ball finials and, to the main elevation, a tall enriched brick chimney stack. Queen Anne style. Open courtyard plan with projection to east. Single storey. Plain entrance on main facade within a gabled lower wing, partly timber framed and having 4 lights with glazing bars beneath a tile hung gable with a vertically set window. Main windows are tall sashes with glazing bars and gauged brick segmental heads with keystones; western facing gable windows of Venetian type. Well detailed terracotta airbricks and metal bootscraper and rosette rainwater pipe brackets. Interior retains original features including open timber roof trusses. This school was built on the site of the demolished chapel of a school founded in 1721 by Edward Polehampton for the education of 10 poor Twyford boys. The master's house still exists (qv) and the school is attached to it, being a replacement for the original school room; it was still only for boys, a girls and infants school being built the following year in the village.

App Number	Proposal	Decision
S/3/1958	Lavatory block	Approved 14 May 1958
S/1/1959	Addition of kitchen and dining room	Approved 14 January 1959
O/257/1965	Scout headquarters	Refused 12 January 1965
O/24/1966	Scout headquarters	Refused 2 March 1966
O/103/1966	Library (outline)	Approved 15 June 1966
03213	Vehicular access	Approved 2 October 1975
10682	Garage and access	Approved 10 April 1979
10683 (LBC)		Approved 5 July 1979
14023	Demolition of coach house for access	Approved 25 September 1980
14024	Car park and access	Approved 25 September 1980
16607	Public library	Temporary 12 February 1992
28064	Store building	Approved 5 August 1987
37441	Conversion to two flats and two storey extension	Approved 1 May 1991
37442 (LBC)		Approved 1 May 1991
38327	Car park to the rear	Approved 14 January 1993
38839	Public library	Temporary 12 February 1992
39537	CoU of school hall to theatre	Refused 18 December 1992
-	Appeal against refusal of 39537	Dismissed 9 September 1993

FP/1994/3174	Refurbishment of public toilets with ramp	Approved 21 December 1994
LB/2002/5642	Exterior renovation works	Refused 29 May 2002
LB/2002/6953	Exterior and interior renovation works	Approve 20 August 2002
153122	Restoration and conversion into a library and police office, single storey lobby/link and two storey extension for community facilities, café, theatre, history society room and ancillary facilities	Approved 11 October 2016
153123 (LBC)		Approved 11 October 2016
201023 (LBC)	Restoration and conversion into a library, single storey lobby/link and two storey extension for community facilities and café following demolition of existing public toilets	Considered concurrently

SUMMARY INFORMATION		
	Existing	Proposed
Site area	820m ²	970m ² (including part of car park)
Land use	Disused hall	Library with community hall and cafe
Floor space	123m ²	376m ²
Employment	Nil	Unknown
Dwellings	2 existing units (not part of application)	
Car spaces	41 in public car park at rear One staff space and two resident spaces at front of site	40 in car park at rear One staff space and two resident spaces at front of site
Cycle spaces	None	7 racks (14 spaces)

CONSULTATION RESPONSES	
WBC Conservation	Concerns were initially raised in relation to the impacts posed by the solar panels on the roof of the listed building and the glazed roof extension. To address these concerns, the solar panels were deleted from the listed building in phase 1 and the design of the glazed roof extension amended. Subject to further details in Conditions 9-11 and specific stipulations in Conditions 24, 25 and 32, the harm to the listed building is limited and no objection is raised.
WBC Drainage	No objections are raised but as there will be increase in impermeable area, further drainage details are required by Condition 14. There should be no drainage features or stormwater discharge to Network Rail land or assets without their consent.
WBC Ecology	The works would require a derogation licence from Natural England but can be covered by a bat licence (Condition 12). Details of biodiversity enhancements (bat boxes) and their location are required to protect the listed status of the building (Condition 13). Compliance with mitigation measures is outlined in Condition 26.
WBC Environmental Health	No objections are raised, subject to Conditions 16 relating to the control of environmental effects during construction, working hours, contaminated land, noise and odour management and opening hours. On the latter, the café can only operate between 7am and 11pm Monday

	to Saturday and 9am and 8pm on Sundays and bank holidays (Condition 31).
WBC Highways	No objection in terms of traffic generation, car parking and construction management. Initial queries were raised in relation to accessible parking, but this is addressed by Informative 4. Further details are required for EV charging (Condition 18) and cycle parking (Condition 23).
WBC Tree and Landscape	The loss of the Sycamore tree is regrettable but accepted subject to landscape enhancement through replacement planting. Further details of the hard and soft landscaping scheme are required in Condition 5. Tree protection forms Condition 6 and landscape management is required in Condition 7. All of these details are first to be guided by a landscape strategy in Condition 4.
WBC Community Engagement	No comments received.
WBC Waste Services	
Network Rail	No in-principle objection is raised, subject to the submission of further details relating to the design of the children's activity area, tree protection, site fencing and construction management. These details are requested as part of Conditions 5, 6, 8 and 16 respectively.
Thames Water	Measures to minimise groundwater discharges into the public sewer would be required and subject to the sequential approach to the disposal of surface water, no objection is raised. There is sufficient water and water treatment infrastructure capacity though the building over or construction within 5m of a Thames Water mains is prohibited. These requirements are covered by Condition 15 and Informative 2.

REPRESENTATIONS	
Twyford Parish Council	No objection, subject to better provision for secure cycle, pushchair and mobility scooter parking. <u>Officer comment:</u> Pushchair and mobility scooter storage would be best accommodated within the internal layout of the library building rather than an allocated external storage area where there are conservation and streetscape impacts and as it would then necessitate unassisted movement into the building. There is a compliant amount of cycle parking, as discussed in 'Highway Access and Parking Provision' and addressed by Condition 23.
Ward Member	No comments received.
Neighbours	No comments received.

APPLICANTS POINTS	
	<ul style="list-style-type: none"> • The building/site is suitable in principle to accommodate town centre uses and also office uses • The re-use and extension to the existing building to provide enhanced community space, allowing for the re-location of the library from temporary modular buildings into a permanent building is a public benefit that weighs in favour of approval • There would also be public benefit arising from the re-use of the building facilitating the longevity of the listed building and in allowing it to be experienced and appreciated by the community

- The public benefits of the scheme would outweigh “the less than substantial harm” identified to the significance of the Listed Building
- Subject to conditions, the development would not unreasonably impact upon the amenity of neighbouring dwellings
- Subject to conditions to secure on-site car and cycle parking, the development would not have a severe impact on the highway network

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
	NPPG	National Planning Policy Guidance
Core Strategy 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
	CP13	Town Centres and Shopping
Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk
	CC10	Sustainable Drainage
	TB15	Major Town, and Small Town/District Centre Development
	TB16	Development for Town Centre Uses
	TB21	Landscape Character
	TB23	Biodiversity and Development
TB24	Designated Heritage Assets	
Supplementary Planning Documents	BDG	Borough Design Guide SPD
	SDC	Sustainable Design and Construction SPD
	SPD	

PLANNING ISSUES	
Description of Development	
1.	<p>The proposal, as amended, involves the change of use of the existing hall for use as a new Twyford Library (including children’s library) with a part one/part two storey rear extension for community and café uses. More specifically, it involves the following works that have been incorporated into two phases for funding and timing purposes:</p> <p><u>Phase 1</u></p> <ul style="list-style-type: none"> • Conversion and renovation of the existing Grade II listed building including, disabled toilet and staff room, and access stairs, platform lift access into new entrance extension. • Single storey entrance extension with flat roof and rooflight to the rear of the listed building with signage to the side elevation denoting ‘Twyford Library’ • Provision of a staff car space and cycle parking in the north western corner and public cycle parking racks in the south eastern corner

- Permeable paving, benches, fencing and soft landscaping to the surrounds, including the provision of a children's activity area to the western side of the building

Phase 2

- Demolition of the existing toilet block
 - Erection of a two storey extension with curved roof comprising of a 45m² community space with ancillary office space, plant room, store, accessible toilet and small kitchen on the lower floor and café (with servery, kitchen, office and store), toilets (including accessible toilets) on the upper floor and a staircase serving both floors
 - The loss of one car space and the provision of an electric vehicle (EV) charging space in the existing car park
 - Continuation of permeable paving on the eastern side of the site, with new accessible pedestrian ramps
 - Provision of four cycle stands
 - Removal of four mature trees and replacement landscaping
2. A listed building consent 201023 was submitted at the same time and is to be assessed concurrently.
 3. With minor changes, the application is a resubmission of planning permission 153122 and listed building consent 153123, which lapsed in October 2019.

Description of Site

4. The Old Polehampton Boys School is a Grade II Listed building on the southern side of High Street, at the western end of Twyford Town Centre and within the Twyford Conservation Area. The Twyford-Henley branch railway line forms the western boundary and Polehampton Close forms the eastern boundary, with a public car park to the rear. There are some residential properties to the west before opening to open countryside and lakes. Commercial and retail premises in the town centre predominate to the east.
5. The school building dates from c1887 and is currently a disused hall. The site measures 970m² and slopes from the street by about 4m down to the rear boundary with the car park. There are two flats within the building on the corner of High Street and Polehampton Close but these are excluded from the application site.

Principle of Development

6. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
7. The building is within a major development location within Twyford Town Centre, which is suitable for town centre uses in accordance with Policy CP13 of the Core Strategy. Town centre uses include retailing, office and community uses (including

libraries and village/community halls). Paragraphs 4.62 and 4.64 of the Core Strategy then note that the aims are to increase to improve the vitality and viability of centres within the borough with potential for additional floorspace to maintain the roles of the town centres.

8. Paragraph 85 of the NPPF seeks to support the role that town centres play at the heart of local communities, by taking a positive approach to their growth, management and adaptation, including allocating a range of suitable sites in town centres to meet the scale and type of development likely to be needed, looking at least ten years ahead.
9. The Council has identified a need for a larger, more modern library in Twyford and the proposed site, which is opposite the existing smaller library, accords with the intent of Policy CP13, as it did when it was last assessed in 2015. The change of use of the existing school hall to a library accords with Policy CP13 as does the addition of 258m² of floorspace for community and retailing uses.
10. Policy TB15 of the MDD Local Plan indicates that retail development is to be of a scale that is compatible to the hierarchy of the centre. Cafés should be in secondary shopping frontages, contribute to the provision of day and evening/night-time uses, be compatible with other uses and enhance the vitality and viability of the area. The café is at the rear of the library as an ancillary function of the main library and children's library. It is not within a primary or secondary retail frontage but because it complements the existing library and because of its modest size, it is an appropriate use for the location.
11. Policy TB16 of the MDD Local Plan indicates that proposals for retail uses of more than 500m² that are outside the primary shopping require a sequential test or retail impact test. The café use is the only retail use in the proposed extension and it measures 78m² such that there is no assumed compromising of existing retail locations and no requirement for the tests.
12. More broadly, the proposal would result in public benefits, from facilitating the re-use of the existing building and extending the longevity of the building and allowing it to be experienced and appreciated by the community who will have access to the building as users of the proposed services. In principle, the conversion and extension to the existing building for the purposes of the proposed uses is acceptable, subject to other considerations, including an assessment of the harm upon the listed building, character of the area, trees, ecology, highway safety and neighbouring amenity, as outlined below.

Heritage and Conservation

13. Section 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that development must ensure the preservation of listed buildings, including their setting, Paragraphs 192-196 of the NPPF require consideration of the harm to the significance of a designated heritage asset and Policy TB24 of the MDD Local Plan requires the conservation and enhancement of Listed Buildings and Conservation Areas, including their views and setting.
14. The Old School building is a Grade II listed former school building of late C19th in the 'Queen Anne' style. It is a distinctive building within a prominent position at the

intersection of Polehampton Close and High Street. It is within the Twyford Village Conservation Area although the rear extensions of Phase 2 will fall outside of the conservation area boundary.

15. Adjoining the old school building to the north is the Grade II listed, C18th Chequer brick, Polehampton School House which is now subdivided into two apartments. Chiswick House (C15th former hall house, now 3 houses), 24 High Street (C16th cottage, now offices) and the Duke of Wellington Public House (C17th house, now a pub) are other Grade II listed buildings in the immediate vicinity.
16. The existing building is sound but in need of modernisation as it does not have heating or toilet facilities. The proposal is to extend the building by way of a glazed link (Phase 1), then opening onto a two storey extension (Phase 2).
17. There is the loss of some features of interest, including part of the triple sash window on the southern side of the building and an existing moveable wooden and glazed partition within the building which divides the rooms of the building. The loss of these features is regrettable but understandable to achieve a workable connection between the two buildings and to achieve a building space that meets the requirements of the end users. As compensation, Condition 11 requires a scheme to be agreed to retain parts of the partition as visual evidence of the way the school rooms were formerly divided and to secure a scheme of recording any lost features of interest.
18. Internal changes to the listed building include the following:
 - Introduction of thermal insulation measures to the internal faces of the external brick walling to the building
 - Thermal insulation of the roof
 - Thermal improvement of the existing windows (and exterior doors)
 - Introduction of thermal insulation of the floors and introduction of underfloor heating
 - A series of metal rods spanning across the width of the hall at the level of the horizontal truss beams to provide structural support
19. These measures have been detailed from a heritage perspective in construction drawings. These have been reviewed by the Conservation Officer and no objection is raised and they form part of the approved details in Condition 2. Any proposal to remove them to facilitate thermal improvements would not be supported.
20. The outstanding aspect of the scheme involves the glazed roof element of the phase 1 link structure. Concerns remain about how this element would sit with the feature brickwork of the southern elevation, including separation from the wall itself. It is, however, concluded that this is not an unresolvable issue and further details are required in Condition 10.
21. Condition 10 also requires the submission of other minor details relating to the construction phase, including materials, joinery, brickwork and the relationship of thermal insulation with the guttering so as to ensure that the guttering still functions satisfactorily. Condition 9 requires a method statement detailing the timetable for all of the above works, Condition 24 requires safe removal and storage of the roof tiles

and Conditions 25 and 32 seek to retain existing windows and prohibit external fixtures.

22. The public benefit arising from the scheme would include the community benefits of a library and community hall uses which would allow the building to be experienced and appreciated by residents who would have access to the building as users of the proposed services. In addition, a purpose built library facility would bring enhanced benefits compared with the existing library facility which is contained within a smaller more constrained building. The provision of a 'community hub' where users can access a number of services within one building is considered to provide positive benefits.
23. There would also be economic benefits resulting from the scheme in terms of the income generation from the renovation and construction of extension itself, the cafe facility and the hire/lease options that could be available for the newly created and renovated floorspace which would bring wider sustainability benefits to the community.
24. With the provision of revised details, the Council's Conservation Officer does not raise any in principle objection (partly because they were approved in largely the same form in 2016). It is therefore considered that subject to the above requirements, the public benefits of the scheme would significantly outweigh the less than substantial harm (ie minor) identified to the listed building which would accord with the NPPF and Policy TB24 of the MDD Local Plan.
25. The proposal also adopts a satisfactory impression within the Twyford conservation area, as detailed below.

Character of the Area

26. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high quality design. Various guidelines in the Borough Design Guide aim to respect the character of the area, including
 - NR1 requires that development respond to features of the site and NR2 states that proposals should improve the area
 - NR3 requires a positive arrival impression and NR4 requires buildings address the street
 - NR5 notes that height, bulk and massing should respond to the local context and NR6 requires moderation where there are differences in height
 - NR7 requires that roofs be designed to the context of the area and NR8 requires high quality and simple materials and components
 - P2 and NR10 requires unobtrusive and compatible car parking and NR11 requires screening to servicing
 - NR12 requires appropriate boundary treatments
 - NR13 states that new public spaces should contribute to the quality of the public realm and NR18 requires a high quality environment for occupants
27. The site occupies a prominent location on Twyford High Street and within the conservation area, built to the street on the corner with a more expansive setback

from the street to its west. It is readily visible in views on Polehampton Close, partly because of the sightlines across the car park at the rear.

28. Externally, there are very little changes to the listed building and the streetscape presentation will remain largely unchanged. Fencing and hard and soft landscaping will contribute partly to the streetscape presence and these details are to be finalised as part of Conditions 5 and 8.
29. The proposed link (Phase 1) joining the old and new buildings has a glazed wall to its eastern side, a large expanse of glass to the rear elevation (which in time will be removed to allow access to Phase 2) and a pitched rooflight, framed by red Reading bricks to match the existing building. The flat roof is covered by a dark grey single ply roof membrane.
30. The form of the rear extension (Phase 2) is modern by comparison to the listed building and includes a curved roof, split into two stepped sections with the elevations articulated to break up the large expanse and mass and to provide more visual interest.
31. Through the design, form, glazing and materials, the rear extension as a whole adopts a relatively contemporary form and deliberately but successfully contrasts with the listed building. The flat roof link in phase 1 provides the required separation from the listed building.
32. The resulting bulk (expressed in building depth and volume) is comparable to the existing listed building but it does not overwhelm or dominate the listed building. This is because of the adequate separation achieved by the link structure, the lesser building width of the rear extension and because the slope of the site allows for the rear extension (which is two storeys with sloping roof) to sit 2.9m below the ridge height of the listed building (which is single storey).
33. The rear extension will be largely screened in views from the east on High Street by the eastern flank of the listed building or from the street vegetation through most of the year. It will be glimpsed in views from the railway bridge to the west but not sufficiently to gain an impression of the built form. It would be apparent in views from passing trains. In the views that are available, it maintains a satisfactory streetscape presentation.
34. The materials used in the rear extension will be Reading red brick to match those of the listed building, which is supported. There is a significant amount of glazing and these windows have adopted a proportion and form that successfully mirrors the windows to the eastern elevation of the listed building. Further material samples are required by Condition 10.
35. Externally, there would be improvements to the external spaces around the building, including demolition of the disused toilet block. This would provide benefits in terms of visual amenity and improvements to the character of the conservation area. Some excavation will be required to accommodate the rear extension but the alteration to the landform is acceptable.
36. On the basis of the above, the proposal achieves a satisfactory impression on the character of the area, including the conservation area, and is supported.

Accessibility (incorporating The Public Sector Equality Duty (Equality Act 2010))

37. In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief.
38. Policy CP2 of the Core Strategy also seeks to ensure that new development contributes to the provision of sustainable and inclusive communities, including for aged persons, children and the disabled. NR17 of the Borough Design Guide requires that new public areas be easily accessible.
39. The main entrance will be from Polehampton Close, with level paved access leading to the proposed extension at the rear of the listed building, which is part of Phase 1. The rear extension is marginally lower than the finished floor level of the listed building and a passenger lift allows disabled access between the two levels. Internally, the layout is open plan and a disabled toilet is provided. Full accessibility appears to be assured as part of Phase 1.
40. The two storey rear extension is accessible from Polehampton Close via new ramped access or from the carpark, which connects with the extension. There is a level threshold from the paved courtyard to the community space on the ground floor. The café on the first floor has level access from phase 1. An accessible toilet and open plan layout is proposed within both levels. This indicates that full accessibility is assured within Phase 2.
41. There is no internal access between the two floors of the rear extension in Phase 2. However, the spaces are intended to operate independent of each other, with the café complementing the library and the community space operating as a separate entity. On this basis, there is no real impediment to access, particularly when there is still wheelchair access within 60m via the proposed ramp and existing footpath on Polehampton Close.
42. There are blue badge parking spaces at the entrance to the car park, but these are located at the far end of the car park. The Council's Highways Officer has indicated a preference to locate the blue badge parking closer to the facility but because the spaces are outside of the red line site area and as there is no additional car parking proposed within the subject application, this will need to occur independently of the planning application. Informative 4 notes as such.
43. Based on the above, the proposal accords with Policy CP2 and there is no indication or evidence that persons with protected characteristics as identified by the Act have or will have different needs, experiences, issues and priorities and there would be no significant adverse impacts as a result of the development.

Residential Amenities

44. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact.

45. The development adjoins two residential flats within the school house building to the north. There is potential for some limited amenity impacts arising from the reactivation of the vacant building and from increased footfall visiting the library, including children. However, the change of use is within the same Class F use class from a hall to a library, which are both similar in nature, although a library use is typically a low intensity use such that the effects will be minimal and not unexpected within a town centre location.
46. The use of the cafe and community space (and associated plant) at the rear of the building (as part of Phase 2) will generate increased noise levels. However, the site is generally well removed from residential properties, with a railway line to the west, car park to the south and substation and pub to the east and it also experiences a degree of background noise of the pub and passing traffic. Even so, it is still appropriate to control the opening and delivery hours for the cafe in Condition 31 (7am and 11pm on Monday to Saturday and 9am-8pm on Sundays and bank holidays) and in doing so, the level of impact to surrounding properties is acceptable. It is not considered necessary to control the hours of operation in the other parts of the building given their uses and location.
47. The Council's Environmental Health Officer has also requested ventilation details for the café in Condition 22.
48. In terms of any impacts upon access to light, privacy and a sense of enclosure, the rear extension is approximately 35m from the nearest residential boundary (with the exception of the two flats on High Street) and this is ample separation to prevent any adverse impacts for existing residents.

Access and Movement

Parking

49. Policy CC07 and Appendix 2 of the MDD Local Plan stipulates minimum off street car parking standards (including provision for charging facilities) and cycle parking standards.
50. The car parking standard for a hall is one space per 20m² of floorspace and one space per 30m² for a library. The café use has a parking standard of one space per 5m². With an additional 253m² of floorspace, there is a net increase in the parking generation rate of 22 spaces.

	Existing		Proposed	
	Floorspace	Requirement	Floorspace	Requirement
Hall	123m ²	6.15 spaces	125m ²	6.25 spaces
Library	0m ²	0 spaces	173m ²	5.75 spaces
Café	0m ²	0 spaces	78m ²	15.6 spaces
Total	123m ²	6 spaces	376m ²	28 spaces

51. It is proposed that a staff car parking space will be maintained off the High Street entrance. Two other spaces will remain available for the two existing flats. There is also a public car park to the rear of the site for long and short stay, comprising 41 spaces (including 4 disabled bays), with one of the spaces lost to the extension in Phase 2. There are existing parking restrictions on Polehampton Close which ensures that there is no indiscriminate parking along the road.

52. As identified in the transport statement, many customers are likely to visit the library as part of a linked trip to the town centre. It is also worth noting that the use with the café (which has highest parking generation rate) is intended as a supplementary/ ancillary aspect of the library and it will also predominantly function as a linked trip as part of a visit to the library.
53. The site is well located within the town centre designation, being 200m from the centre of Twyford, within walking distance of a large part of Twyford and readily accessible by public transport, being located less than 250m from bus stops with regular buses to Reading and Wokingham and 450m from Twyford railway station. The proximity of the car park to the rear of the site to serve the scheme is a significant benefit in terms of providing parking for the scheme were visitors to require it.
54. The transport statement concludes that there is capacity within Polehampton Close to accommodate the local car parking demands and although the proposed extension would reduce the capacity of the existing public car park, this would not have a severe impact on the local highway network. The Council's Highways Officer does not disagree and raises no objection subject to conditions to secure the on-site vehicle parking.
55. The MDD Local Plan specifies one long term (staff) and one short term (visitor) cycle space per five library staff. One long term (staff) space as well as one short term (visitor) space for 15 visitors is required for the community use and one space is required for the café use. In the absence of final staff details, it is estimated that it amounts to approximately six spaces.
56. A total of seven cycle spaces are provided and these are spread across three areas and the two phases of the development. They appear as stands on the submitted plans, indicating that they are likely to be able to be used by up to 14 bicycles. On this basis, the Council's Highways Officer raises no objection.
57. P2 and P3 of the Borough Design Guide SPD require that the spaces are conveniently located (which they are). However, they should also be secure, undercover and compatible in the streetscene and there are no further details of surfacing or enclosures and because of the sensitivity of the listed building, the details are secured by Condition 17.
58. A rapid charge EV parking space is provided in the north western corner of the public car park (within the red line). It represents 2.5% of the total spaces in the car park. The installation is proposed with the agreement of the Council and details of the charging unit are secured by Condition 18.

Traffic

59. The transport statement dates from 2016 in support of a previous application but the Council's Highways Officer has accepted the findings. It is still concluded that traffic from this development would not have an adverse impact on the highway network.
60. Delivery arrangements for the library can be accommodated within the existing courtyard to the north of the listed building. Food deliveries for the café are likely to

be accommodated within Polehampton Close or within the existing car park. In both cases, there is unlikely to be any impediment to existing traffic flows and the arrangement is workable.

61. Due to the site location on High Street, limitations posed by the listed status of the building and the excavation of soil, a Construction Management Plan is required in Condition 16.

Landscaping and Trees

62. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.
63. The site is characterised by its distinct change of topography, the railway line and trees, particularly those to the western and eastern edges of the site. This is illustrated in the Tree Condition Survey showing a good selection of 'A' and 'B' quality London Planes, Sycamore, Oaks and Tree of Heaven. The mature and broad canopies of the London Planes are suppressing nearby Oaks and the Tree of Heaven, but regular and sustained maintenance may help in this respect. Beyond the site further to the south and the Volunteer Centre are more mature Oak trees.

Phase 1

64. Phase 1 allows for the retention of the 11m high T1 Sycamore alongside the Network Rail boundary. The contours around it will be altered with the extension of the retaining wall and changes to the stairs but this allows for increased soft landscaping and continued growth of the tree. As the rooting area is currently buried beneath hard standing, an arboricultural watching brief would be required in Condition 6 prior to the commencement of Phase 1.
65. The proposed technical measures to retain and enhance the rooting environment is noted as is the existing stepped retaining wall to the step risers being kept in situ. The addition of bow top fencing is not supported as it is a distracting feature and would benefit from a more contemporary design that complements the historic brickwork. Further discussion is required in Condition 8.
66. The provision of an outdoor children's space along the western boundary is supported and it is noted that Network Rail have given conditional support. Despite constraints from the proximity of the railway, the narrowness of the space and the shading from the T1 Sycamore, it still presents as a desirable space. Hard and soft landscaping details, which includes outdoor furniture and other treatments, are required in Condition 5. It would need to incorporate soft surfacing rather than paving and could feasibly involve a boundary treatment to screen the utilitarian character of the safety fencing.

Phase 2

67. Phase 2 will require the removal of four trees –T1, T2 (12m high Oak), T4 (14m high Ash) and T5 (9m high Sycamore). The Council's Trees and Landscape Officer does not oppose the removal of T2, T3 and T5, all of which are Category C trees that

provide limited contribution, particularly when replacement planting is proposed. However, there are questions raised about the loss of T1.

68. There is agreement that the tree is a B1 category tree but the Council's Landscape Officer has concluded that the tree is displaying signs of a veteran tree i.e. one that is worthy of retention. It has shown sizeable growth since its last survey as part of the 2015 application and it shows signs of cavities, bark damage and rot holes (roosting feature for bats) and thus meets the criteria which is features of biological, cultural or aesthetic value that are characteristic of, but not exclusive to, individuals surviving beyond the typical age range for the species concerned.
69. The initial recommendation from the Council's Trees Officer was that the loss of the tree was unacceptable because of its growth and habitat potential. The assessment of the previous application in 2015 also proposed the removal of the Sycamore and this was not raised as an issue at the time. The change in stance since then is due to further investigations into the tree, the continued growth of the tree and the increased importance provided to trees in the 2019 version of the NPPF.
70. The applicant's arborist has submitted a further arboricultural report which fully considers the importance of the tree and the overriding reasons that justify its removal. This has been reviewed by the Landscape Officer, who concludes that it is regrettable that the Sycamore will be lost as part of the phase 2 works; however, overall, it is considered that the loss of the tree is not fatal to the application because there are exceptional reasons and suitable compensation.
71. In terms of any exceptional reasons or suitable compensation, the relocation of the two storey extension closer to Polehampton Close could feasibly allow for the retention of the tree. However, the building will still block some views of the tree, still intrude into the 10.7m root protection area and substantially reduce views of the listed building from Polehampton Close, and this is therefore not considered to be a practical solution. There will be replacement tree, and opportunities for landscaping can be increased as part of a consolidated hard and soft landscaping scheme in Condition 5. A management plan is also required in Condition 7 to ensure ongoing maintenance.

Ecology

72. Policy CP7 of the Core Strategy states that sites designated as of importance for nature conservation are to be conserved and enhanced and inappropriate development will be resisted. Policy TB23 of the MDD Local Plan requires the incorporation of new biodiversity features, buffers between habitats and species of importance and integration with the wider green infrastructure network.
73. The site is located in habitat where bat roosts have previously been found and is surrounded by habitat suitable for use by foraging and commuting bats as it is adjacent to the wooded railway corridor.
74. A bat survey report was submitted with the planning application. It summarises the results of activity surveys for bats recommended in the earlier preliminary roost assessment dated July 2020. The activity results indicate that the listed building is used as a day roost for a low number of two of the more common species of bat.

The toilet block is also identified as a day roost of a common species of bat and this is proposed for demolition in Phase 2.

75. The proposed work would require a derogation licence from Natural England and the Council's Ecology Officer agrees that the current species and status of the roosts identified mean that the site could be covered through a conventional bat licence. In terms of mitigation, the provision of external bat boxes (Condition 13) appears sufficient to be granted a licence by Natural England and the proposal is satisfactory in terms of ecological viability.
76. The proposal seeks to remove all access for bats to the roof of the listed building during the reroofing works as this space will be occupied by energy saving insulation. As compensation roost features, external bat boxes have been proposed to work with the existing features of the building. The Council's Ecology Officer considers there to be sufficient scope to provide compensation roost features either within the roof voids using appropriate access features, or in external bat boxes but that the final detail can be secured through a pre-commencement condition. This element is covered as a pre-commencement requirement in Condition 13.
77. The ecological surveys, most recently updated in December 2020, have recognised that a bat roost is at risk of destruction during Phase 2. The mitigation strategy includes relevant measures and compensation roosts to be provided within this phase of the development. The Council's Ecology Officer considers there to be sufficient scope to provide compensation roost features within the proposed extension for Phase 2. Condition 12 secures the mitigation through the submission of licence details for each phase of the development and Condition 26 requires compliance with the other measures of the ecology report.
78. Subject to the above requirements, the Council's Ecology Officer raises no objection.

Secure by Design

79. Phase 1 includes a secure children's activity area on the western side of the site. It will be enclosed by fencing to a height of 2m along the Network Rail boundary and by marginally lower fencing to the front and rear. It will be a secure space and poses limited opportunities for anti-social behaviour. The existing toilet block on the eastern side of the site will be retained and paving will be incorporated around it, resulting in a more inviting relationship to Polehampton Close. It improves the appearance and surveillance across the site and is supported.
80. The Phase 2 building design extends the development southwards toward the car park, but also westwards where it creates a staggered edge and a dead space between the railway line and the building. Some windows overlook the space but the residual space is very narrow and could attract misuse and anti-social behaviour. However, as it adjoins Network Rail land and will need to be fenced to their specifications, the likelihood of this occurring is reduced and it is concluded that the potential impacts are not sufficient to warrant variation to the scheme.

Building Sustainability

81. Policy CC04 of the MDD Local Plan and the Sustainable Design and Construction SPD require sustainable design and conservation. Solar panels were initially

proposed to the roof of the listed building. As they have now been removed from the proposal on heritage grounds, there are no other specific details (such as building materials, energy efficient appliances or water saving devices) that would commit to the building being considered as energy efficient. In their absence, these details are required by pre commencement Condition 20. Notwithstanding, it is feasible that the solar panels could be accommodated within phase 2 of the development, which does not include works to the listed building.

Waste Storage

82. Policy CC04 of the MDD Local Plan requires adequate internal and external storage for the segregation of waste and recycling and an appropriate area for ease of collection.
83. The community hall, café and library uses have varying levels of waste generation, including from food waste from the cafe. Whilst the carpark at the rear of the site provides for convenient refuse collection, there are no details relating to how waste, recycling and food waste will be stored and collected. Indeed, to account for food waste from the café (in phase 2), it may be beneficial to include composting facilities within the courtyard of phase 1, which would provide an educational tool for the children's outdoor activity area. These details are required as part of each phase of the development in Conditions 5 and 21.

Flooding and Drainage

84. Policy CC09 of the MDD Local Plan requires consideration of flood risk from historic flooding. The site and access thereto is located within Flood Zone 1 and the proposal represents no additional flood risk or vulnerability. It is therefore acceptable in terms of Policy CC09.
85. Policy CC10 of the MDD Local Plan requires sustainable drainage methods and the minimisation of surface water flow. There will be increase in impermeable area and modest changes in ground levels, which pose implications for the existing drainage management. The Council's Drainage Officer raises no in principle objection but in the absence of drainage details, Condition 14 requires further details prior to the commencement of the development.
86. The site is near to Network Rail's boundary and there shall be no soakaway or drainage features within 5m distance of the western boundary. Stormwater discharge to the Network rail drain is also prohibited without their consent.

Site Contamination

87. The Council's Environmental Health Officer has recommended Condition 19 to address potential contamination and any required remediation.

Employment Skills

88. There is no requirement for an employment skills plan.

Relationship to Railway Corridor

89. The Twyford-Henley railway corridor adjoins to the west. Network Rail has raised no objection in principle but to ensure that there is no adverse impact upon the safety, operation and integrity of the operational railway, the following comments are noted:

- a) The position of the children's reading area and benches may result in a risk of people climbing onto the boundary wall and fence and potentially falling down onto the railway
- b) It is recommended no trees are planted closer than 1.5 times their mature height to the boundary fence.

Officer comment: This is to be addressed as part of the hard and soft landscaping details in Condition 5.

- c) A suitable trespass proof fence (to at least 1.8m high or 2m alongside the children's activity area) is required adjacent to the boundary.

Officer comment: This is incorporated into the boundary treatment details that are required in Condition 8.

- d) It is recommended that the building be at least 2m from the fence to allow for maintenance

Officer comment: The rear extension in Phase 2 varies from 0.3m-2.0m from the boundary and, on average, about 1.8m from the boundary. It is not anticipated that maintenance access along this side of the building would be impeded as a result.

- e) Where trees exist on Network Rail, land the design of foundations close to the boundary must take into account the effects of root penetration

Officer comment: T3 (Oak) is within the rail corridor but the arboricultural impact assessment indicates that there is unlikely to be any adverse harm to the root protection area. Condition 6 requires additional details relating to tree protection, although it will remain protected by existing fencing.

- f) Demolition works must be carried out in accordance with the agreed method statement so that they do not endanger the safe operation or stability of the railway

Officer comment: This is incorporated into the requirement for a construction management plan in Condition 16 which will be consulted to Network Rail.

90. Other informatives relate drainage, tree planting and scaffolding, as noted in Informative 3.

Community Infrastructure Levy

91. The application is not liable for CIL payments because it involves floorspace to be used for retail purposes (café) within a town centre location or a community facility (library and community space), both of which are exempt from liability.

CONCLUSION

92. The proposal would provide positive benefits to the community in the provision of community facilities and the re-use and enhancement of a listed building. The improvements to the external space, including demolition of the disused toilet block would also provide benefits in terms of visual amenity and would enhance the character of the conservation area.
93. The site is situated within a sustainable location and proposes uses compatible with the site context. There is adequate parking within the vicinity of the site and the town centre to address the parking demand arising from the development.
94. The proposal would result in less than substantial harm to the listed building and the loss of a mature Sycamore that provides expansive canopy coverage is regrettable. However, as discussed above, the harm is adequately managed and the loss of the Sycamore is recognised as unavoidable and adequately compensated for. Subject to a series of conditions, the public benefits would outweigh the identified harm and as such it is considered that planning permission and listed building consent should be granted.

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PLANNING REF : 201022
PROPERTY ADDRESS : PO Box 8250
: Reading
: RG6 9SZ
SUBMITTED BY : Twyford Parish Council
DATE SUBMITTED : 01/07/2020

COMMENTS:

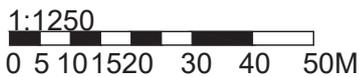
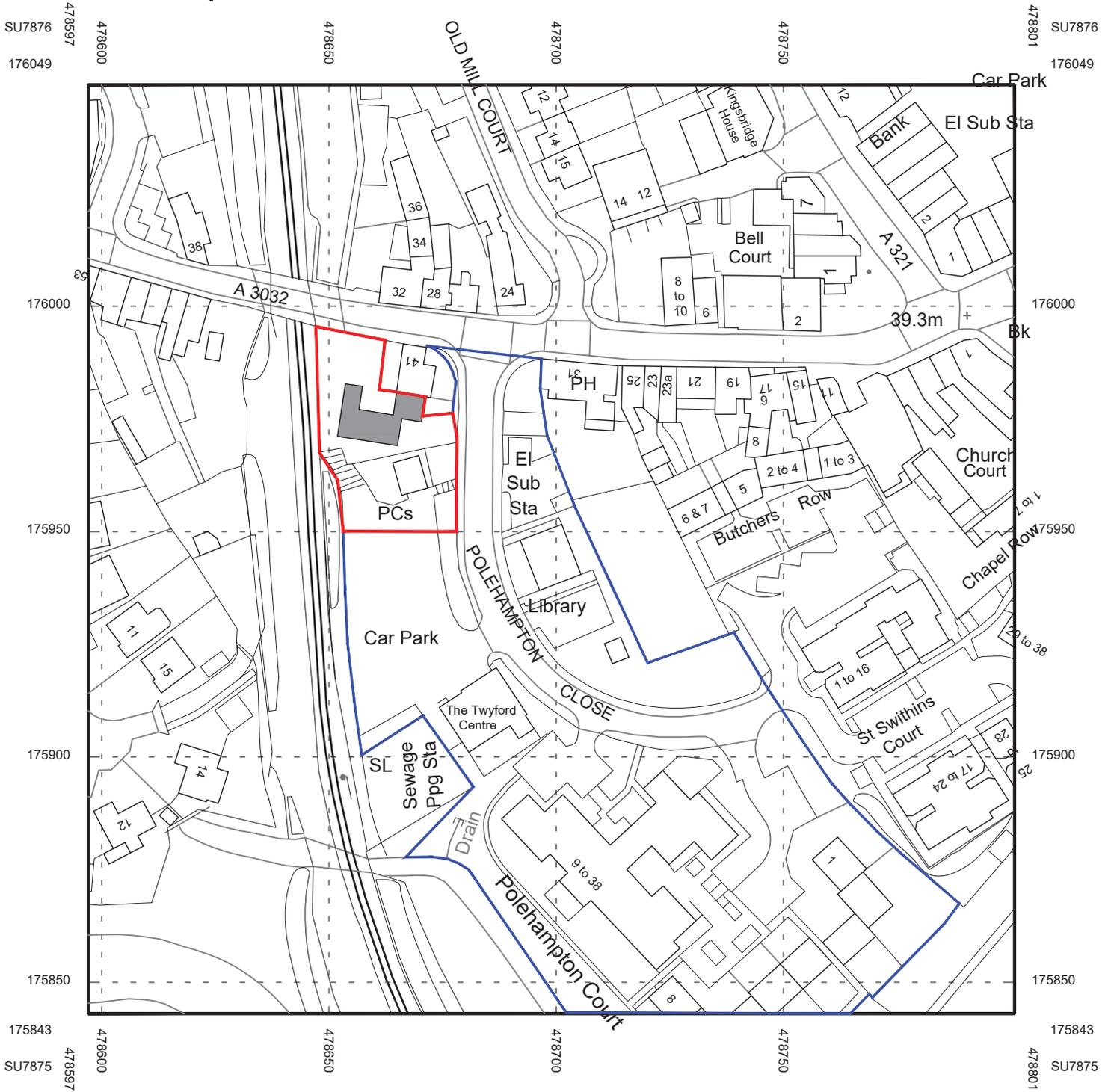
The Committee supports the application however, they wish to see better provision for secure cycle, pushchair & mobility scooter parking so that there is easier access to both the front & back of the development & to encourage local visitors.

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MapServe

Disclaimer Notes:

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Client:	Wokingham Borough Council	A Rev	30/11/20	Blue line added	JW	JW
Project:	The Old Boys School, Twyford	Date	Feb 2020		By	Checked
Title:	Location Plan	Scale:	1:1250@A4			
Drawn by	SM	Drawing Ref:	200009 - A001 Rev A			



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Rev	Date	Description	By	Checked
F	30/11/20	PV Panels Removed Staff parking Red Line Amended	JW	JW
E	18/9/20	Rooflight amended	JW	JW
D	15/09/20	Rear garden annotated Link and roof light amended Staff cycle parking added Staff car parking space indicated Flat top fencing	JW	JW
C	23/06/20	Bow top fencing added Hoops added to front of bldg Cycle store removed	JW	JW
B	26/05/20	Retaining wall reinstated in original location and works around T1 confirmed	JW	JW
A	17/03/20	Demolition Lines Removed	SM	JW

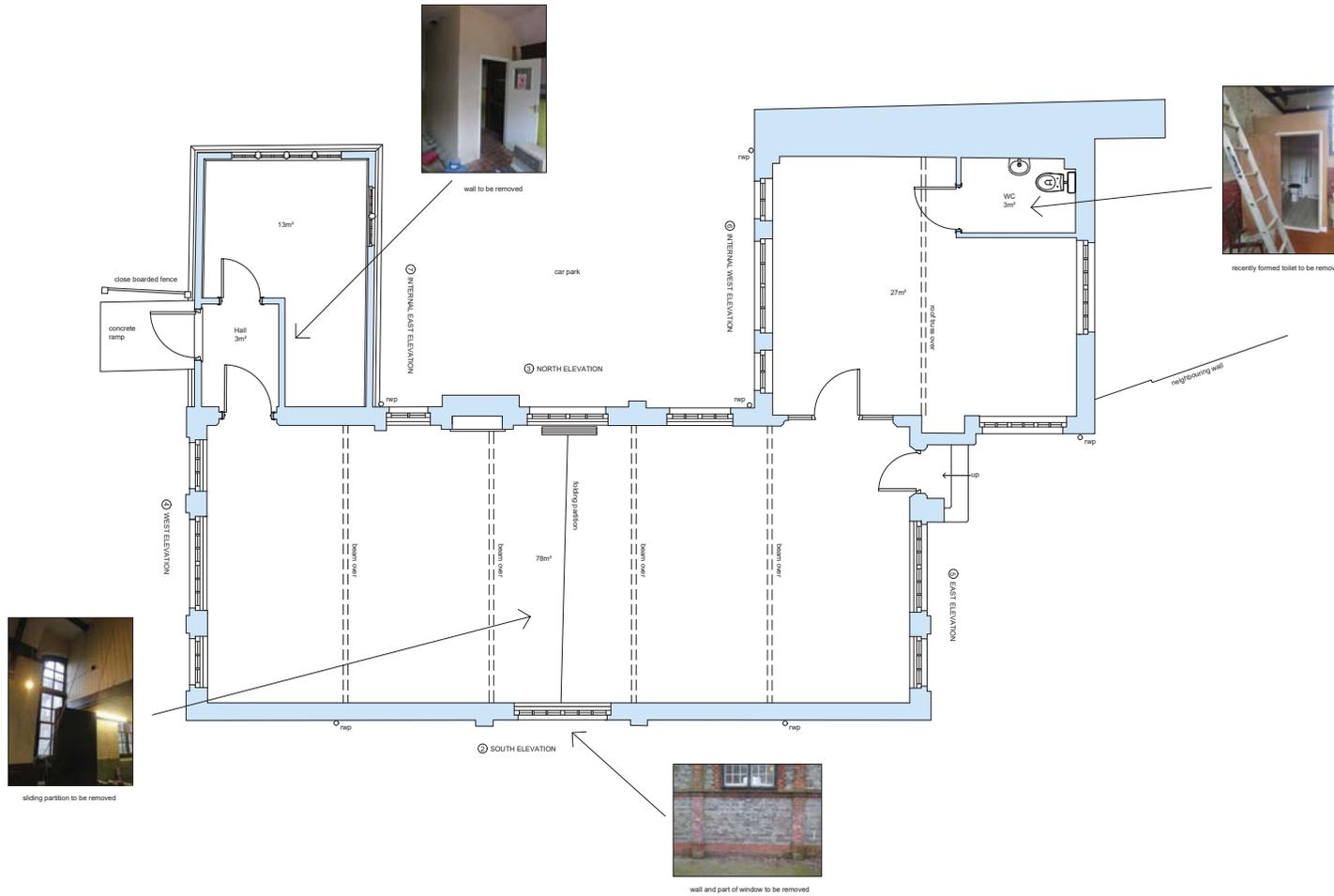


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Client:	Wokingham Borough Council
Project:	The Old Boys School, Twyford
Title:	Proposed Block Plan - Phase 1
Drawn by:	SM
Date:	February 2020
Scale:	1:200@A3
Drawing Ref:	200009 - A010f

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Rev	Date	Description	By	Checked
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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

Title: Existing Floor Plan

Drawn by: SM

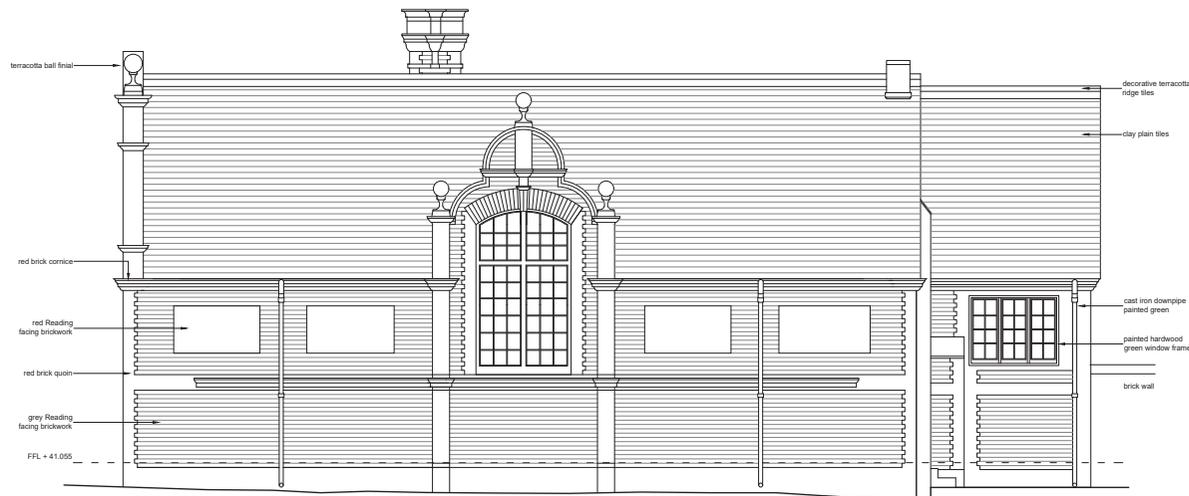
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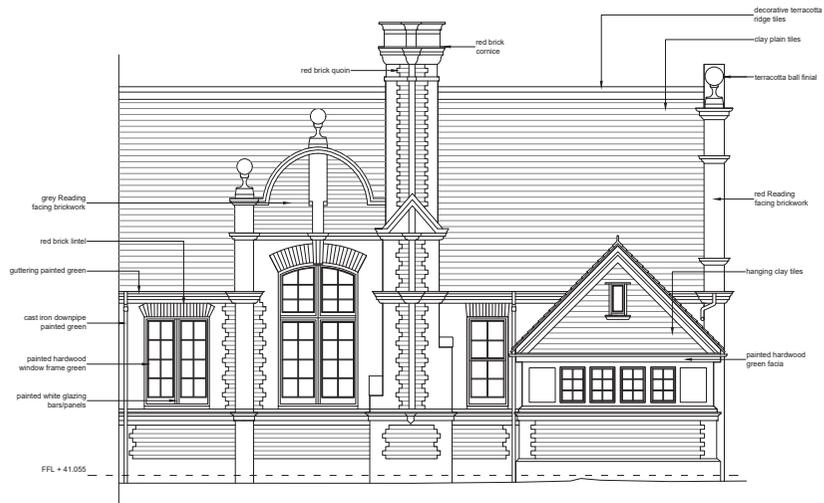
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⊙ SOUTH ELEVATION

146



⊙ NORTH ELEVATION



⊙ WEST ELEVATION

Rev	Date	Description	By	Checked
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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

Title: Existing Elevations

Drawn by SM

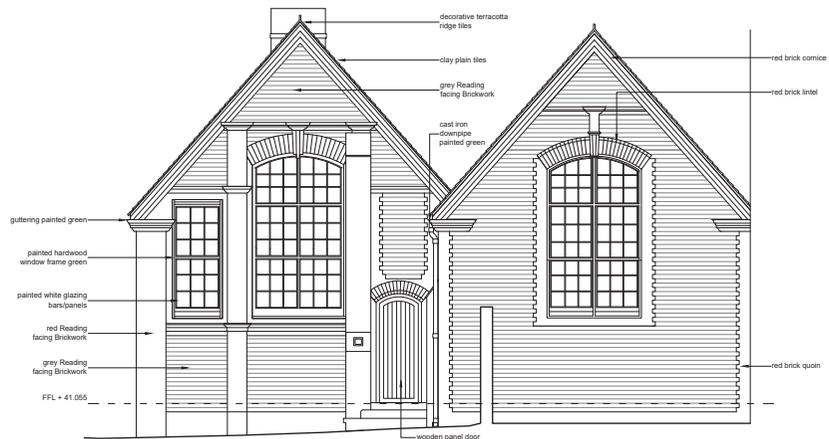
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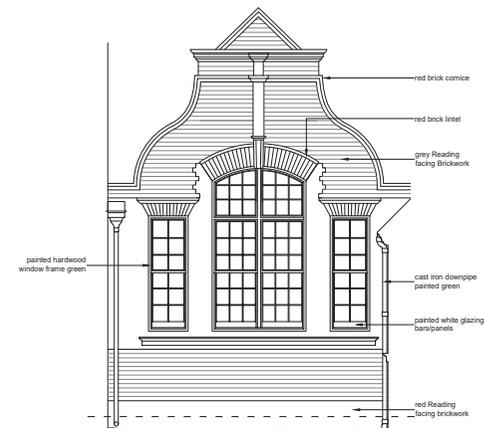
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④ EAST ELEVATION



④ INTERNAL WEST ELEVATION



④ INTERNAL EAST ELEVATION



147

Rev	Date	Description	By	Checked
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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

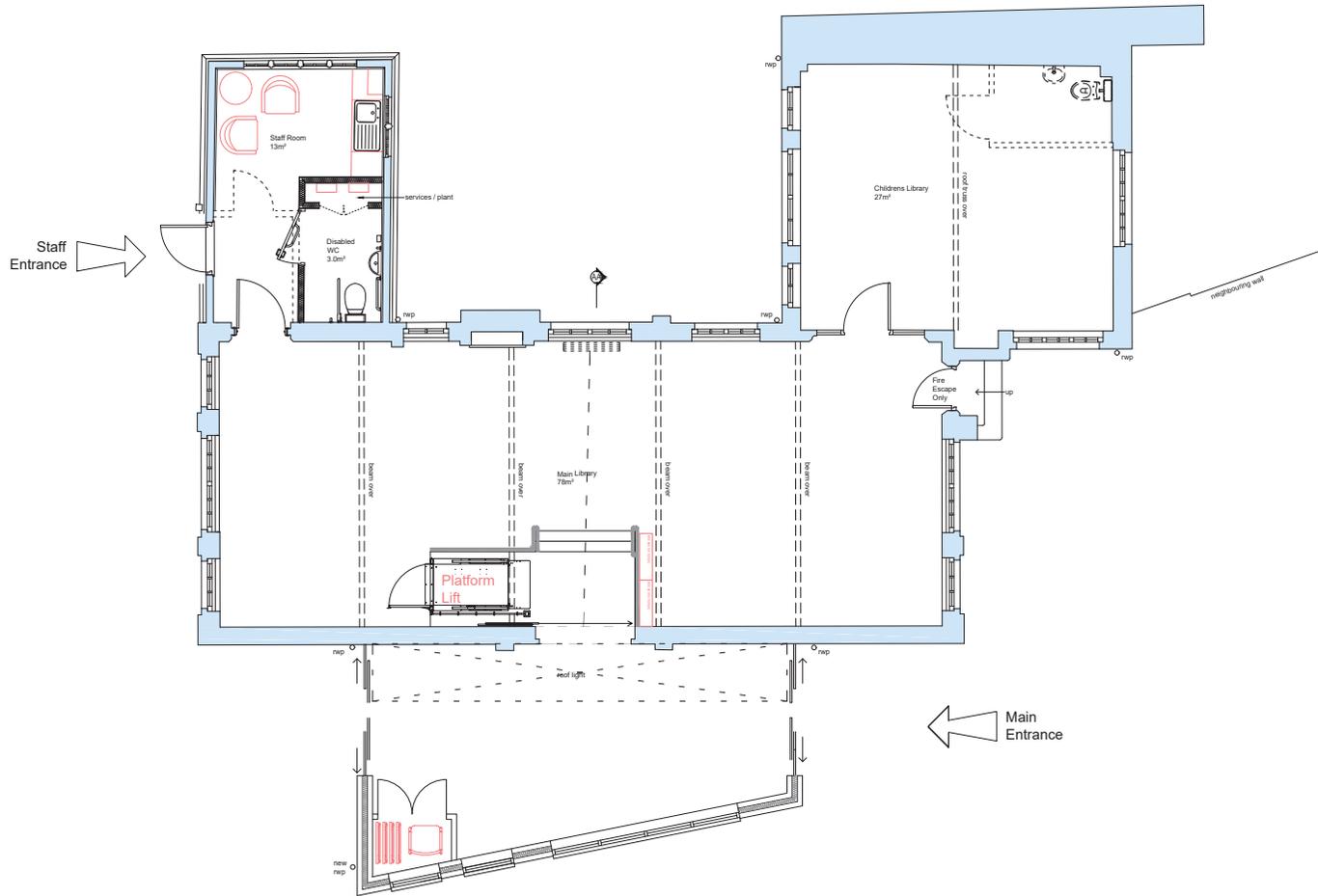
Title: Existing Elevations

Drawn by: SM

Date: February 2020

Scale: 1:50@A1

Drawing Ref: 200009 - A005



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Rev	Date	Description	By	Checked
E	30/11/20	Windows to South elev of link	JW	JW
D	18/9/20	Rooflight amended	JW	JW
C	16/09/20	New roof light position indicated Platform lift amended	SM	JW
B	23/06/20	East door swing left as original	JW	JW
A	17/03/20	Demolition Lines Removed	SM	JW



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Client:	Wokingham Borough Council
Project:	The Old Boys School, Twyford
Title:	Phase 1 - Floor Plan
Drawn by	SM
Date	February 2020
Scale:	1:50@A1
Drawing Ref:	200009 - A011d

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EAST ELEVATION



WEST ELEVATION

Rev	Date	Description	By	Checked
D	19/1/21	Door swing annotation removed	JW	JW
C	18/9/20	Rooflight amended	JW	JW
B	16/09/20	Elevations updated	SM	JW
A	23/06/20	East door swing left as original	JW	JW



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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

Title: Phase 1 - Elevations

Drawn by SM

Date: February 2020

Scale: 1:50@A1

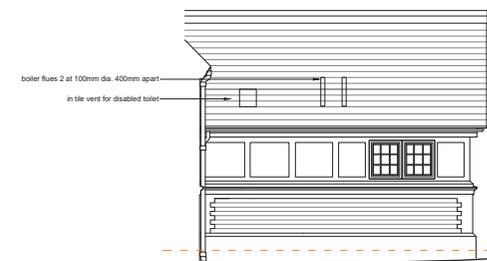
Drawing Ref: 200009 - A012d

Disclaimer Notes:

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© SOUTH ELEVATION



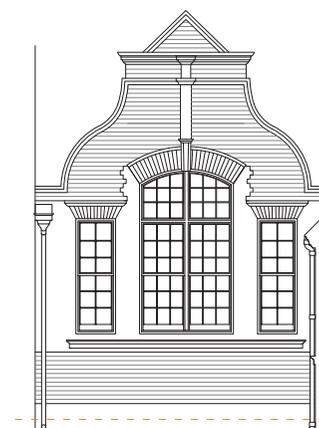
INTERNAL EAST ELEVATION



150



© NORTH ELEVATION



© INTERNAL WEST ELEVATION

Rev	Date	Description	By	Checked
D	19/1/21	PV Removed	JW	JW
C	30/11/20	N & Int W Eleivs added	JW	JW
B	18/9/20	Rooflight amended	JW	JW
A	16/09/20	Elevations updated	SM	JW



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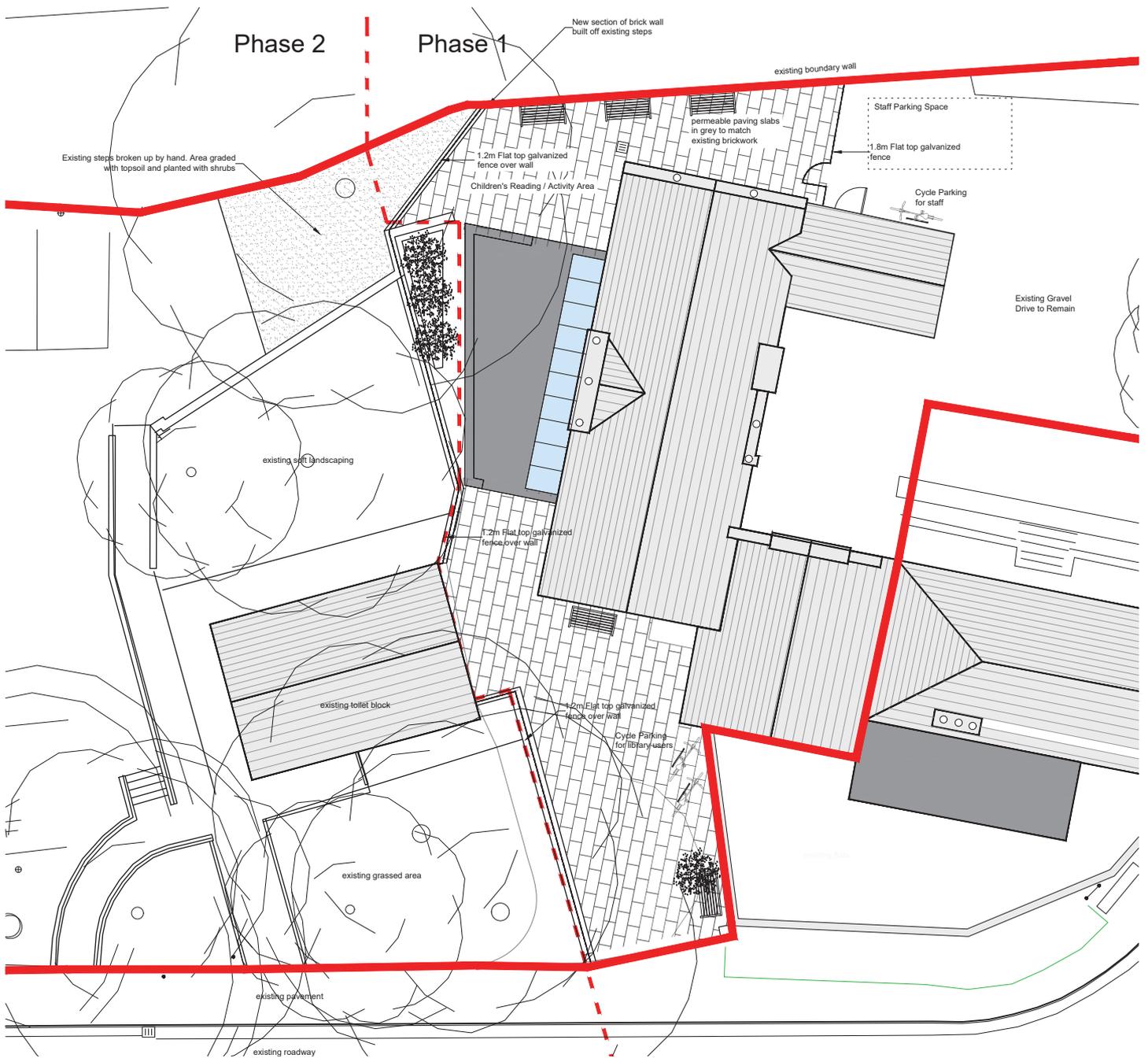
Drawn by: SM

Date: February 2020

Scale: 1:50@A1

Drawing Ref: 200009 - A013d

Disclaimer Notes:
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Galvanized Flat Bar Fencing
40x8mm profile bars and rails



Marshall's Priora Conservation
Permeable Paving



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Client: Wokingham Borough Council
 Project: The 504 Boys School, Twyford
 Title: Phase 1 - External Landscaping
 Drawn by: SM

Rev	Date	Description	By	Checked
D	30/11/20	PV Panels Removed Railings indicated Staff Parking	JW	JW
C	18/09/20	Rooflight amended Link and roof light amended Staff cycle parking added Staff car parking space indicated	JW	JW
B	15/09/20	Flat Bar Fencing Bow top fencing Cycle store removed. Hoops added.	JW	JW
A	23/06/20	Paving amended	JW	JW

Date: Feb 2020
 Scale: 1:100@A2
 Drawing Ref: 200009 - A014d

railway line

existing boundary wall

permeable paving slabs
in grey to match
existing brickwork

children's Reading / Activity Area

Staff Parking Space
**Library Staff
Parking Area**

1.8m Flat top galvanized
fence

Cycle Parking
for staff

Turning / Access
Area for Cars

Existing Gravel
Drive to Remain

Existing informal parking
for flats to continue as
existing

152

permeable paving slabs
in grey to match
existing brickwork

1.2m Flat top galvanized
fence over wall

Disclaimer Notes:
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KEEP CLEAR

Rev	Date	Description	By	Checked



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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

Title: Library Staff Parking

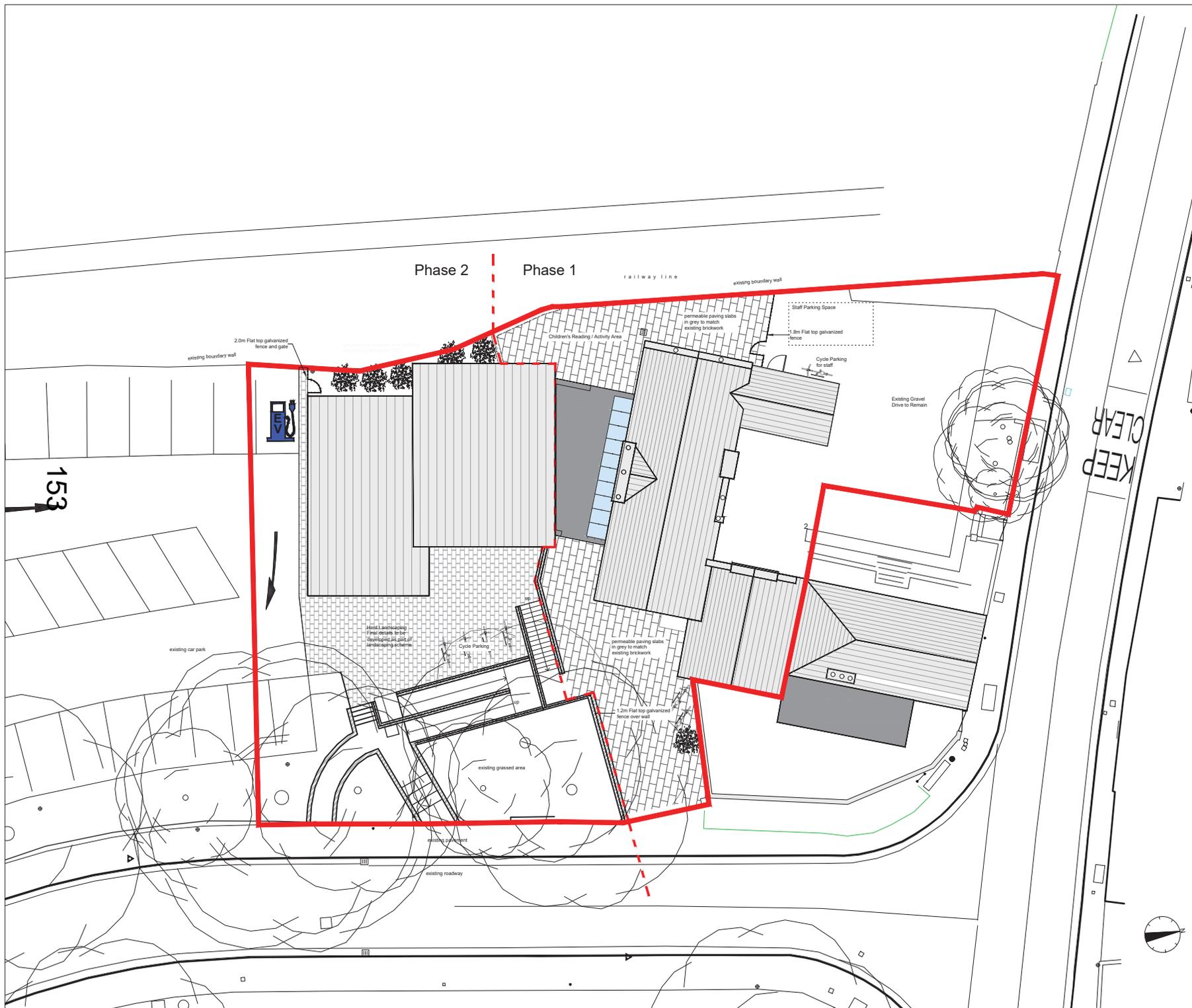
Drawn by: SM

Date: 29/1/21

Scale: 1:100@A3

Drawing Ref: 200009 - A030





Disclaimer Notes:
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Rev	Date	Description	By	Checked
		PV Panels Removed		
		Gate and fence to rear		
G	30/11/20	Red Line Amended	JW	JW
F	18/9/20	Rooflight amended	JW	JW
		Rear garden annotated		
		Link and roof light amended		
		Cycle parking added		
		EV Point added		
		Planting to North of new building		
E	15/9/20	Flat top fencing	JW	JW
		Bow top fencing		
D	23/6/20	Cycle store swapped for hoops	JW	JW
		Retaining wall reinstated in original location		
C	22/5/20	Retaining wall reinstated in original location	JW	JW
B	21/5/20	Path reconfigured	JW	JW
A	17/03/20	Demolition Lines Removed	SM	JW

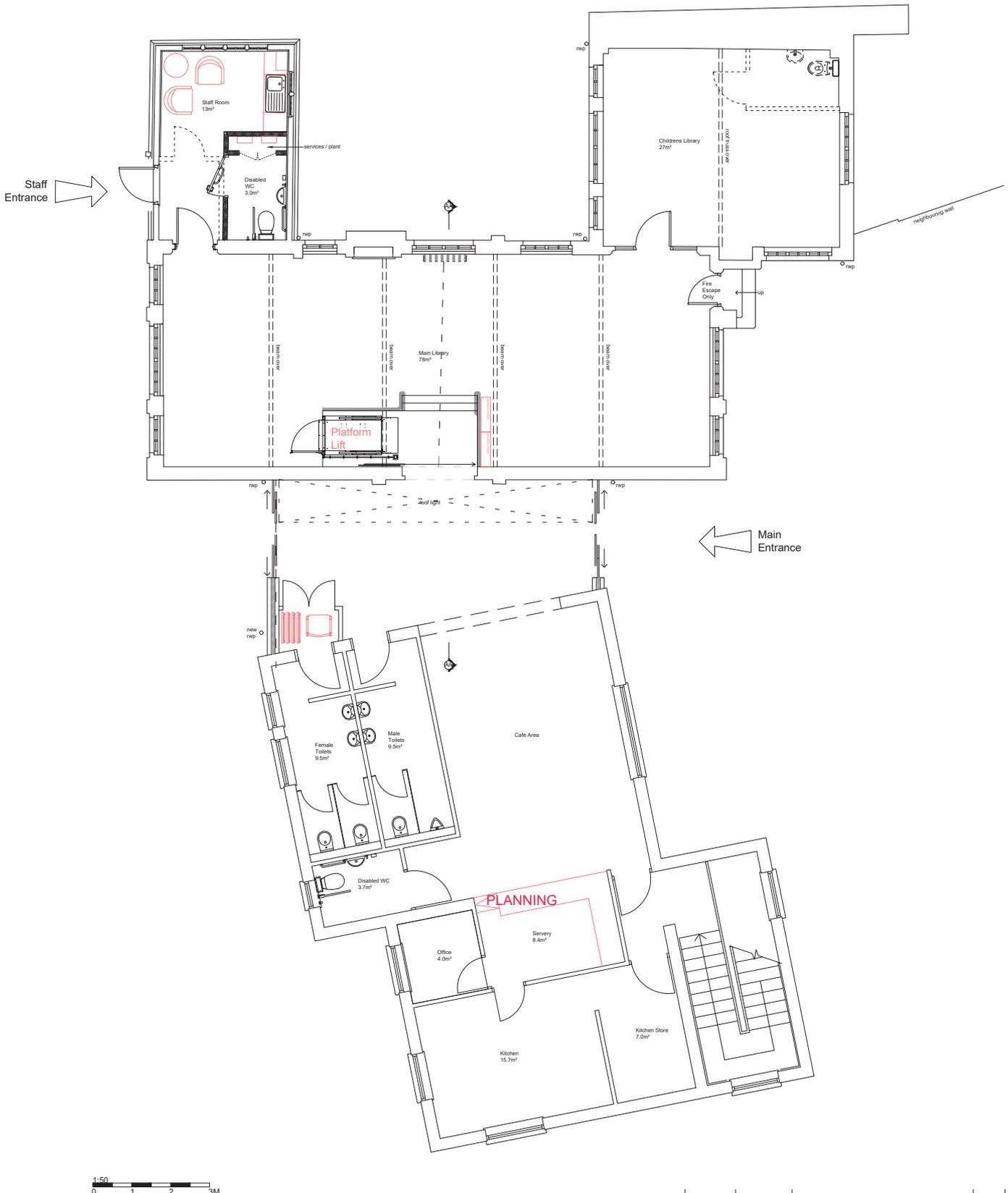


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Client:	Wokingham Borough Council
Project:	The Old Boys School, Twyford
Title:	Proposed Block Plan - Phase 2
Drawn by:	SM
Date:	February 2020
Scale:	1:200@A3
Drawing Ref:	200009 - A020g

Disclaimer Notes:

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1:50
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Client:	Wokingham Borough Council	D	18/9/20	Roof light amended	JW	JW
Project:	The Old Boys School, Twyford	C	16/09/20	Platform lift amended Phase 2 building amended	JW	JW
Title:	Phase 2 - Floor Plan	B	23/06/20	Link and roof light amended	JW	JW
Drawn by	SM	A	17/03/20	East door swing left as original	JW	JW
				Demolition Lines Removed	SM	JW
		Rev	Date	Description	By	Checked
		Date	Feb 2020			
		Scale:	1:50@A1			
		Drawing Ref:	200009 - A021c			

Disclaimer Notes:

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Client:	Wokingham Borough Council
Project:	The Old Boys School, Twyford
Title:	Phase 2 - Floor Plan (Lower)
Drawn by	SM

Rev	Date	Description	By	Checked
B	18/09/20	Rooflight Amended	JW	JW
A	16/09/20	Double doors omitted Accessible WC added	SM	JW
Date	Feb 2020			
Scale:	1:50@A3			
Drawing Ref:	200009 - A022b			

PLANNING

155

Disclaimer Notes:

- The Contractor must verify all dimensions on site
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PLANNING

B	18/09/20	Rooflight amended	JW	JW
A	16/09/20	Elevation updated	SM	JW
Rev	Date	Description	By	Checked



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Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

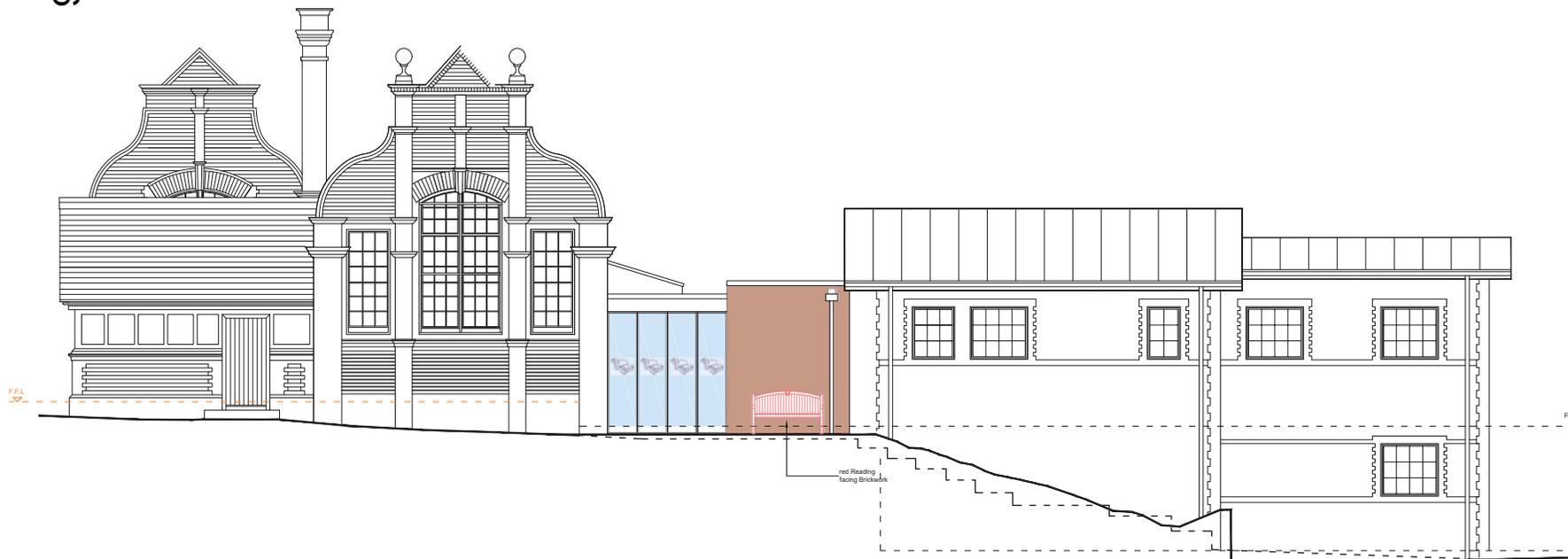
Title: Phase 2 - Elevations

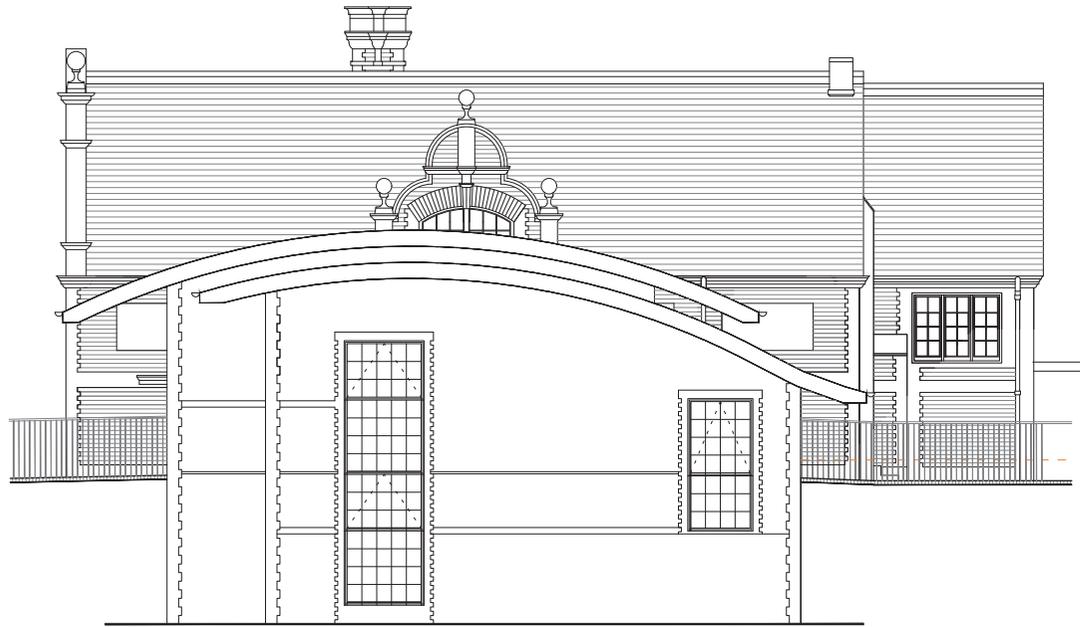
Drawn by: SM

Date: February 2020

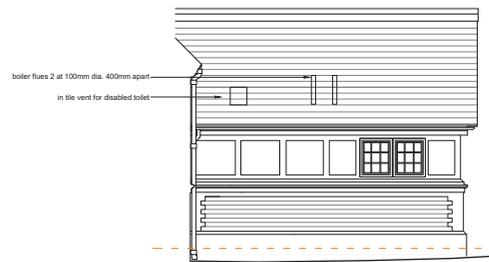
Scale: 1:50@A1

Drawing Ref: 200009 - A023b





SOUTH ELEVATION



INTERNAL EAST ELEVATION

Disclaimer Notes:

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- Do not rely on scaled dimensions from this drawing

C	19/1/21	PV Removed	JW	JW
B	18/09/20	Rooflight amended	JW	JW
A	16/09/20	Elevation updated	SM	JW
Rev	Date	Description	By	Checked



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 W. www.thamesvalleysurveying.co.uk

Client: Wokingham Borough Council

Project: The Old Boys School, Twyford

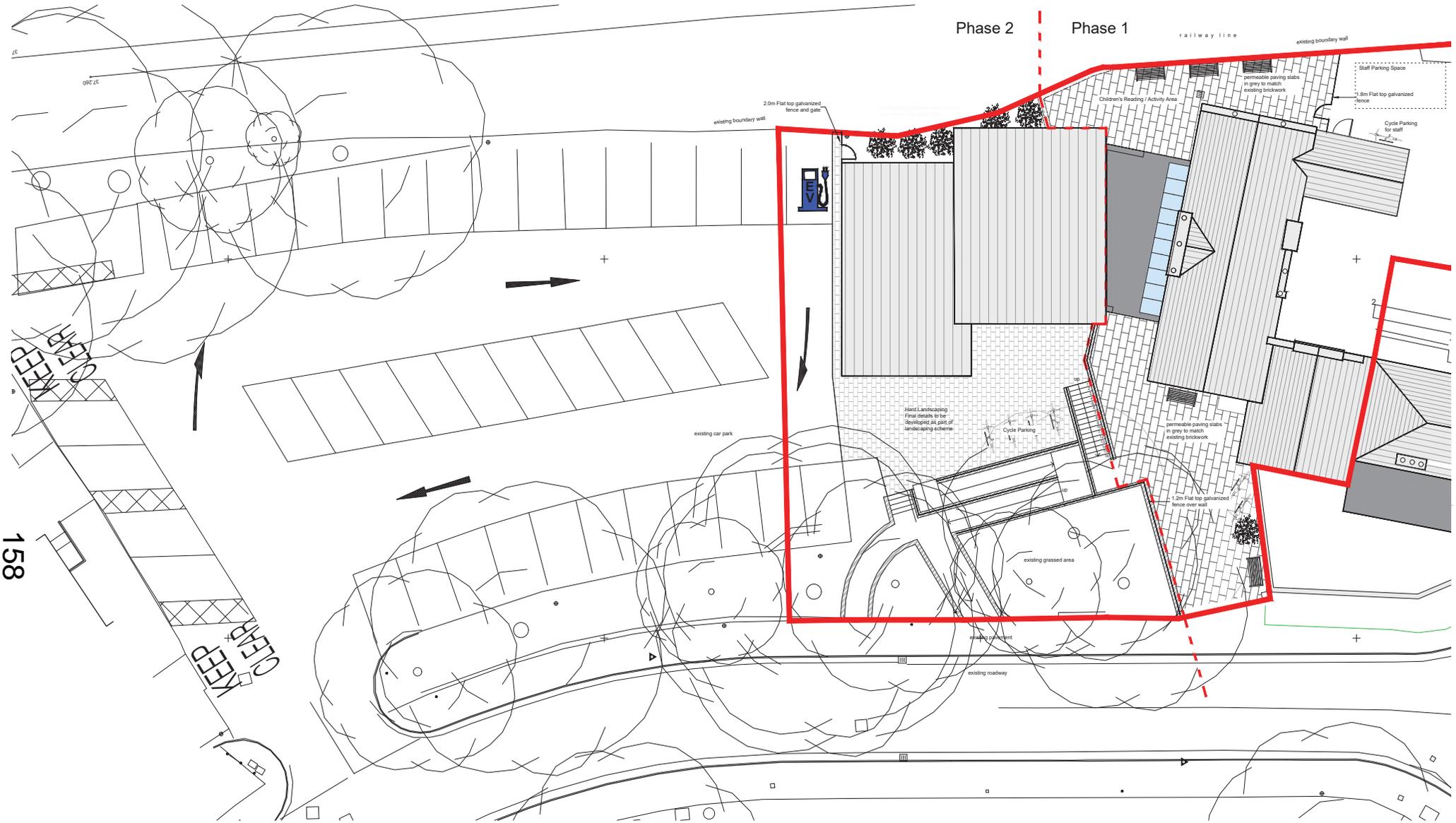
Title: Phase 2 - Elevations

Drawn by SM

Date February 2020

Scale: 1:50@A1

Drawing Ref: 200009 - A024c



158



Galvanized Flat Bar Fencing
40x8mm profile bars and rails



Marshalls Piora Conservation
Permeable Paving



Client:	Wokingham Borough Council
Project:	The Old Boys School, Twyford
Title:	Phase 2 - External Landscaping
Drawn by:	SM

Rev	Date	Description	By	Checked
D	30/11/20	PV Panels Removed Fence and gate to rear Railings Indicated Staff Parking	JW	JW
C	18/9/20	Red Line Added Rooflight amended	JW	JW
B	15/9/20	Cycle parking added EV Point added Defensive planting added Car park shown Flat top fencing Slow top fencing Cycle store removed. Hoops added	JW	JW
A	23/8/20	Paving amended	JW	JW

Date: Feb 2020

Scale: 1:100@A1

Drawing Ref: 200009 - A025d



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Agenda Item 56.

Application Number	Expiry Date	Parish	Ward
203439	12/03/2021	Winnersh	Winnersh;

Applicant	WBC Housing Services, Shute End, Wokingham, RG40 1BN
Site Address	36 and 39-48 Grovelands Park, Winnersh, RG41 5LD
Proposal	Full application for the proposed removal of 12 no. existing mobile homes and the erection of 11 no. two storey pre-fabricated temporary accommodation units consisting of one self-contained two- bedroom housing unit per floor (plots 39-48) and 1 no. single storey mobile house (plot 36).
Type	Full
Officer	Senjuti Manna
Reason for determination by committee	Applicant is Wokingham Borough Council's Housing Services.

FOR CONSIDERATION BY	Planning Committee on Wednesday, 10 February 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>Grovelands Park is a residential mobile home park owned and operated by Wokingham Borough Council. A part of the mobile home park is used by WBC Housing Services to provide temporary emergency accommodation to households, often including children or vulnerable people, while the household waits for permanent accommodation. As such, mobile home units in Grovelands Park not only perform an essential role in the provision of temporary emergency housing, but also its redevelopment would ensure a better environment and quality of accommodation whilst the occupants wait to be re-housed.</p> <p>The proposal includes removal of 12 existing mobile home units on 12 individual plots and replacing them with 23 two-bedroom units in the form of one single storey accessible unit and 11 two storey pre-fabricated units of one self-contained residential accommodation on each floor. Whilst this would result in almost doubling the residential density of this area, since the site is located within a sustainable major development location, it is considered acceptable. The proposal would comply with policies CP2 of the Core Strategy and would be consistent with the intents of policy TB09 of MDD Local Plan and is acceptable in principle.</p> <p>The proposed units would be two storey which would be twice the height of other units of the area. However, it is considered acceptable since the proposal would be sited near the southern end of the larger mobile home park and will not be readily visible from most parts of the public realm due to the presence of existing vegetation. The proposal would retain existing trees and hedges and includes plans to enhance the existing landscaping. As such, the proposal would not have any detrimental impact on the character of the area.</p> <p>The site is located within a sustainable major development location and the proposal includes improvements of existing parking and turning by extending the access road. Disabled parking spaces as well electric vehicle charging points are provided in accordance with the Borough's standards and these are considered acceptable. A flood risk assessment is submitted with the application which has been assessed by the</p>

Council's officers who have not raised any objection subject to a pre-commencement condition securing further details.

The proposed units do not comply with the standards for internal and external residential amenity and as such would not be appropriate for permanent habitation. However, these units are proposed to be for temporary accommodation and it is mentioned in the supporting document that most families will spend between six and nine months at these units. For this reason, the lack of amenity is considered acceptable on balance in this instance subject to a temporary 12 months tenancy period, which is secured by condition 17.

Fire safety information have been provided by the applicant which is considered acceptable subject to the building meeting the current building regulation's fire safety requirements. There are no objections to the proposal in terms of neighbour amenity, environmental health, ecology and archaeology grounds. The application is accordingly recommend for approval subject to conditions for the reasons set out in this report.

PLANNING STATUS

- Major development location
- Adjacent to designated countryside
- Residential caravan site
- Landfill gas consultation zone
- Flood zones 1 and 2
- Bat roost habitat suitability
- SSSI Risk zone
- Mineral site consultation area

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Conditions and informatives:

Conditions:

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved Details

This permission is in respect of the submitted application plans and drawings numbered 5651 – (1100D; 1103H; 1202A; 1203; 1204, 1205 and 1206) and drawing titled Section Through GF to FF received by the local planning authority on 10/12/2020 and Design and Access Statement (report reference: 5651-C1-01-F) received by the local planning authority on 20/01/2021. The development shall

be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. External Materials

Before the development hereby permitted is commenced, samples and details of the materials to be used in the construction of the external surfaces of the building/s shall have first been submitted to and approved in writing by the local planning authority. Development shall not be carried out other than in accordance with the so-approved details.

*Reason: To ensure that the external appearance of the building is satisfactory.
Relevant policy: Core Strategy policies CP1 and CP3.*

4. Privacy Screening

Prior to the commencement of the development, details of privacy screening including height, design and materials for the first floor staircase landing shall be submitted to and approved in writing from the Local Planning Authority. The approved scheme shall be implemented prior to the first occupation of the development and shall be maintained in the approved form for as long as the development remains on the site.

*Reason: To safeguard the residential amenities of neighbouring properties.
Relevant policy: Core Strategy policy CP3.*

5. Drainage Details

Prior to the commencement of the development hereby approved a drainage scheme shall be submitted and approved in writing by the Local Planning Authority. The drainage scheme shall include:

- a) A surface water drainage layout drawing showing all proposed pipes, connections, SuDS features and discharge points if there are such.
- b) Identification of the destination of discharge (in accordance with Building Regulation Part H hierarchy) with appropriate justification for the selection with related drawings/sketch and approval for the specific discharge amount if there are such.

The approved scheme shall be implemented prior to the first occupation of the development and shall be maintained in the approved form for as long as the development remains on the site.

*Reason: This is to prevent increased flood risk from surface water run-off.
Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change,*

6. Contamination

Unless otherwise agreed by the Local Planning Authority, development other than that required to be carried out as part of an approved scheme of remediation must not commence until parts A) to D) (below) have been complied with. If unexpected contamination is found after development has begun, development must be halted on that part of the site affected by the unexpected contamination to the extent specified by the Local Planning Authority in writing until part D of the condition has been complied with in relation to that contamination.

A) Site Characterisation:

An investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the Local Planning Authority. The report of the findings must include:

- (i) a survey of the extent, scale and nature of contamination;
- (ii) an assessment of the potential risks to:
 - human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwaters and surface waters,
 - ecological systems,
 - archeological sites and ancient monuments;
- (iii) an appraisal of remedial options, and proposal of the preferred option(s).

This must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'.

B) Submission of Remediation Scheme

A detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be prepared, and is subject to the approval in writing of the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

C) Implementation of Approved Remediation Scheme

The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development other than that required to carry out remediation, unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be produced, and is subject to the approval in writing of the Local Planning Authority.

D) Reporting of Unexpected Contamination

In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of part A), and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of part B), which is subject to the approval in writing of the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with part C).

Reason: To ensure that risks from land contamination to the future users of the land are minimised. Relevant policies: NPPF Section 15 (Conserving and Enhancing the Natural Environment) and Core Strategy policies CP1 & CP3.

7. **Noise control**

Before the development hereby permitted commences a scheme shall be submitted to and approved in writing by the local planning authority which specifies the provisions to be made for the control of noise emanating from the first floor units and external staircases and the approved scheme shall be fully implemented prior to the occupation of the building(s).

Reason: To protect the occupiers of nearby premises from unreasonable noise levels. Relevant policy: Core Strategy policies CP1 and CP3 and Managing Development Delivery Local Plan policy CC06.

8. **Foundation Design**

No development or other operations shall take place until a pile and beam foundation design has been submitted to the local planning authority for approval, in order to prevent damage to the adjacent off-site trees.

Reason: In the interests of the amenity and landscape character of the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

9. Protection of Trees

- a) No development or other operations shall take place except in complete accordance with the Arboricultural Impact Assessment and Method Statement and associated Tree Protection Plan reference 370/01A (hereinafter referred to as the Approved Scheme).
- b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

10. Landscaping

Planting shall be carried out in accordance with the landscape details as shown on the Drawing No. Tree and Shrub Planting Proposals 370/04 dated 8.12.2020 and Drawing No. 5651-1104 Proposed Hard Landscaping and Boundary Treatments dated Jan 2021 in the first planting and seeding seasons following the occupation of the building(s).

Any trees or plants which, within a period of 5 years from the date of the planting (or within a period of 5 years of the occupation of the buildings in the case of retained trees and shrubs) die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species or otherwise as approved in writing by the local planning authority.

Reason: To ensure adequate planting in the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

11. Retention of Trees and Shrubs

No trees, shrubs or hedges within the site which are shown as being retained on the Drawing No. 370/01 Rev. A dated 18.1.2021 – Tree Survey, Constraints and Protection Plan shall be felled, uprooted wilfully damaged or destroyed, cut back in any way or removed without previous written consent of the local planning authority; any trees, shrubs or hedges removed without consent or dying or being severely damaged or becoming seriously diseased within 5 years from the completion of the development hereby permitted shall be replaced with trees, shrubs or hedge plants of similar size and species unless the local planning authority gives written consent to any variation.

Reason: To secure the protection throughout the time that development is being carried out, of trees, shrubs and hedges growing within the site which are of amenity value to the area. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

12. Electric Vehicle Charging Details

Prior to the occupation of the development hereby permitted, details for Electric Vehicle Charging points serving the development shall be submitted to and approved in writing by the Local Planning Authority. The Electric Vehicle Charging points shall be implemented in accordance with such details as may be approved and shall be permanently retained in the approved form for the charging of electric vehicles and used for no other purpose.

Reason: In order to ensure that secure electric vehicle charging facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

13. Parking and Turning

The development hereby permitted shall not be occupied until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

14. Cycle Parking

No building shall be occupied until details of secure and covered bicycle storage/ parking facilities for the occupants of [and visitors to] the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

15. Bin Store

No building shall be occupied until the bin storage area/ facilities have been provided in full accordance with the approved details. The bin storage area and facilities shall be permanently so-retained and used for no purpose other than the temporary storage of refuse and recyclable materials.

Reason: In the interests of visual and neighbouring amenities and functional development. Relevant policy: Core Strategy CP3 and Managing Development Delivery Local Plan policy CC04.

16. Obscure Glazing

The first floor windows in the west elevations of units 44a, 45, 46 and 47 shall be fitted with obscured glass and shall be permanently so-retained. The window shall be non-opening unless the parts of the window which can be opened are more than 1.7 metres above the finished floor level of the room in which the window is installed and shall be permanently so-retained.

Reason: To safeguard the residential amenities of neighbouring properties. Relevant policy: Core Strategy policy CP3

17. Temporary Permission

Except for unit 36, all other units hereby permitted shall be occupied by a particular tenant (family or individual) for a maximum period of 12 months only, unless agreed otherwise in writing by the Local Planning Authority.

Reason: The type of building is not one that the local planning authority is prepared to approve other than for a limited occupancy period, having regard to the size, materials and type of structure proposed. Relevant policy: Core Strategy policies CP1 and CP3.

Informatives:

1. Pre-commencement conditions

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

2. **Fire safety**

The applicant is reminded that the development will need to be designed and built in accordance with the fire safety standards of current Building Regulation requirements. Relevant information is available in the following web link: <https://www.gov.uk/government/publications/fire-safety-approved-document-b>. Full assessment of the proposed development in respect of 'Building Control' matters will be undertaken during the formal statutory Building Regulations consultation.

3. **Thames Water**

Records indicate presence of public sewers and water mains beneath or in close proximity to the proposed development. In order to protect public sewers and water mains and to ensure that Thames Water can gain access to those sewers and mains for future repair and maintenance, approval must be sought from Thames Water where the erection of a building or an extension to a building or underpinning work would be over the line of, or would come within 3 metres of, a public sewer or a water main. Thames Water will usually refuse such approval in respect of the construction of new buildings, but approval may be granted in some cases for extensions to existing buildings. The applicant is advised to contact Thames Water Developer Services on 0845 850 2777 to discuss the options available at this site.

4. **CIL Liable Development**

The development hereby permitted is liable to pay the Community Infrastructure Levy. The Liability Notice issued by Wokingham Borough Council will state the current chargeable amount. A revised Liability Notice will be issued if this amount changes. Anyone can formally assume liability to pay, but if no one does so then liability will rest with the landowner. There are certain legal requirements that must be complied with. For instance, whoever will pay the levy must submit an Assumption of Liability form and a Commencement Notice to Wokingham Borough Council prior to commencement of development. For more information see - <http://www.wokingham.gov.uk/planning/developers/cil/cil-processes/>

5. **Positive and Proactive Discussion**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:

- a full pre-application process was undertaken by the applicant;
- amended plans being submitted by the applicant to overcome concerns relating to Trees and Landscape and Highway Safety.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

PLANNING HISTORY		
Application Number	Proposal	Decision
F/2004/2329	Proposed replacement of 2 mobile homes with 3 new ones.	Approved 01/12/2004
B/2/61	Site to accommodate 80 caravans, 56 garages	Approved 12/07/1961
B/17/59	Site to be used for stationing of caravans	Approved 10/02/1960

SUMMARY INFORMATION	
For Residential	
Site Area	0.42 hectares
Existing units	12
Proposed units	23
Previous land use	Residential Caravan Park
Proposed Public Open Space	N/A
Existing parking spaces	14
Proposed parking spaces	23

CONSULTATION RESPONSES	
Crime Prevention Design Officer	No comments received
Royal Berkshire Fire and Rescue	No comments received
Thames Water	No objections subject to informative
WBC Biodiversity	No comments received
WBC Economic Prosperity and Place (Community Infrastructure)	Support the application
WBC Drainage	No objections subject to condition 5
WBC Education (School Place Planning)	No comments received
WBC Environmental Health	No objection subject to condition 6
WBC Highways	No objection subject to conditions 12, 13 and 14
WBC Tree & Landscape	No objection subject to conditions 8, 9, 10 and 11
WBC Cleaner & Greener (Waste Services)	No objection subject to condition 15
WBC Growth and Delivery (Planning Policy)	No objections

REPRESENTATIONS

Town/Parish Council: **Winnersh Parish Council**: Objected to the proposal on following grounds:

- The site is a mobile home park with single story mobile homes, the proposed new 2 story prefabricated ones are out of character and will look unsightly.
- The existing sewage system is not fit for purpose currently, it constantly fails and would not cope with a further 11 homes.
- The proposed 2 storey dwellings will overlook the existing properties.

- The plans show the proposed dwellings have a single access point which is a potential fire risk.

Local Members: Cllr Paul Fishwick has objected to the proposal on following grounds:

- The two storey units would appear out of character within the wider Grovelands Park area which include only single storey units.
- The existing sewage system and pumping station are not fit for purpose for the existing residents and adding more homes to this failing system will create further issues for its residents.

Officer's note: The two storey units would be located at the furthest side of the area and would not be readily visible from majority of the caravan park.

The WBC Drainage Officer has reviewed the Flood Risk and Drainage Assessment report submitted with the application and has not raised any objection subject to a condition securing further details.

The possible point of overlooking is from the proposed landing of the external staircase. Any use of the external staircase landing is likely to be brief – used only for accessing the first floor unit. Notwithstanding, a condition is included to secure privacy screening for the landing to minimise overlooking.

The applicant has provided details of fire safety measures within the proposed units. Informative 2 is included to remind the applicant that the proposed development would have to meet the building regulations safety standards.

Neighbours: 12 residents of Grovelands Park (unite numbers 1, 9, 10, 11, 34, 35, 37, 38, 40, 50, 55 and one unspecified unit number) and 1 resident from 2 Carpenters Close, Woodley have objected to the proposal on following grounds:

- **Caravan Park requirements:** The site falls within a caravan park and therefore the proposal must adhere to the mobile homes act 1983. These units would have heights more than what is allowed by the legal definition of caravans.

Officer's note: Proposed two storey units are not considered as caravans. They have been described as 'two-storey prefabricated temporary accommodation units'. A planning permission would result in change of use of this part of the park into residential.

- **Fire safety:** These units are inadequate in terms of fire safety since they do not have two doors. Mobile homes catch fire and burn in 5-8 minutes.

Officer's note: See comment above regarding fire safety.

- **Sewage/Drainage issues:** The existing sewage and drainage systems are inadequate and the area experiences regular sewage problems. The sewage system is owned by WBC and maintenance of the pumping station as well as drainage infrastructure is extremely poor. The drainage infrastructure is already inadequate for the existing units – adding 11 more family units will cause further problems. These have not been addressed appropriately in the FRA report.

Officer's note: See comment above regarding drainage.

- **Two storey units:** These would appear out of character. These units would not be stable against high cross-winds. The poor quality of existing ground conditions would not allow for strong foundations and these units would be unsafe to live in. The existing units are not maintained well by WBC and if similar lack of maintenance continue, these two storey units would appear unsightly. The existing units are damp and lack proper insulation. It would be better to replace the existing units with more energy efficient units rather than two storey units.

Officer's note: These units would be located at the furthest side of the park and would not be visible from most areas due to the presence of existing trees and vegetation. Building and fire safety requirements would have to be in accordance with building regulations.

- **Impact on neighbouring amenity:** The proposed two storey units would have loss of light, overlooking and overbearing impacts on neighbouring single storey units. Due to their southern location, these units would have detrimental loss of light impact particularly on existing units 38 and 53.

Officer's note: These units would have acceptable neighbour impact, see neighbour amenity section (paragraphs 19 – 27) below.

- **Lack of outdoor amenity:** With 11 additional families, where children are expected to live, there is no outdoor amenity area for them to play. These units themselves lack rear amenity space and no ball game is allowed on the common greenspace.

Officer's note: These units would be used for temporary emergency accommodation. For this reason, private amenity space to meet standards is not considered essential.

- **Trees and Landscape:** The proposal would result in damaging some of the existing trees, resulting in harm to the area's visual amenity.

Officer's note: The application is supported by detailed arboricultural reports. The WBC Trees and Landscape Officers have reviewed the submitted information and raised no objection subject to conditions including appropriate foundation design and protection of trees.

- **Parking:** There is an existing problem with parking within the site and additional 11 units would cause further problems.

Officer's note: The applicant has provided parking information demonstrating policy compliant parking spaces including two disabled spaces and electric vehicle charging point.

- **De-valuing of permanent units:** The proposal would result in devaluing of permanent units which are maintained by mobile home owners. Additionally, permanent home owners are not allowed to add a second storey to their mobile homes but the Council is proposing two storey units on their side, which is unfair.

Officer's note: Loss of property value is not a material consideration in planning.

- **Antisocial behaviours:** This section of Grovelands Park is used as emergency temporary accommodation, which is different from the rest of the park where permanent residents live. Residents of the application site often demonstrate anti-social behaviours and adding more units of similar nature would cause additional problems. Some of the future occupiers would be drug addicts and existing park users have children. It will not be safe to accommodate them together in the same area. Lack of proper sound insulation would result in ground floor units being disturbed by the occupants of upper storey units.

Officer's note: Whilst anti-social behaviour by others can cause some annoyance, there is no evidence that the occupants of the development would cause a rise in incidents. Noise mitigation measures are secured using a condition.

APPLICANTS POINTS		
•	Wokingham homelessness and extreme housing need are fast-growing issues and the proposal would contribute in addressing them.	
•	Most families housed in these units will spend between six and nine months at the facility and so over a year the scheme could help up to 46 households per annum.	
•	The proposal would comply with policy CP2 of the Core Strategy which requires proposals to meet community need.	
•	The proposal would result in doubling the residential density of this part of the park. However, the site falls within a major development location and additional density would be acceptable here.	
•	All new units would be sited on the footprint of the existing units as closely as possible. The proposal would have no detrimental impact on levels of daylight in the habitable rooms of adjoining properties.	
•	The proposal would comply with the intents of policy TB09 of MDD Local Plan which supports residential accommodation for vulnerable groups.	
•	Existing units were designed as holiday homes, thus not purposed for long-term habitation. They are thermally inefficient causing additional hardship for already vulnerable families.	
•	All plots have been provided with bins and cycle storage areas.	
•	Additional landscaping is proposed for the site to achieve the sense of integration and sense of place echoed in Policy CP3.	
•	The modules will be manufactured off-site contributing to a greener and sustainable construction.	
•	The Modular design adopts a passive fire protection approach to protect the internal elements of the building in accordance with current building regulations.	

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP5	Housing mix, density and affordability
	CP6	Managing Travel Demand
	CP9	Scale and Location of Development Proposals

Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB05	Housing Mix
	TB07	Internal Space standards
	TB09	Residential accommodation for vulnerable groups
	TB21	Landscape Character
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide – Section 4
		DCLG – National Internal Space Standards
		CIL Guidance

PLANNING ISSUES

Description of Development:

1. The application is for the proposed replacement of 12 existing single storey mobile homes, located on 12 individual plots within a larger residential caravan park, with 11 two storey and 1 single storey temporary units of accommodation. The two storey units would consist of a separate two-bedroom housing unit per floor. Therefore, a total of 23 units are proposed to replace the existing 12.
2. The single storey unit (no. 36) would be a two-bedroom Category 3 Part M wheelchair user dwelling, designed in accordance with Part M of the Building Regulations to meet required internal space. The unit will have a dimension of 15m x 4.3m x 3m (internal height) that would comply with the legal definition of a caravan. This unit would have its own private parking space in the front garden.
3. The 11 no. two storey pre-fabricated units (no. 39 – 48) would be 14.5m (length) x 4.25m (width) x 6m (height) in dimension and would contain one no. two-bedroom self-contained residential unit in each floor, thus totalling 22 units. The first floor units would be accessed by an external metal staircase.
4. The proposal also includes an extension of the existing access road to include additional parking and turning areas and additional landscaping.

Principle of Development:

5. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
6. The site is located within the built-up area of Winnersh, identified as a Major Development Location in Core Strategy Policy CP9 (Scale and location of development proposals) and Policy CC02 (Development Limits) of the Managing Development Delivery (MDD) Local Plan. Development in this location is acceptable in principle providing it is of an appropriate scale and character, and accords with the relevant policies set out in the wider development plan and national policy as a whole.
7. Additionally, proposals would comprise the use of previously developed land, which is supported by local and national policy. Section 11 of the NPPF refers to making effective use of land in meeting the need for homes and other uses. Among other considerations, paragraph 118 e) states that planning policies and decisions should: *“support opportunities to use the airspace above existing residential and commercial premises for new homes. In particular, they should allow upward extensions where the development would be consistent with the prevailing height and form of neighbouring properties and the overall street scene, is well designed (including complying with any local design policies and standards), and can maintain safe access and egress for occupiers.”*
8. Though the proposed 2 storey units would be replacing existing units rather than extending them upwards, the proposals would broadly result in more effective use of existing PDL. Therefore, subject to character and other impacts, the principle of the proposals is supported by this aspect of national policy.
9. Policy CP6 of the Core Strategy seeks to ensure that travel demand is managed. The proposal site is a sustainable location since it is located within 0.8 miles of Winnersh Station and a bus stop on Robin Hood Lane. It is also within walking distance from primary and secondary schools. There is no objection to the proposed development on sustainability grounds.

Housing need

10. The site is an existing residential caravan park consisting of homes originally designed as holiday homes. These mobile homes have been used to provide temporary accommodation for homeless households but are not fit for purpose. The replacement pre-fabricated units will have a higher standard of construction and so the proposals are intended to provide greater quality as well as quantity of accommodation. The supporting documentation sets out that the proposals are required to satisfy the Council's statutory duties to accommodate homeless households, particularly those with vulnerable children. It would remove the current reliance on expensive nightly paid B&B accommodation, which is typically out of Borough, as temporary accommodation.
11. The Wokingham Council's development plan and supporting evidence is silent on residential accommodation that is specifically for homeless people or accommodation

of a temporary nature. However, a number of policies broadly support the proposals. Core Strategy Policy CP2 relates to inclusive communities. As is clarified in supporting paragraph 4.15, this policy recognises that to achieve sustainable development “*it is important that the needs of all sections of the population are met through development*”. CP2 states that “*planning permission will be granted for proposals that address the requirements of... b) children, young people and families, including the co-ordination of services to meet their needs.*” The proposed per-fabricated units are designed with multiple rooms to accommodate families with children; therefore the development appears consistent with CP2.

12. Similarly, MDD policy TB09 relates to residential accommodation for vulnerable groups. The development plan does not provide a definition of the term ‘vulnerable’ and it is noted that the policy relates specifically to provision for elderly residents and adults and children in care. However, it is considered reasonable that homeless individuals would fall under a common sense definition of vulnerable and therefore the thrust of this policy would broadly support housing provision to address those needs.
13. Overall, the proposed development is for a demonstrable short term need for a distinct vulnerable group and is therefore consistent with the broad aims of these policies. For the reasons mentioned above, the proposal is considered acceptable in principle. However, the proposed units are substandard in terms of internal and external amenities of the future occupiers. To overcome these issues, the proposal would clearly have to be for temporary accommodation and would have to be controlled by a planning condition to restrict the length of stay. It is considered that any tenancy of more than 12 months could result in harm to the amenity of the occupiers and condition 17 is included to secure this.

Character of the Area:

14. The proposal includes erection of two storey pre-fabricated units with external staircases. These units would be externally finished with timber cladding and would have single ply flat roof with felt top. Due to the nature of the proposed units, which do not fall within the legal definition of caravan, this section of the larger caravan park would result in change of use from residential caravan park to residential. However, this change of use would not be significantly dissimilar from the existing use and as such would not have any additional impact.
15. These units would be twice the height of the other units within the area and would therefore be prominent within this section of the park. Objections have been received from the Parish Council as well as from neighbouring residents relating to the out of character appearance of these units. The proposal is considered to not have any detrimental impact on the character of the area, which is predominantly residential, since these units would have visually similar external finishes to other timber lodges within the park. Moreover, this section of the mobile home park is sited near the edge of the park area and is not readily visible from majority of public realm. The two storey units would be sited predominantly on the footprint of the existing units and would not significantly impact the existing building lines or pattern of units. For this reason, the additional height of these prefabricated units are not considered to appear an intrusive feature within the existing street scene and would not have detrimental impact on the character of the area.

16. The proposal would replace 12 existing units with 23 units, thus almost doubling the residential density of the area. However, the proposal site falls within a major development location and because of this, an increased residential density will not result in detrimental impact on character of the area.
17. This section of Grovelands Park includes existing trees and hedgerows along the field boundary adjacent to the site and marks the edge of the settlement boundary. These vegetation also contribute to the visual character of the area when viewed from the countryside. Whilst some of the two storey units would be visible from the open fields to the south, it is not considered to have any detrimental impact as long as the existing vegetation is protected and retained. This is discussed further in the trees and landscape section below.
18. Overall, the proposal is considered to not have a significant detrimental impact on the character of the area warranting a refusal.

Residential Amenities:

19. Policy CP3 of the Core Strategy aims to protect neighbouring amenity and R23 of the Borough Design Guide SPD requires that extensions relate to neighbouring properties.
20. **Overlooking:** R15 of the Borough Design Guide SPD requires the retention of reasonable levels of visual privacy to habitable rooms, with separation of 22-30m to the rear and 10-15m to the street. R23 notes that the side walls must not contain windows, especially at first floor level. It is to be noted that these standards are set for residential developments that are to be used for permanent occupancy where privacy of the rear amenity space is the primary concern. The current application is for emergency temporary occupation and there are no development plan policies for these types of developments. Notwithstanding, the two storey units are designed to maintain privacy for both the future occupiers of the development as well as for neighbouring residential amenity.
21. The proposed two storey units have been designed such that all habitable windows are located on southern elevations – thus minimising mutual overlooking. The northern elevations include the main entrance door and staircase landing which can have potential overlooking. Whilst it is considered that the nature of use of the landing would be brief – only to gain access into the unit, nonetheless, condition 4 is included securing privacy screening for the landing to minimise any overlooking impact.
22. Objections have been received from residents of neighbouring properties on overlooking impact of the proposal, particularly arising from the external staircase and first floor side windows of some of the units. First floor eastern windows of units 39 – 43 would have an outlook towards Emm Brook and would not have any loss of privacy impact on neighbouring properties, particularly on no. 38 Grovelands Park. The Borough Design Guide SPD requires 10m front-to-front separation for visual privacy. First floor road facing windows of all units would have appropriate separation from units across the road and would not have additional overlooking impact. It is considered that due to their location, first floor western windows of units 44a, 45, 46 and 47 could have a degree of overlooking on neighbouring properties. Since these windows are secondary windows to a habitable room, it is considered reasonable to condition these windows to remain obscure glazed and non-opening up to 1.7 m above the finished floor level of the room (condition 16) to minimise additional overlooking impact.

23. **Sunlight and Daylight:** R18 of the Borough Design Guide SPD aims to protect sunlight and daylight to existing properties, with no material impact on levels of daylight in the habitable rooms of adjoining properties. The two storey units are not considered to have any detrimental impact on the sunlight and daylight of neighbouring units since these would mostly maintain a mutual 6m separation.
24. Objections have been received from neighbouring properties 38 Grovelands Park on loss of light grounds since the two storey units will be located to the southern side of this single storey unit. Unit 38 would be located 6.9m from the two storey unit at plot 39. This is an acceptable separation distance and no harmful loss of light impact is considered to occur. Similarly, unit 47 would not result in loss of light impact on neighbouring property no. 53 since these units will be separated by more than 8m.
25. **Overbearing and Sense of Enclosure:** Except for the separation between unit 44 and 48, the proposed two storey units would maintain 6m or above separation from neighbouring units on all sides and as such the proposal would not have any overbearing impact on neighbouring amenity. Whilst the separation between units 44 and 48 would be 2.5m, it is more than the Borough Design Guide SPD recommended 2m flank-to-flank separation and as such, the proposal is not considered to have overbearing impact.
26. **Noise disturbance:** Policy CC06 and Appendix 1 of the MDD Local Plan requires that development protect noise sensitive receptors from noise impact. The proposed units could result in noise disturbance, particularly for the ground floor units since prefabricated units may not have sufficient noise insulation for the ceiling. Additionally, the external metal staircase would produce noise when in use, especially during late evenings when ambient noise level would be lower.
27. The proposal includes first floor units and external metal staircases which can result in potential noise disturbances to neighbouring properties. A sectional drawing has been submitted to demonstrate acoustic insulation between flats on both floors of the two storey units, and no objection has been raised by Environmental Health. However, since no technical information has been provided, it is considered reasonable to include a condition securing noise mitigation measures (condition 7).

Landscape and Trees:

28. Policy CC03 of the MDD Local Plan aims to protect green infrastructure networks, promote linkages between public open space and the countryside, retain existing trees and establish appropriate landscaping and Policy TB21 requires consideration of the landscape character.
29. The site is located on the settlement edge of Winnersh next to the countryside with expansive views across fields toward the Emm Brook river corridor. The site is located in Landscape Character Area J1 – Wokingham Winnersh Settled and Farmed Clay. The static homes proposed for replacement are located at the south-eastern end of the site which backs onto an area known as Maidensfield, consisting of open agricultural fields and remnant hedgerows. The application site itself includes existing trees and hedgerows along the field boundary adjacent to the site and marks the line of the settlement area. The vegetation plays an important part in the area's visual amenity and contributes positively to the character of the area.

30. An Arboricultural Implications Assessment and Method Statement has been submitted that assesses the condition of the trees and recommends tree works that are acceptable and these works should be carried out prior to the contractor taking possession of the site. This is secured using condition 9. The assessed trees are located between the plots, and along the boundaries and proposals do not include any felling of trees but adhere to the recommendations contained in BS5837:2012 for avoidance as far as possible. This approach is appropriate and there is no objection to the proposal on tree protection grounds.
31. There are minor conflicts with tree Root Protection Area (RPA) for units 40, 41, 44A and 48 which can be avoided using appropriate foundation design. A pre-commencement condition (no. 8) is included to secure these details to avoid any unwanted impact on existing trees.
32. The proposal includes additional hard and soft landscaping and boundary treatment. The WBC Trees and Landscape Officer has reviewed the proposal and has not raised any objection subject to condition 10 securing their implementation.

Access and Movement:

33. 11 additional units would not have significant impact on existing highways network. The proposal does not include any alteration to the existing access road other than a southern extension to accommodate additional parking and turning spaces. This is considered acceptable.

Parking:

34. The proposal includes 1 allocated and 22 unallocated parking spaces which exceeds Borough's parking standard by 7. All parking spaces are 5m x 2.5m which is acceptable. Proposed parking includes disabled spaces and electric vehicle charging points which meet the Borough's standard. Condition 12 is included to secure electric vehicle charging details and condition 13 secures permanent retention of the proposed parking and turning areas.
35. Cycle Parking is proposed to be provided for each unit which is acceptable. However, since no details of the storage units are provided, condition 14 is included to secure this.

Construction Management:

36. A construction health and safety plan is provided with the application which is considered acceptable and there is no objection to the proposal on highway safety grounds.

Flooding and Drainage:

37. The main section of the proposed development is in Flood Zone 1 with low risk of surface water flooding according to the EA mapping with a small eastern section in Flood Zone 2.

38. Detailed Flood Risk Assessment dated December 2020 has been provided with the application. The FRA provides information on:
- a) Details of how surface water runoff will be collected and dealt with from the proposed development have been presented and described including existing and proposed methods. (Sections 4.1, 4.2 and 5)
 - b) A flood risk assessment is shown for all forms of flooding and it is stated that the proposal is likely to improve the flood risk management by employing the SuDS Hierarchy. (sections 3)
 - c) Soil infiltration results demonstrating whether the achievability has been presented. (Appendix C).
 - d) MicroDrainage calculations in Appendix F for the proposed drainage strategy including 1 to 100 storm rain return period.
 - e) Detailed drawing with all the features and elements on the site including existing drainage presented in Appendix B.
39. The WBC Flooding and Drainage Officer has reviewed the FRA and has not raised any objection in principle to the proposed development. However, since a proposed drainage plan/layout has not been provided and additional information and justification for the disposal to the existing water course has not been shown, condition 5 is included to secure these details.
40. Objections have been received on lack of sewage management grounds. The WBC Drainage Officer has commented that most of the existing problems are likely to be the result of poor infrastructure which can cause congestion and may lead to flooding on the road during heavy rains. It is stated on the Application Form that surface water will be managed via Sustainable Drainage System and also existing watercourse disposal, but there is no proposed drainage methods or systems in the documentation. For this reason, condition 5 is included as a pre-commencement condition to ensure no additional flood risk from the proposed development.

Environmental Health:

Contamination:

41. A phase 2 Site Investigation Report (Your Environment Report RefYE989 dated June 2020) has been submitted with the application. The report identifies some contamination at depths likely to be encountered by future site users/occupiers and the level of risk is assessed to be moderate. Recommendations are made regarding the potential risk to site workers during construction.
42. The report recommends a remediation strategy for the eastern portion of the site across entire area of soft landscaping which involves removal of soil to 600mm depth, provision of a geotextile barrier and reinstatement with clean material. The report proposes an alternative remediation strategy to concrete over the areas proposed for /soft landscaping to provide a capping later. No risk from ground gas is identified so no gas protection measures are required.

43. The WBC Environmental Health Officer has reviewed the submitted information and has not raised any objection to the proposal subject to condition 6 to secure remediation strategy followed by validation when the approved works have been completed. The condition also makes provision for the discovery of unexpected contamination.

Amenity Space for future occupiers:

44. The proposal is for the erection of 23 two-bedroom temporary accommodation units in the form of 11 no. two storey pre-fabricated units and 1 no. single storey mobile home unit. The single storey unit would replace a similar unit at the same location and there is no objection to this aspect of the proposed development.
45. The two storey pre-fabricated units would not fall within the legal definition of caravans and as such should be considered as residential units. R16 of the SPD requires a minimum depth of 11m for rear gardens and a 1m setback from the site boundary to allow access thereto for all residential development. The rear amenity should be private, should receive direct sunlight and be capable of accommodating play, clothes drying and storage.
46. Whilst each unit has its allocated drying areas, they do not have adequate private amenity spaces, particularly for units 44 and 44a. The complete lack of any amenity area, either in front or back, makes these units unacceptable for long term occupation. However, since these units would be for short term needs and non-permanent accommodation, the non-compliance with policy is considered acceptable. These units would provide much needed solution for homelessness and it is considered that subject to a condition restricting the occupation of each unit to a maximum of 12 months tenancy, the lack of external amenity can be considered acceptable.

Internal Space Standards:

47. Policy TB07 of the MDD and R17 of the SPD require adequate internal space to ensure the layout and size achieves good internal amenity. In accordance with the Technical housing standards – nationally described space standard, a minimum standard of 61 sq. m applies for 1 storey 2 bedroom 3 person occupancy flats. A dwelling with more than one bedroom should have a main bedroom, which is to have a minimum area of 11.5 sq. m. Secondary of single bedrooms should have a minimum area of 7.5 sq. m. There should also be provision for storage.
48. Whilst the proposed units would comply with the internal space standards for each bedroom, the total internal area would be less than the area required for 2 bedroom 4 person occupancy flats. Additionally, policy TB07 of MDD Local Plan requires a 4 person unit to have a minimum combined floor area of living, dining and kitchen space of 27 sq. m. The proposed units would have a combined living, dining and kitchen space of 21.7 sq. m which would be less than the required standard. However, since these units would be for short term needs and non-permanent accommodation, no objection is raised, subject to condition 14 to ensure that these units are strictly used for 12 month temporary accommodations only.

Fire Safety Design/Construction:

49. Concerns have been raised by both Parish Council as well as local residents on fire safety requirements of these units. It has been stated in the Design and Access Statement (page 8) that the proposal utilises a passive fire protection approach to protect the internal elements of the building in accordance with current building regulations. The proposed structures would be protected using non-combustible sheathing material, intumescent sealants, and tapes to give a 60-minute fire resistance from inside to out.
50. An automatic fire suppression system will be fitted in each of the new homes to enhance the safety of the occupants from fire. Whilst this is considered acceptable, it is a matter dealt with under the Building Regulations. Informative 2 is included to remind the applicant of this requirement.

Infrastructure:

Community Infrastructure Levy:

51. The proposal is for additional habitable floorspace and would be a CIL liable development.

Employment Skills:

52. The proposal would utilise pre-fabricated units that would be manufactured off-site and will be brought into the site for assembly. These types of proposal are considered to not contribute to additional employment or skills needs and an Employment Skills Plan is not sought in this instance.

The Public Sector Equality Duty (Equality Act 2010)

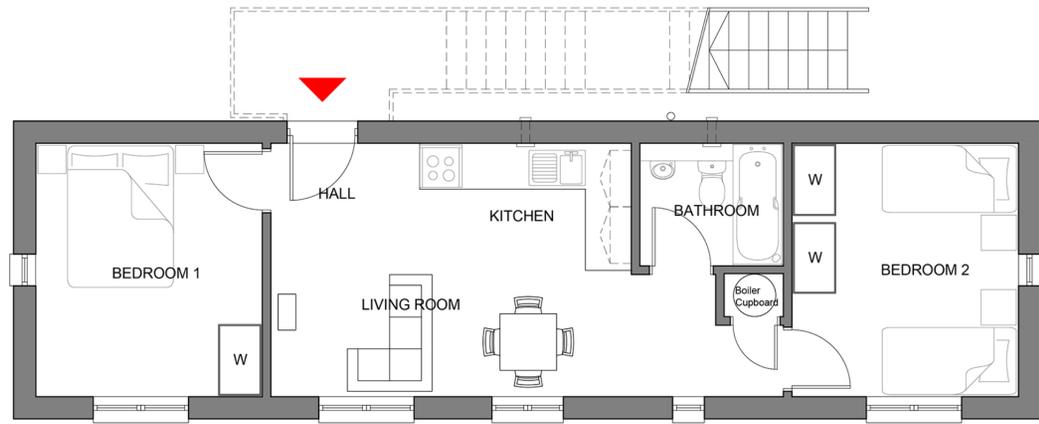
53. In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief.
54. The proposal would provide emergency shelter for individuals and families with young children who are homeless or at a risk of being homeless. One of the proposed units would be for disabled users and two parking spaces are provided for blue badge holders. Save for the potential positive impact on those with disability as a protected characteristic there is no indication or evidence (including from consultation on the application) that groups sharing other protected characteristics will be disproportionately affected in an adverse way by this planning application. In terms of the key equalities protected characteristics it is considered there would be no significant adverse impacts as a result of the development.

CONCLUSION

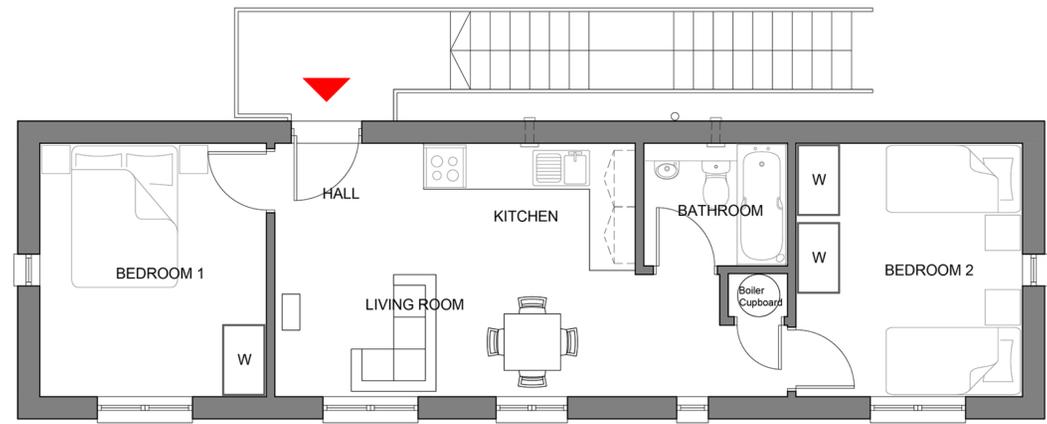
55. The proposal would provide much needed temporary accommodation to vulnerable group within a major development location and is acceptable in principle provided the temporary length of occupation for each unit does not extend beyond 12 months. The proposal would result in change of use of a section of the existing mobile home park since the two storey prefabricated units would not fall within the legal definition of mobile homes. However, this would not have any detrimental impact on the residential

character of the area and is considered acceptable. The proposal would not have any unreasonable landscape, flooding or traffic impact, subject to recommended conditions. The application is therefore recommended for an approval.

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Temporary Accommodation Unit Ground Floor Plan
Scale 1:50 @ A1



Temporary Accommodation Unit First Floor Plan
Scale 1:50 @ A1



North Elevation
Scale 1:50 @ A1



West Elevation
Scale 1:50 @ A1



South Elevation
Scale 1:50 @ A1



East Elevation
Scale 1:50 @ A1



Key Plan
Scale 1:500 @ A1



Grovelands Park - Proposed Accommodation

Proposed Units / Plots 39, 40, 41, 42, 43 - Scale 1:50 - December 2020

5651-1203

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CHARTERED ARCHITECTS

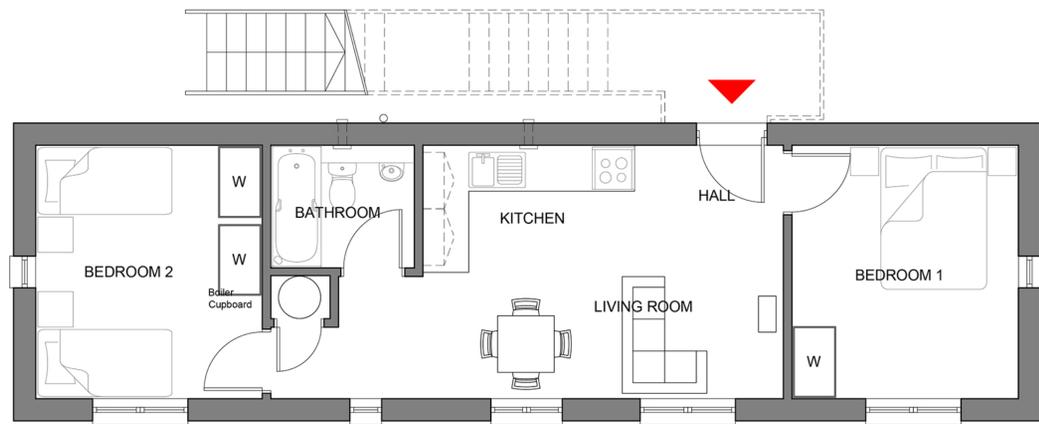
MERIDIAN HOUSE, 2 RUSSELL STREET, WINDSOR,
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TEL: 01753 857092 FAX: 01753 857096
WEB: www.edgingtons.co.uk EMAIL: mail@edgingtons.co.uk

Grovelands Park
Winkersley, RG41 5LE
Wokingham

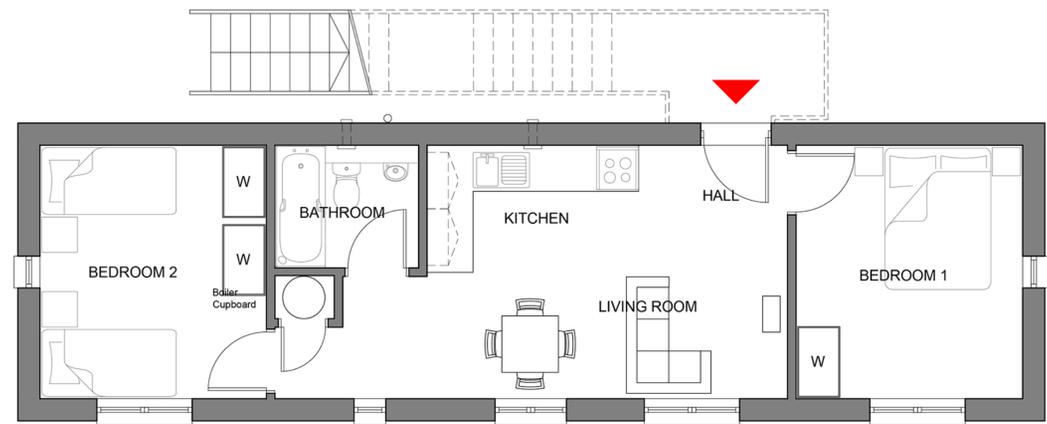
Proposed Accommodation
Proposed Units - Plots 39,40,41,42,43

SCALE (A1)	1:50	CAD REF	5651-1200
DATE	Dec 2020	DRAWN	VK
JOB No.	5651	DWG No.	1203

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Temporary Accommodation Unit Ground Floor Plan
Scale 1:50 @ A1



Temporary Accommodation Unit First Floor Plan
Scale 1:50 @ A1



North Elevation
Scale 1:50 @ A1



East Elevation
Scale 1:50 @ A1



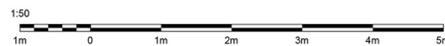
South Elevation
Scale 1:50 @ A1



West Elevation
Scale 1:50 @ A1



Key Plan
Scale 1:500 @ A1



Grovelands Park - Proposed Accommodation

Proposed Units / Plots 44A, 45, 46, 47 - Scale 1:50 - December 2020

5651-1205

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Wokingham

Proposed Accommodation
Proposed Units - Plots 44A, 45, 46, 47

SCALE (A1)	1:50	CAD REF	5651-1200
DATE	Dec 2020	DRAWN	VK
JOB No.	5651	DWG No.	1205

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Accommodation Schedule

- 12 Ground Floor Mobile Houses

Parking Schedule

- 14 parking spaces



187

Grovelands Park - Proposed Accommodation

Existing Site Plan - Scale 1:200 - December 2020

5651-1101-C



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Grovelands Park
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 Wokingham

Proposed Accommodation
 Existing Site Plan

SCALE (A1)	1:200	CAD REF	5651-1100
DATE	March 2020	DRAWN	VK
JOB No.	5651	DWG No.	1101

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Accommodation Schedule

- 1 Category 3 Part M Wheelchair Accessible Dwelling
- 11 Ground Floor Double Bedroom Dwellings
- 11 First Floor Double Bedroom Dwellings

Parking Schedule

- 1 private parking space for wheelchair user
- 22 parking spaces

189

-  Proposed Trees
-  Existing Trees
-  Existing Hedging / Planting
-  Proposed Hedging / Planting



Grovelands Park - Proposed Accommodation

Proposed Site Plan - Scale 1:200 - December 2020

5651-1103-H

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CHARTERED ARCHITECTS

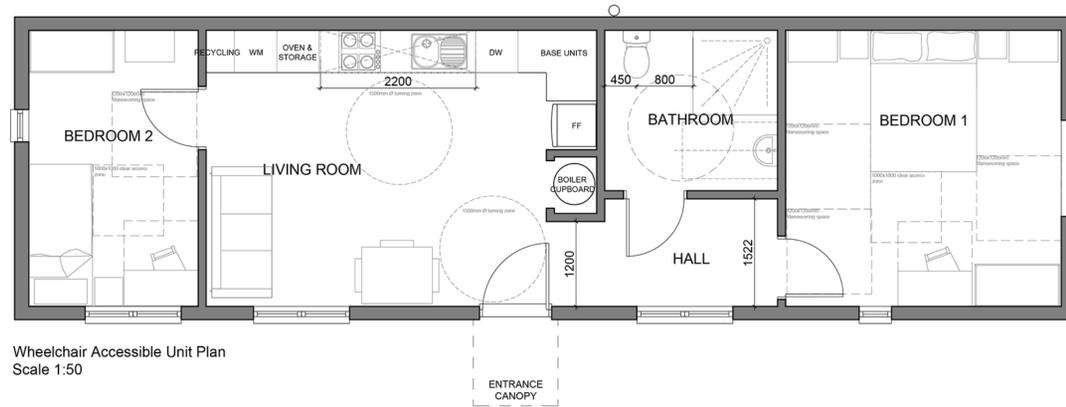
Grovelands Park
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Proposed Accommodation
Proposed Site Plan

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SCALE (A1)	1:200	CAD REF	5651-1100
DATE	March 2020	DRAWN	VK
JOB No.	5651	DWG No.	1103
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Wheelchair Accessible Unit Plan
Scale 1:50



Key Plan
Scale 1:500 @ A1



South Elevation
Scale 1:50 @ A1



East Elevation
Scale 1:50 @ A1

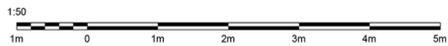


North Elevation
Scale 1:50 @ A1



West Elevation
Scale 1:50 @ A1

191



Grovelands Park - Proposed Accommodation

Proposed Wheelchair Accessible Unit - Plot 36 - Scale 1:50 - December 2020

5651-1202-A



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Wokingham

Proposed Accommodation
Proposed Wheelchair Accessible Unit - Plot 36

SCALE (A1)	1:50	CAD REF	5651-1200
DATE	Dec 2020	DRAWN	VK
JOB No.	5651	DWG No.	1202 A

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Agenda Item 57.

Application Number	Expiry Date	Parish	Ward
202106	17/02/2021	Charvil	Charvil;

Applicant	Mr Donald MacDonald, Loddon District Scouts
Site Address	Paddicks Patch, Waingels Road, Charvil, RG10 0UA
Proposal	Full Planning application for the proposed erection of new meeting hall following demolition of existing meeting hall, relocation of three metal storage containers, plus car park improvements with the installation of a cycle stand.
Type	Full
Officer	Senjuti Manna
Reason for determination by committee	Major application (site area > 1 Ha)

FOR CONSIDERATION BY	Planning Committee on Wednesday, 10 February 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY
<p>Paddick's Patch is the district headquarters of Loddon District Scouts group. It is located within the ancient woodlands of a Local Wildlife Site known as Norris' Copse Bird Sanctuary. The site is normally (without pandemic restrictions) used for meetings of various Scouting groups in the Loddon District during weekday evenings and for camps and other special activities on the site during weekends. The site currently includes a meeting hall and three steel storage containers as well as various activity equipment.</p> <p>The proposal is for the erection of a new meeting hall and storage facilities near the site entrance and improvement of parking area following demolition of existing meeting hall and storage containers. This will not result in any change in the existing land use and is acceptable in principle. The new meeting hall would be located closer to the main entrance gate and would be approximately 34% larger than the existing building in terms of footprint and 51% larger in volume. Whilst the proposal would result in expansion of development away from the original building, contrary to policy CP11, in this instance it is considered acceptable since the proposal includes demolition of the existing building, secured by a condition, and as a result will not result in spread of development within countryside. The re-siting of the hall would be acceptable since the proposal would result in more compact form of development closer to the main entrance to the site as well as due to the presence of mature trees along the front boundary which will partially obscure the new building. Whilst the proposed volume increase would be slightly more than the recommended 50%, in this instance it is considered acceptable since the new building would not appear as an intrusive feature within the existing street scene.</p> <p>The site is located within an ancient woodland and local wildlife site. An Arboricultural Method Statement (AMS) including tree constraints plan has been submitted to support the application. The WBC Trees and Landscape officer has reviewed the submitted information and recommended approval subject to conditions. Additionally, an ecological assessment report has been provided which is acceptable. The Council's ecology officer has commented that subject to conditions securing appropriate ecological protection and enhancement, the proposal would not have any detrimental impact on the ancient woodland and the local wildlife site.</p>

It is proposed to retain the existing meeting hall during the construction of the replacement building in order to facilitate the continued use of the site by the Scouts groups. This is considered acceptable. However, a condition is included requiring the original hall to be demolished and all materials removed from the site prior to the occupation of the replacement hall since retention of both halls within this countryside location will result in unacceptable urbanisation impact.

There are no objections to the proposal in terms of neighbour amenity, heritage, environmental health, fire safety and archaeology grounds. The application is accordingly recommended for approval subject to conditions for the reasons set out in this report.

PLANNING STATUS

- Designated Countryside
- Local Wildlife Site (Norris's Copse Bird Sanctuary)
- Ancient Woodland
- Radon Affected Area
- Contaminated Land Consultation Zone
- Landfill Gas Consultation Zone
- Groundwater Consultation Zone 3
- Heathrow Aerodrome Consultation Zone
- Tree Preservation Order TPO-0028-1971 (area TPO)
- SSSI Impact Risk Zone
- Unclassified Road
- Landscape Character Assessment Area J4: Settled and Farmed Clay

RECOMMENDATION

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

A. Conditions and informatives:

Conditions:

1. Timescale

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details

This permission is in respect of the submitted application plans and drawings numbered 201802/09A Location Plan; 201802/11 Proposed Site Plan; 201802/03 A Proposed Floor Plan and Elevations; 201802/04 Building Foundation Details; 201802/08 Existing Three Metal Storage Containers; Planning Statement; Design and Access Statement; and Ecological Assessment received by the local planning authority on 17 August 2020; Updated Arboricultural Method Statement; and HTS-

TPP-01C Tree Protection Plan received by the local planning authority on 21 December 2020 and drawing numbered 201802.06.C Proposed Building and Car Park Plan received by the local planning authority on 15 January 2021. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. Drainage details

Prior to the commencement of the development hereby approved a drainage scheme shall be submitted and approved in writing by the Local Planning Authority. The drainage scheme shall include:

- BRE 365 test results demonstrating whether infiltration is achievable or not.
- Calculations demonstrating the performance of soakaways.
- A drainage strategy plan indicating the location and sizing of SuDS features, with the base of any SuDS features located at least 1m above the seasonal high-water table level.
- Details demonstrating how any SuDS for this development would be managed throughout the lifespan of the development and who will be responsible for maintenance

The approved scheme shall be implemented prior to the first occupation of the development and shall be maintained in the approved form for as long as the development remains on the site.

Reason: This is to prevent increased flood risk from surface water run-off. Relevant policy: NPPF Section 10 (Meeting the Challenge of Climate Change, Flooding and Coastal Change), Core Strategy policy CP1 and Managing Development Delivery Local Plan policies CC09 and CC10.

4. Construction Method Statement

No development shall take place, including any works of demolition, until a Construction Method Statement has been submitted to, and approved in writing by, the local planning authority. The approved Statement shall be adhered to throughout the construction period. The Statement shall provide for:

- i) the parking of vehicles of site operatives and visitors,
- ii) loading and unloading of plant and materials,
- iii) storage of plant and materials used in constructing the development,
- iv) the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate,
- v) wheel washing facilities,
- vi) measures to control the emission of dust and dirt during construction,
- vii) a scheme for recycling/disposing of waste resulting from demolition and construction works.

Reason: In the interests of highway safety & convenience and neighbour amenities. Relevant policy: Core Strategy policies CP3 & CP6.

5. Protection of trees

- a) No development or other operations shall take place except in complete accordance with the Arboricultural Method Statement (AMS) dated June 2020 and updated December 2020 and Tree Constraints Plan (HTS-TPP-01C) dated 18/12/20 (hereinafter referred to as the Approved Scheme).
- b) No operations shall commence on site in connection with development hereby approved (including any tree felling, tree pruning, demolition works, soil moving, temporary access construction and or widening or any other operation involving use of motorised vehicles or construction machinery) until the tree protection works required by the Approved Scheme are in place on site.
- c) No excavations for services, storage of materials or machinery, parking of vehicles, deposit or excavation of soil or rubble, lighting of fires or disposal of liquids shall take place within an area designated as being fenced off or otherwise protected in the Approved Scheme.
- d) The fencing or other works which are part of the Approved Scheme shall not be moved or removed, temporarily or otherwise, until all works including external works have been completed and all equipment, machinery and surplus materials removed from the site, unless the prior approval in writing of the local planning authority has first been sought and obtained.

Reason: To secure the protection throughout the time that the development is being carried out of trees shrubs or hedges growing within or adjacent to the site which are of amenity value to the area, and to allow for verification by the local planning authority that the necessary measures are in place before development and other works commence Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

6. Demolition required

The development hereby permitted shall not be occupied until the existing structure(s) shown to be demolished on the approved plans have been so demolished and all materials arising from the demolition of buildings have been permanently removed from the site.

Reason: In the interest of the amenity of the designated Countryside. Relevant policy: Core Strategy policies CP1, CP3, and CP11, and Managing Development Delivery Local Plan policies CC03 and TB21.

7. External materials

The materials to be used in the construction of the external surfaces of the development hereby approved shall be those specified in the application form and approved drawings unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

8. Parking and turning space to be provided

No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking and turning space has been provided in accordance with the approved plans. The vehicle parking and turning space shall be retained and maintained in accordance with the approved details and the parking space shall remain available for the parking of vehicles at all times and the turning space shall not be used for any other purpose other than vehicle turning.

Reason: To provide adequate off-street vehicle parking and turning space and to allow vehicles to enter and leave the site in a forward gear in the interests of road safety and convenience and providing a functional, accessible and safe development and in the interests of amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

9. Cycle parking

Prior to the occupation of the development hereby permitted details of secure and covered bicycle storage/ parking facilities for the occupants of [and visitors to] the development shall be submitted to and approved in writing by the local planning authority. The cycle storage/ parking shall be implemented in accordance with such details as may be approved before occupation of the development hereby permitted, and shall be permanently retained in the approved form for the parking of bicycles and used for no other purpose.

Reason: In order to ensure that secure weather-proof bicycle parking facilities are provided so as to encourage the use of sustainable modes of travel. Relevant policy: NPPF Section 9 (Sustainable Transport) and Core Strategy policies CP1, CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

10. Access surfacing

No building shall be occupied until the vehicular access has been surfaced with a permeable and bonded material across the entire width of the access for a distance of 10 metres measured from the carriageway edge.

Reason: To avoid spillage of loose material onto the highway, in the interests of road safety. Relevant policy: Core Strategy policy CP6.

11. Access to be widened

Development shall not be occupied until the access has been widened in accordance with the approved drawing numbered 201802.06.C Proposed Building and Car Park Plan.

Reason: In the interests of highway safety and convenience. Relevant policy: Core Strategy policies CP3 & CP6.

12. Gates

Notwithstanding the approved plans and provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), no gates or barriers shall be erected unless set back a distance of at least 5 metres from the carriageway edge and so as to open away from the highway.

Reason: To ensure that vehicles do not obstruct the highway whilst waiting for gates or barriers to be opened or closed, in the interests of road safety. Relevant policy: Core Strategy policies CP3 & CP6.

13. External lighting

Prior to occupation, an external lighting design strategy for biodiversity to cover the meeting hall, car park, and storage areas shall be submitted to and approved in writing by the local planning authority. The strategy shall:

- a) Identify the ecological receptors on the site that are sensitive to artificial lighting; and
- b) Show how and where external lighting will be installed to adequately mitigate the risk to these ecological receptors.

All external lightings shall be installed in accordance with the specifications and locations set out in the strategy and these shall be maintained thereafter in accordance with the strategy.

Reason: To avoid and mitigate the deterioration of habitat of principal importance and maintain the integrity of a Local Wildlife Site in line with Core Strategy policy CP7.

14. Positive conservation management / Biodiversity Net Gain

A Landscape and Ecological Management Plan (LEMP) detailing the means by which the Local Wildlife Site shall receive positive conservation management shall be submitted to, and be approved in writing by, the local planning authority prior to occupation of the new meeting hall. The content of the LEMP shall include the following:

- a) Description and ecological evaluation of the site;
- b) Aims and objectives of management;
- c) Appropriate management options for achieving aims and objectives; and
- d) Prescriptions for management actions for the following five year period.

The approved plan shall be implemented in accordance with the approved details thereafter.

Reason: To minimise impacts on and provide net gains for biodiversity as part of a coherent ecological network, as per the NPPF and Core Strategy policy CP7 and Managing Development Delivery policy TB23.

15. Protected species mitigation

Works are to be carried out in full accordance with the mitigation strategies for protected species given in section 4 of the submitted Ecological Assessment report (GS Ecology, ref: ECO2613b, August 2020) unless otherwise agreed in writing by the council.

Reason: To ensure that reptiles, breeding birds and badgers, which are all protected species, are not adversely affected by the proposals.

16. Landscaping

Planting shall be carried out in accordance with the landscape details as shown on the Tree Protection Plan (HTS-TPP-01C) in the first planting and seeding seasons following the occupation of the building(s).

Any trees or plants which, within a period of 5 years from the date of the planting (or within a period of 5 years of the occupation of the buildings in the case of retained trees and shrubs) die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species or otherwise as approved in writing by the local planning authority.

Reason: To ensure adequate planting in the interests of visual amenity. Relevant policy: Core Strategy policy CP3 and Managing Development Delivery Local Plan policies CC03 and TB21.

Informatives:

1. Pre-commencement conditions

The applicant is reminded that this approval is granted subject to conditions which must be complied with prior to the development starting on site. Commencement of the development without complying with the pre-commencement requirements may be outside the terms of this permission and liable to enforcement action. The information required should be formally submitted to the Council for consideration with the relevant fee. Once the details have been approved in writing the development should be carried out only in accordance with those details. If this is not clear please contact the case officer to discuss.

2. Demolition notice

The applicant is reminded that a Demolition Notice may be required to be served on the Council in accordance with current Building Regulations and it is recommended that the Building Control Section be contacted for further advice.

3. Fire safety

The applicant is reminded that the development will need to be designed and built in accordance with the fire safety standards of current Building Regulation requirements. Relevant information is available in the following web link: <https://www.gov.uk/government/publications/fire-safety-approved-document-b>. Full assessment of the proposed development in respect of 'Building Control' matters will be undertaken during the formal statutory Building Regulations consultation.

4. Access construction

The Head of Highways at the Council Offices, Shute End, Wokingham [0118 9746000] should be contacted for the approval of the access construction details before any work is carried out within the highway (including verges and footways). This planning permission does NOT authorise the construction of such an access or works.

5. Positive and proactive discussion

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received. This planning application has been the subject of positive and proactive discussions with the applicant in terms of:

- a full pre-application process was undertaken by the applicant;
- amended plans being submitted by the applicant to overcome concerns relating to Trees and Landscape and Highway Safety.

The decision to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF is considered to be a positive outcome of these discussions.

PLANNING HISTORY		
Application Number	Proposal	Decision & Date
19674	Scout camp, fence, access, car park	Approval 22/08/83
33454	Permanent portaloo	Temporary permission until 31/07/94 granted on 05/07/89
36669	Building for shelter and training, re-site toilet, relocate cess pit (33454)	Refusal 05/12/90
37156	Building for shelter and training	Refusal 26/04/91
37848	Building for scout HQs	Temporary Permission until 31/07/96 granted on 24/07/91
44481	Shed, fencing, car parking and lighting	Temporary Permission until 31/07/96 granted on 02/01/96
F/1996/63966	Renewal of permission 37848 and 44481	Approved 03/09/96

SUMMARY INFORMATION	
For Non-residential	
Site Area	1.37 Hectares
Previous land use(s) and floorspace(s)	Assembly and leisure (Scouts Meeting Hall) 118 sq.m.
Proposed floorspace of each use	Assembly and leisure 158 sq.m.
Change in floorspace (+/-)	+ 40 sq.m.
Number of jobs created/lost	N/A
Existing parking spaces	20
Proposed parking spaces	13

CONSULTATION RESPONSES	
Crime Prevention Design Officer	No comments received
Royal Berkshire Fire and Rescue	No objections subject to informative
Southern Gas Networks	No objections
Thames Water	No comments received
WBC Biodiversity	No objections subject to condition
WBC Drainage	No objections subject to condition
WBC Environmental Health	No objections
WBC Highways	No objections subject to condition
WBC Tree & Landscape	No objections subject to condition
WBC Cleaner & Greener (Waste Services)	No comments received

REPRESENTATIONS

Town/Parish Council:

- **Woodley Town Council:** No objections.
- **Charvil Parish Council:** Supports the application as the proposal would enhance the site in the long run.

Local Members: No comments received

Neighbours: No comments received

APPLICANTS POINTS
<ul style="list-style-type: none"> • The site is currently used for camping and scouting and the meeting hall is ancillary to this use. • The proposal would comply with exception point 1 of policy CP11 which supports proposals for recreation in, and enjoyment of, the countryside since camping and scouting contribute to the recreational use of the site. • Existing building has poor environmental performance. Its replacement with a new building will result in environmental improvement. • Relocating the building closer to the main entrance would result in compact development, leaving the rest of the site in natural forested condition. • The replacement meeting hall would be approximately 51% larger than the existing hall in terms of volume. The additional volume mostly comes from the proposed pitched roof. The increase in footprint is approximately 34%.

- The proposal is located within a forested site and some distance from the adjacent properties. It is relatively isolated and so is unlikely to be considered to have any detrimental impact.
- The existing cess pit drainage will be replaced with mains soil drainage. This will have a positive impact on neighbouring properties.

PLANNING POLICY		
National Policy	NPPF	National Planning Policy Framework
	NPPG	National Planning Practice Guidance
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP6	Managing Travel Demand
	CP7	Biodiversity
	CP9	Scale and Location of Development Proposals
	CP11	Proposals outside development limits (including countryside)
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)
	CC10	Sustainable Drainage
	TB21	Landscape Character
	TB23	Biodiversity and Development
Supplementary Planning Documents (SPD)	BDG	Borough Design Guide – Section 4

PLANNING ISSUES

Description of Development:

1. The proposal is for the erection of new meeting hall, relocation of three metal storage containers, plus car park improvements with the installation of a cycle stand and demolition of the existing meeting hall. The new meeting hall is proposed to be located closer to the existing access gate and near the car park area and will have 158 sq.m of building footprint and 560 cu.m of volume. Three existing metal storage containers are proposed to be relocated closer to the car parking and cycle storage area.

2. It is proposed to retain the existing meeting hall during construction of the new hall to allow continued use of the site for scouting. Whilst it is considered acceptable, condition 6 is included to ensure demolition of the existing hall prior to occupation of the new hall. This is to protect the character of the area including designated countryside since retention of both buildings would result in inappropriate urbanisation and spread of development within the site.

Site Description:

3. The application site is located to the south of Charvil Village on Waingels Road within designated Countryside. The site is currently used for camping and scouting and existing buildings on the site are associated with this use. Existing development consists of a timber clad scouts meeting hall, three metal storage containers and some play equipment as well as car parking area near the main gate. The site is located within an ancient woodland as well as within a local wildlife site named Norris' Copse Bird Sanctuary and next to a waterbody. The camp site is located opposite to Waingels College and the neighbouring area has some linear residential development along Waingels Road.

Principle of Development:

4. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
5. The proposal site is located within designated Countryside outside any defined settlement boundary and Core Strategy Policy CP11 should be considered while assessing the impact of the proposal. Policy CP11 states that In order to protect the separate identity of settlements and maintain the quality of the environment, proposals outside of development limits will not normally be permitted except where:
 - 1) It contributes to diverse and sustainable rural enterprises within the borough, or in the case of other countryside based enterprises and activities, it contributes and/or promotes recreation in, and enjoyment of, the countryside; and
 - 2) It does not lead to excessive encroachment or expansion of development away from the original buildings; and
 - 3) It is contained within suitably located buildings which are appropriate for conversion, or in the case of replacement buildings would bring about environmental improvement; or
 - 4) In the case of residential extensions, does not result in inappropriate increases in the scale, form or footprint of the original building;
 - 5) In the case of replacement dwellings the proposal must:
 - i) Bring about environmental improvements; or

- ii) Not result in inappropriate increases in the scale, form or footprint of the original building.
 - 6) Essential community facilities cannot be accommodated within development limits or through the re-use/replacement of an existing building;
 - 7) Affordable housing on rural exception sites in line with CP9.
6. The proposal is considered to comply with criterion 1 of the above list since it is proposed to erect a replacement scout meeting hall which would satisfy the requirement of contributing to recreation in, and enjoyment of, the countryside. Whilst the new hall would be located on a different area from the existing hall, subject to removal of the existing hall before occupation of the new hall the proposal is considered not to result in expansion of development. The new hall would be located closer to the main entrance and car park to the front, thus reducing spread of development within the. The replacement building would improve the condition of the scout hall and as such would be considered to bring about environmental improvement. The scouts meeting hall is a community facility and associated camping activities would require countryside location. The proposed car parking would comply with the Borough parking standards. As such, the proposal complies with parts 1, 2, 3 and 6 of policy CP11 and is acceptable in principle.

Character of the Area:

- 7. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale, mass, layout, built form, height and character of the area and must be of high quality design. RD1, RD2, NR1 and NR2 of the BDG require that development contribute positively towards and be compatible with the historic or underlying character and quality of the local area.
- 8. The proposal site is located within designated Countryside within ecologically sensitive area. The Borough Design Guide SPD states that within designated countryside, any replacement building not larger than 50% of the existing building would be acceptable, provided the building brings about environmental improvements. Whilst this guidance primarily relates to residential buildings, it can be considered as a guidelines for all replacement buildings within countryside.
- 9. The proposed hall would be larger than the existing hall by 34% in terms of footprint and by 51% in terms of volume. Whilst the volume increase would be more than the recommended amount, in this instance it is considered acceptable since the building would have suitable design. The proposed walls and roof would include matt green colour cladding similar to agricultural buildings that will be appropriate for the countryside location. A condition is included to ensure appropriate materials are used for the construction of the new hall (condition 7). Moreover, the building will be partially obscured from public views by existing mature vegetation along the front boundary and will not appear as an intrusive feature within the existing street scene.
- 10. The new hall would have a gabled roof with 4.9m ridge height. Whilst this would be higher than the 3m high flat roof of the existing hall, it is considered acceptable since there are other buildings of similar height or even taller within the existing street scene including Waingels College located across the road.

11. The replacement hall would be located closer to the main entrance and car park, resulting in more compact form of development reducing spread. Care has been taken in designing the car park area to retain most of the protected trees to minimise harmful impact on the landscape character. Additional planting is proposed around the car park area to soften its view from the woodland. New planting is also proposed on the site of the existing meeting hall to help the area returned to woodland.
12. The relocation of metal storage containers will not result in additional built development and would not have additional impact. Their re-siting closer to the front near the car park area would result in reduction of spread of development. Overall, the proposal is considered acceptable in terms of its impact on the character of the area including the countryside.

Residential Amenities:

13. The proposed replacement building and other ancillary activities including car parking area are not considered to have any detrimental impact on any neighbouring properties due to the isolated location of the proposal site away from any other property.

Access and Movement:

Highway Safety:

14. There is an existing access from Waingels Road that is proposed to be widened to 4.8m. This is considered acceptable and there is no highway safety concerns relating to the proposed development subject to condition 12 requiring any gate to be set back from the carriageway edge by 5m.

Traffic Impact:

15. The proposal is for a replacement scouts meeting hall with an increased floorspace of 40 sq.m. This increase in floorspace is not large enough to cause significant traffic impact within this area.

Parking:

16. For the proposed floorspace, the Borough Parking Standard requires 8 spaces of 2.5m x 5m dimension for the previous use class D2 (new use class F1e). The proposed car park plan shows 13 parking spaces including one disabled parking which meets the parking standards. A condition (no. 8) is included to secure appropriate level of parking within the site. Cycle parking area for 6 cycles is proposed which is acceptable. However, since no details of the cycle storage is provided, this is secured using condition 9.

Construction Method Statement:

17. Due to the proximity of the site to Waingels College, a construction method statement is required. This is secured using condition 4.

Flooding and Drainage:

18. The proposed development would be located within Flood Zone 1 with low risk of surface water flooding. However, the proposal would result in increase in impermeable area leading to increase in surface water run-off. Additionally, it is proposed to remove some of the existing vegetation which currently act as a natural flood management system and any removal of planting can have a negative impact in consideration to the surface water flooding.
19. Based on the above, whilst there is no objection to the proposed scheme from flooding and drainage aspect, since no drainage details have been provided with the application, a condition (no. 3) is included to ensure appropriate drainage of the site.

Landscape and Trees:

20. The site is located within the countryside and consists of a woodland protected by a TPO (28/1971). The site is also designated as a Local Wildlife Site. The proposal is to erect a new scouts meeting hall and demolition of the existing hall that will require removal of 8 individual TPO trees and 3 groups of 2 and 5 trees (in total 17 trees). It has been mentioned in the supporting Arboricultural Method Statement that all of the trees identified for removal belong to categories C and U and most of them have poor structural form. As such, removal of these trees would not have a negative impact on the character of the existing woodland.
21. Replacement planting is proposed both on the footprint of the existing hall to return this area into its natural setting as well as near the proposed car park to soften its appearance from the woodland. The landscape proposal includes 25 trees to be planted where the old scout hall is to be removed and is made up of the following trees: 10 English Oaks; 5 Scots Pines and 10 Small-Leaf Lime. Car parking planting includes a mix of informal under-planting to match the woodland nearby. This is considered acceptable.
22. The application is supported by an Arboricultural Method Statement that details out the trees to be removed and protection of existing trees. The WBC Trees and Landscape Officer has reviewed this document and has raised no objection subject to conditions securing protection of existing trees (no. 5) and implementation of appropriate landscape scheme (no. 16).

Ecology:

Ancient Woodland:

23. The application site is listed on Natural England's Ancient Woodland Inventory. It is therefore important to consider if the proposed development would cause deterioration or loss of the woodland habitat.
24. The proposed use is not different from the current use of the site and as such, the nature of this use would not be intensified by the slightly larger footprint of the proposed hall. Additionally, the proposed foundation design would result in a net improvement to the woodland over time. The rationalisation of the built structures to be nearer the entrance will also reduce the disturbance of the woodland soil by heavy vehicles. As

such, the proposal is considered to not have any detrimental impact and in fact is likely to be beneficial for the ecology of the ancient woodland.

Local Wildlife Site:

25. The site is also listed as part of the Local Wildlife Site, Norris's Copse Bird Sanctuary (SU77S01). It is considered that the proposed development will not result in the deterioration of the Local Wildlife Site subject to condition (no. 13) securing an external lighting strategy that is appropriate for the light sensitive species which are likely to be using the site.
26. The local authority has an interest, under the NERC Act, in promoting positive conservation management measures on Local Wildlife Sites. The Council's records indicate that Norris's Copse Bird Sanctuary has a current management plan. It will be relevant for the management plan to be reviewed and revised to take into account the changes proposed in the current scheme. It is considered reasonable to include a condition (no. 14) to secure the submission and approval of a Landscape Environmental Management Plan for the site.

Protected Species:

27. The application is supported by an ecological assessment report which details the findings of an appropriately undertaken survey and states that the proposal is unlikely to result in any harmful impact on bats. However, the report includes precautionary mitigation measures for other protected species to be undertaken during the construction process. Condition 15 is included to secure this.

Community Infrastructure Levy:

28. The proposal is for a scout hall which is a non-residential development scheduled to have a CIL rate of £0.00 per square metre.

Other:

29. There is no other objection to the proposal on Environmental Health; Archaeology; Heritage and Fire Safety grounds. Whilst no fire safety information is provided with the application, Royal Berkshire Fire and Rescue Service has not raised any objection to the proposal subject to the proposed building meeting functional requirements of current building regulations in terms of fire safety. An informative (no. 3) is included to remind the applicant of this requirement.

The Public Sector Equality Duty (Equality Act 2010)

30. In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief. There is no indication or evidence (including from consultation on the application) that the protected groups identified by the Act have or will have different needs, experiences, issues and priorities in relation to this particular planning application and there would be no significant adverse impacts upon protected groups as a result of the development.

CONCLUSION

31. The proposal is for the erection of a new scout meeting hall, relocation of three metal storage containers, plus car park improvements with the installation of a cycle stand that would contribute and promote recreation in, and enjoyment of, the countryside. The proposed development would be suitably located and would be appropriate in design for the rural character subject to condition requiring removal of the existing hall prior to occupation of the new hall. The proposal would not have any unreasonable landscape, ecological or traffic impact, subject to recommended conditions. The application is therefore recommended for an approval.



Site Plan
Scale 1:500

Henley Surveying and Design Services

56, Elizabeth Road, Henley on Thames, RG9 1RA

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Tel 07879 600063

Client

Loddon District Scouts

Project

Replacement of Scout Meeting Hall
at Paddicks Patch, Waingels Road, Charvil
RG10 0AU

Drawing

Existing Site Plan

Scale
1:500
at A1

Date
July 2020

Drawn by
KW

Drawing No.
201802/10

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Site Plan
Scale 1:500

Henley Surveying and Design Services

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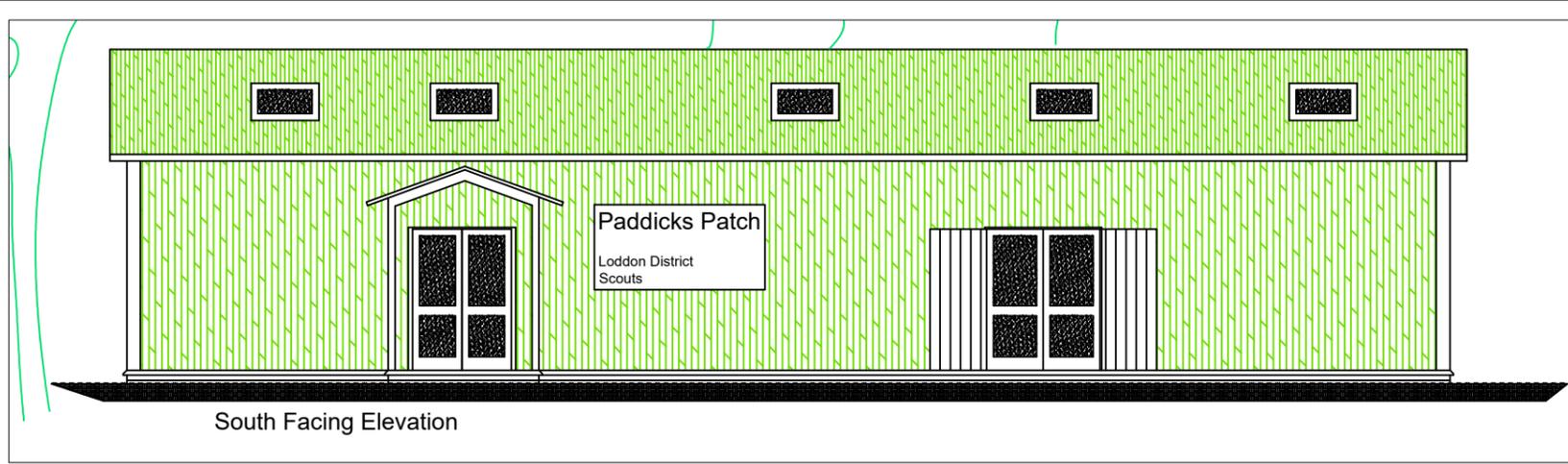
Client
Loddon District Scouts

Project
Replacement of Scout Meeting Hall
at Paddicks Patch, Waingels Road, Charvil
RG10 0AU

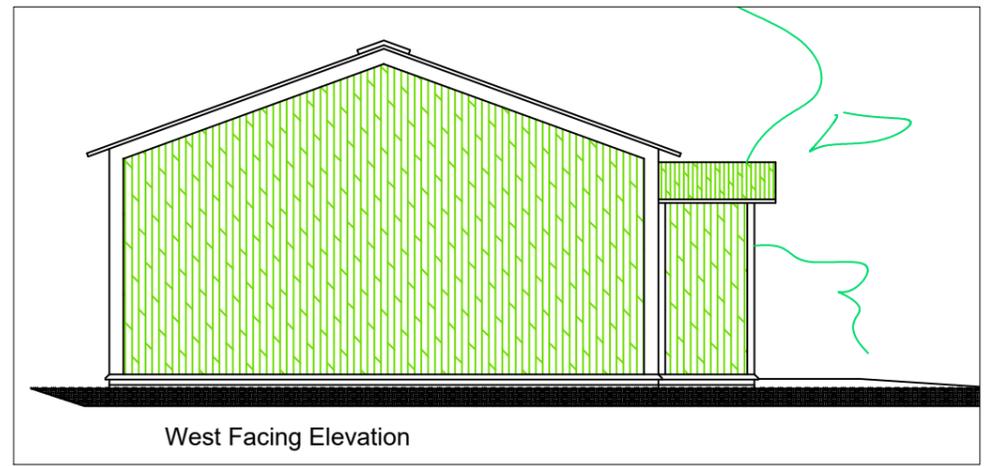
Drawing
Proposed Site Plan

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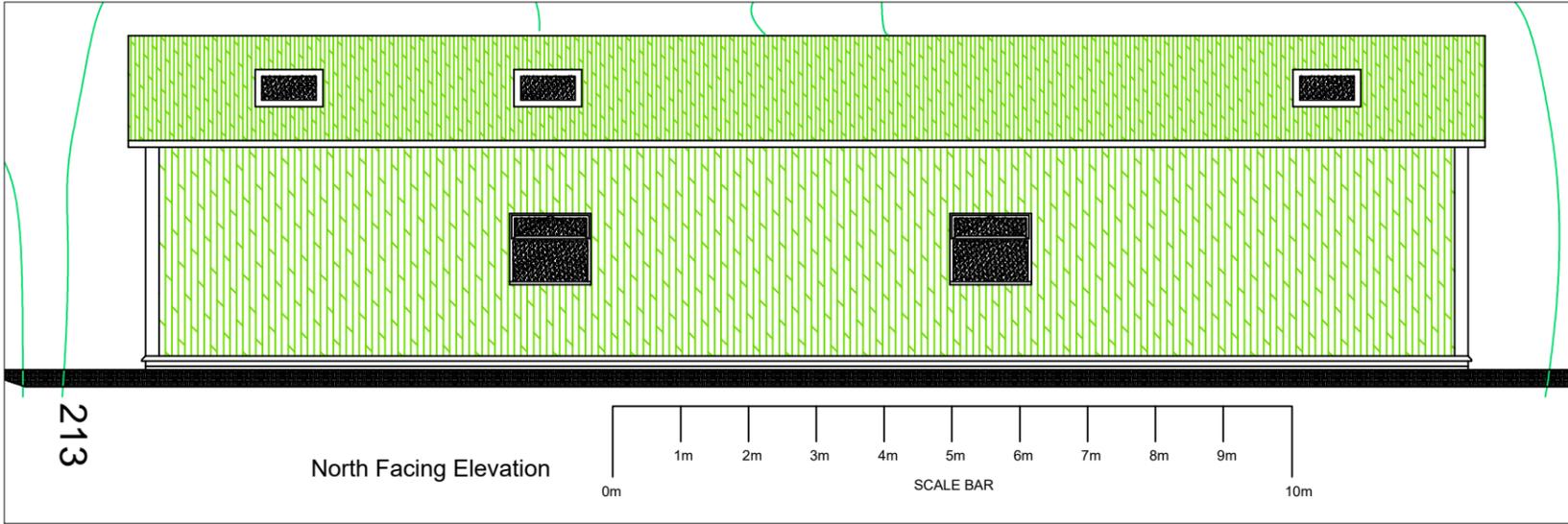
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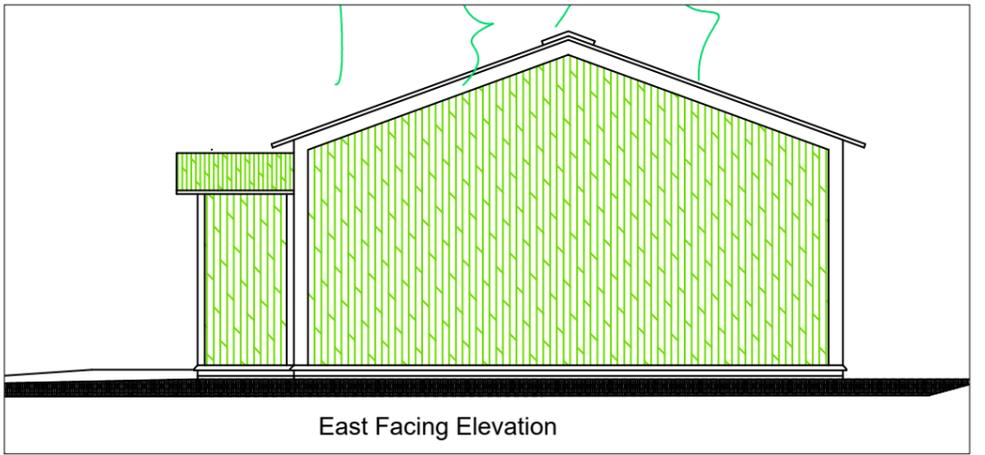
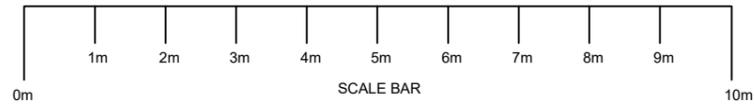
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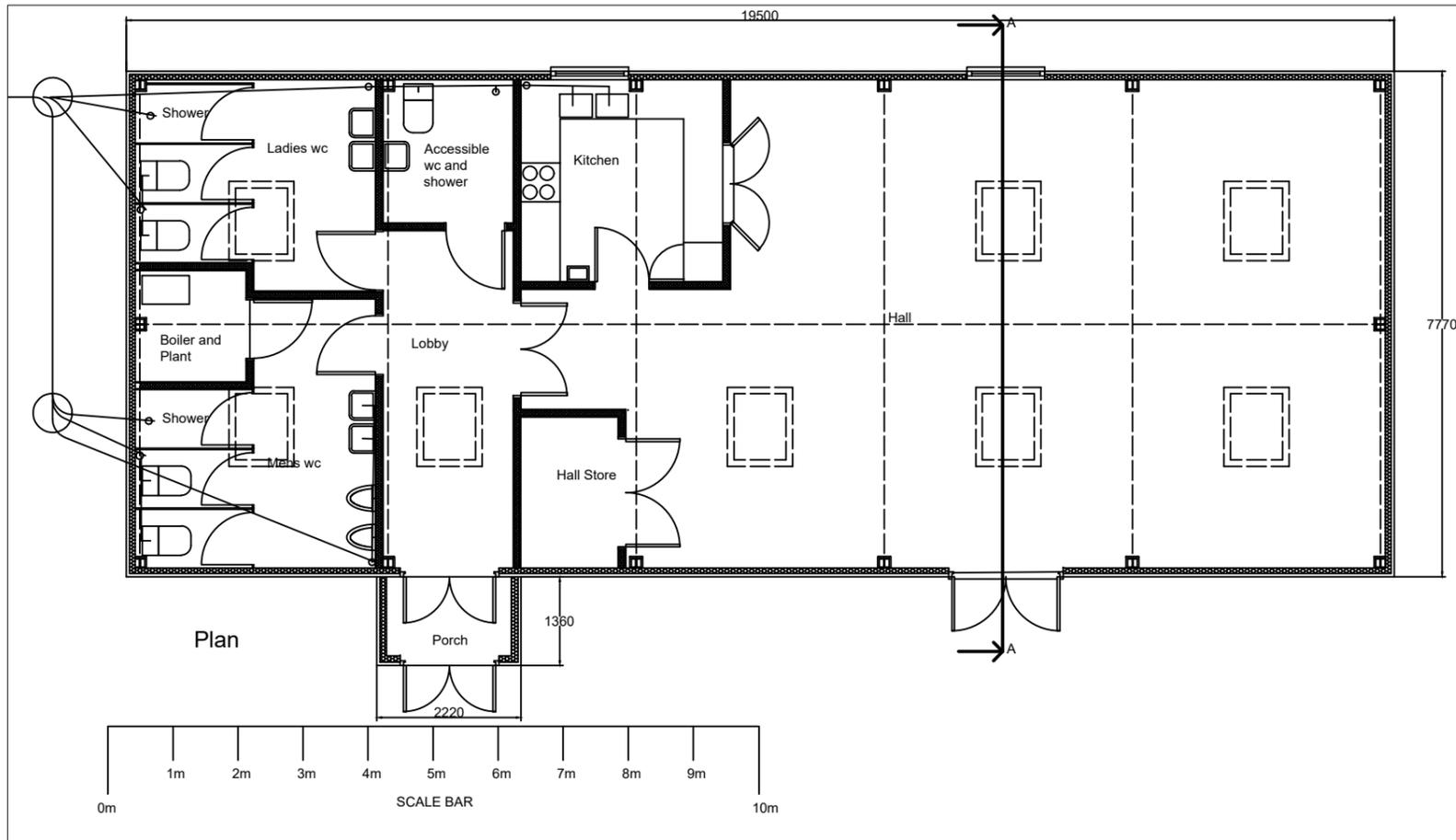
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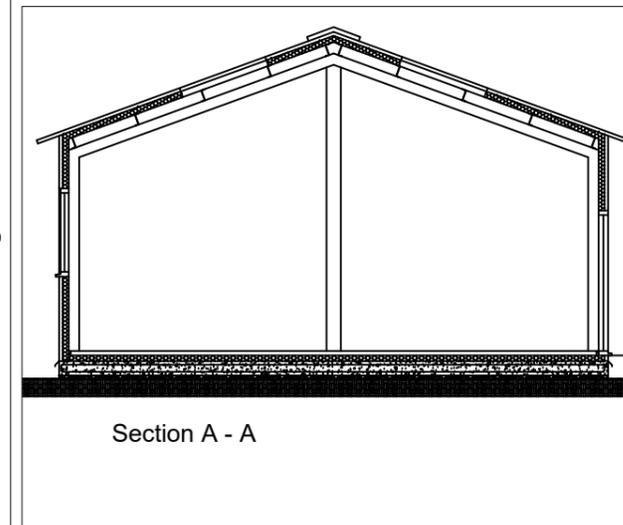
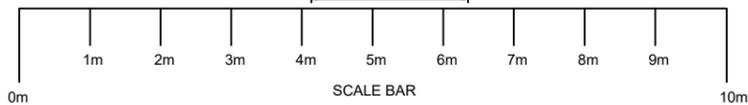
North Facing Elevation



East Facing Elevation



Plan

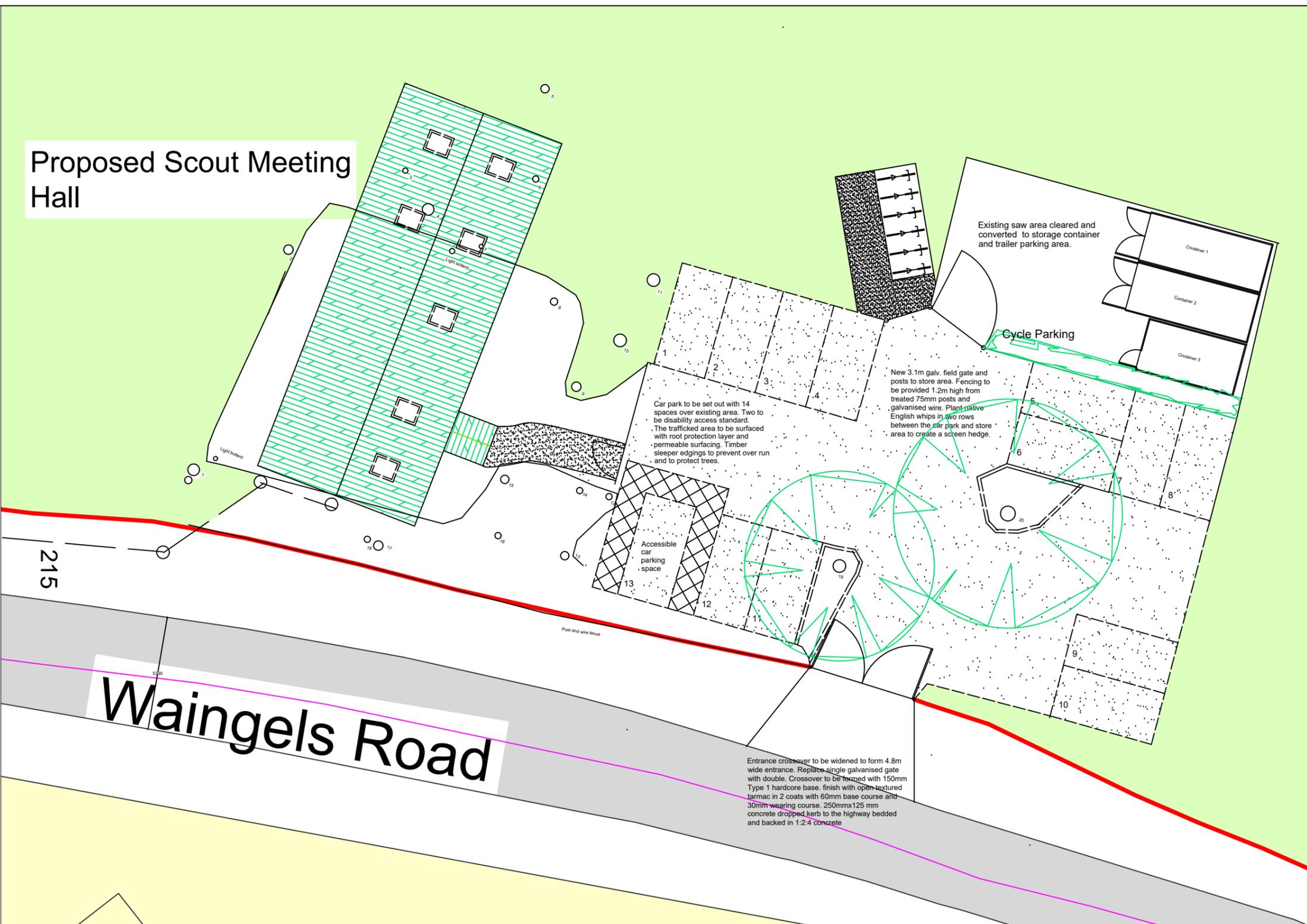


Section A - A

Revisions			
A Scale of drawing corrected. July 2020			
Henley Surveying and Design Services			
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e-mail keithwalker56@btinternet.com Tel 07879 600063			
Client			
Loddon District Scouts			
Project			
Replacement of Scout Meeting Hall at Paddicks Patch, Waingels Road, Charvil RG10 0AU			
Drawing			
Proposed Floor Plan and Elevations			
Scale	Date	Drawn by	Drawing No.
1:100 at A3	July 2020	KW	201802/03 A

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Proposed Scout Meeting Hall



Car Park Plan and Site of the Proposed Meeting Hall

Revision

C Accessible car parking space relocated to the other end of the car park. Clear space marking for access extended to rear of space/vehicle.

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Client

Loddon District Scouts

Project

Replacement of Scout Meeting Hall at Paddicks Patch, Waingels Road, Charvil RG10 0UA

Drawing

Building and Car Park Plan as Proposed

Scale
1:200
At A3

Date
July 2020

Drawn by
KW

Drawing No.
201802.06.C

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Agenda Item 58.

Application Number	Expiry Date	Parish	Ward
203344	10/02/2021	Woodley	South Lake;

Applicant	Wokingham Borough Council
Site Address	Highwood Bungalow, Fairwater Drive, Woodley, RG5 3JE
Proposal	Full application for a change of use from residential dwelling (Class C3(b)) to Residential institution/nursing home (C2), including a single storey side extension following demolition of the existing carport.
Type	Full Planning Application
Officer	Kieran Neumann
Reason for determination by committee	Applicant is Wokingham Borough Council

FOR CONSIDERATION BY	Planning Committee on Wednesday, 10 February 2021
REPORT PREPARED BY	Assistant Director – Place

SUMMARY

This planning application is before the planning committee as the application is made on behalf of Wokingham Borough Council and involves an increase in staff numbers.

The application site consists of a two bedroom detached bungalow which was originally part of the adjacent Highwood Primary School and served as accommodation for the school's caretaker. The site has since been separated from the school.

This application seeks consent to change the use of Highwood Bungalow from a residential dwelling (Class C3) to a residential institution (Class C2) in order to care for vulnerable adults. The application also includes the proposed construction of a single storey side extension to provide an extra bedroom and bathroom for carers of the residents as well as the installation of an internal sprinkler system.

The scheme provides great social benefit in the provision of living accommodation to vulnerable adults, thus complying with Core Strategy Policy CP2 and policy TB09 of the MDDL. This far outweighs the technical loss of a residential dwelling regardless of minor policy conflict with Core Strategy CP3.

The proposed change of use, combined with the subservient nature of the side extension results in a development which would not adversely impact the character of the area. Likewise, the proposal would not result in any undue harm to the residential amenities of neighbouring properties. Paragraphs 1-30 provide further details to these material considerations, and subject to conditions 1-6 the application is recommended for approval.

PLANNING STATUS

- Major Development location
- Groundwater protection zone
- Bat Roost Habitat Suitability
- SSSI Impact Risk Zones

RECOMMENDATION:

That the committee authorise the GRANT OF PLANNING PERMISSION subject to the following:

Conditions and informatives:

1. Timescale - The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In pursuance of s.91 of the Town and Country Planning Act 1990 (as amended by s.51 of the Planning and Compulsory Purchase Act 2004).

2. Approved details - This permission is in respect of the submitted application plans and drawings numbered 200178 - A005, 200178 - A006 & 200178 - A007 received by the local planning authority on 02/12/2020. The development shall be carried out in accordance with the approved details unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: For the avoidance of doubt and to ensure that the development is carried out in accordance with the application form and associated details hereby approved.

3. External materials - The materials to be used in the construction of the external surfaces of the extension hereby permitted shall be of a similar appearance to those used in the existing building unless other minor variations are agreed in writing after the date of this permission and before implementation with the Local Planning Authority.

Reason: To ensure that the external appearance of the building is satisfactory. Relevant policy: Core Strategy policies CP1 and CP3.

4. Restriction of change of use - The premises shall be used only for accommodating and caring for vulnerable adults and for no other purpose, including any other purpose in Class C2 of the Schedule to the Town and Country Planning [Use Classes] Order 1987 (as amended) or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order [with or without modification].

Reason: In granting this permission the local planning authority has had regard to the special circumstances of this case, being the care of vulnerable adults, and wishes to have the opportunity of exercising control over any subsequent alternative use in the interests of the amenities of the area. Relevant policy: Core Strategy policies CP1 & CP3.

5. The residential occupation of the building as a care and dwelling facility for vulnerable adults shall be limited to no more than two individuals requiring care at any one time.

Reason: In the interests of retaining the character of the area, character of the existing dwelling and the amenities of neighbouring occupants. Relevant Policy: Core Strategy policies CP1 and CP3.

6. Parking to be provided - No part of any building(s) hereby permitted shall be occupied or used until the vehicle parking space has been provided in accordance with the approved plans. The vehicle parking space shall be permanently maintained and remain available for the parking of vehicles at all times.

Reason: To ensure adequate on-site parking provision in the interests of highway safety, convenience and amenity. Relevant policy: Core Strategy policies CP3 & CP6 and Managing Development Delivery Local Plan policy CC07.

Informatives

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development as set out in the NPPF.

PLANNING HISTORY

Application Number	Proposal	Decision
081384	Proposed creation of new vehicular access, creation of 2 x off road parking spaces and erection of new timber access gates. Erection of 1.8m high close board fencing in place of steel palisade fencing	Approved 04/02/2009
092009	Application for a certificate of lawful proposed use for a single storey extension to bungalow	Refused 03/06/2009
090739	Application for a certificate of lawful proposed use for a single storey extension to bungalow	Refused 10/10/2009
090012	Proposed erection of new car port and covered walkway	Approved 11/02/2010

SUMMARY INFORMATION**For Commercial**

Site Area – 576m²

Existing Use – Residential Dwelling
(120.52m²)

Proposed Use – Residential institution
(138.07m²)

Number of jobs created – 3 full-time carers, working 1 at a time on a rota to support the permanent residents.

Existing parking spaces – 2 Spaces

Proposed parking spaces – 2 spaces

CONSULTATION RESPONSES

WBC Drainage	No objections
WBC Environmental Health	No objections
WBC Highways	Recommend conditional approval

REPRESENTATIONS

Town/Parish Council: No objections

Local Members: No comments received

Neighbours: No comments received

APPLICANTS POINTS

- The maximum number of residents will be 3, however the carer will not be in full time residence
 - *Officer comment – the number of permanent residents (vulnerable adults) would be 2. The additional side extension to implement an additional bedroom and bathroom would be used by the carer on shift at the time if they are needed to stay overnight.*
- The care staff are to work on shift patterns with different staff in attendance during day and night
- Wokingham Borough Council are installing sprinkler systems to all their domestic projects to protect human life and protect their building stock

PLANNING POLICY

National Policy	NPPF	National Planning Policy Framework
Adopted Core Strategy DPD 2010	CP1	Sustainable Development
	CP2	Inclusive Communities
	CP3	General Principles for Development
	CP4	Infrastructure Requirements
	CP6	Managing Travel Demand
	CP8	Thames Basin Heaths Special Protection Area
	CP9	Scale and Location of Development Proposals
	CP15	Employment Development
Adopted Managing Development Delivery Local Plan 2014	CC01	Presumption in Favour of Sustainable Development
	CC02	Development Limits
	CC03	Green Infrastructure, Trees and Landscaping
	CC04	Sustainable Design and Construction
	CC05	Renewable energy and decentralised energy networks
	CC06	Noise
	CC07	Parking
	CC09	Development and Flood Risk (from all sources)

		CC10	Sustainable Drainage
Supplementary Documents (SPD)	Planning	BDG	Borough Design Guide – Section 4
			DCLG – National Internal Space Standards

PLANNING ISSUES

Site Description:

1. The application site consists of a moderately sized two-bedroom bungalow-style property which includes a more recently constructed carport located on the eastern flank wall of the building. The site is also approximately 0.8 miles west of Woodley Town Centre and is set back approximately 7.4 metres from the road and it is accessed via (Fairwater Drive). The site is accessed through a private drive off of this street.
2. The building was originally part of the adjacent Highwood Primary School and served as accommodation for the school’s caretaker. The site is now under separate ownership.
3. The existing layout is a simple rectangular configuration, with the interior comprising of two bedrooms, a singular bathroom, a kitchen and separate lounge/dining area. The site harbours ample amenity space to the north/rear of the site and also has a large front garden located to the west of the site. The site includes only a minor amount of vegetation and hedgerows on the southern boundary shared with no.1 Fairwater Drive.
4. From the information provided in the Planning Statement, the lawful use of the premises is a single family residential dwelling (C3 use). The property is currently uninhabited.
5. To the north/ west of the site is the adjoining Highwood Primary School. To the east is a number of residential dwellings situated on Fairwater Drive. To the south is the adjoining 1 Fairwater Drive which is part of two dwelling semi-detached arrangement with no.3 Fairwater Drive. Situated just south-west is no.1 Kingfisher Drive.

Proposal Description:

6. This application seeks consent to change the use of Highwood Bungalow from a residential dwelling (Class C3) to a residential institution (Class C2) in order to care for vulnerable adults.
7. Alongside the change of use, the proposals also include the proposed construction of a single storey side extension to provide an extra bedroom and bathroom for carers of the residents following the demolition of the existing carport, as well as the installation of an internal sprinkler system. The proposed extension on the eastern flank wall of the building would adopt a shallow gable roof significantly set back from the building line and set down from the ridge height of the host building’s roof. The extension would measure approximately 3.4 metres in width and 4.8 metres in depth.

8. The proposed scheme would only include minor internal modifications to accommodate the extension. As the proposals would retain the existing concrete access ramp, it appears that the development is proposed to be fully wheelchair accessible.

Principle of Development:

9. The National Planning Policy Framework has an underlying presumption in favour of sustainable development which is carried through to the local Development Plan. The Managing Development Delivery Local Plan (MDD) Policy CC01 states that planning applications that accord with the policies in the Development Plan for Wokingham Borough will be approved without delay, unless material considerations indicate otherwise.
10. Policy CC02 of the MDD sets out the development limits for each settlement as defined on the policies map and therefore replaces the proposals map adopted through the Core Strategy, as per the requirement of policy CP9. Policy CP9 sets out that development proposals located within development limits will be acceptable in principle, having regard to the service provisions associated with the major, modest and limited categories. As the site is within a major development location, the proposal is acceptable in principle.
11. Policy CP3 of the Core Strategy states that development must be appropriate in terms of its scale of activity, mass, layout, built form, height, materials and character to the area in which it is located and must be of high quality design without detriment to the amenities of adjoining land uses and occupiers.
12. Section (i) of CP3 also highlights that planning permission will be granted for proposals that do not lead to a net loss of dwellings and other residential accommodation or land.
13. With regard to the proposed C2 use, paragraph 91 of the NPPF states that *decisions should aim to achieve healthy, inclusive and safe places which promote social interaction, including opportunities for meetings between people who might not otherwise come into contact with each other (...), are safe and accessible (...) and enable and support healthy lifestyles.*
14. Policy CP2 of the Core Strategy states that planning permission will be granted for proposals that address the requirements of:
 - An ageing population, particularly in terms of housing, health and wellbeing;
 - Children, young people and families, including the co-ordination of services to meet their needs;
 - People with special needs, including those with a physical, sensory or learning disability or problems accessing services
15. Policy TB09 of the Managing Development Delivery Local Plan (MDDLDP) (Residential accommodation for vulnerable groups) also states that the Council will in principle support proposals which provide for the following types of accommodation to provide for peoples' needs over a lifetime.

Loss of C3 Residential:

16. The proposed change of use to Class C2 would result in the loss of a residential dwelling, thus conflicting in part with policy CP3 of the Core Strategy which states that planning permission will be granted on schemes that “*do not lead to a net loss in dwellings and other residential accommodation or land*”. On the other hand, the proposal would still comply with parts (a) and (h) of Policy CP3.
17. The scheme provides great social benefit and an essential community facility for supporting vulnerable adults within the Borough, subsequently helping ensure essential care is delivered. The proposal therefore complies with Core Strategy Policy CP2 and policy TB09 of the MDDL. This far outweighs the loss of a residential dwelling, regardless of the minor policy conflict with Core Strategy CP3. As such, given the above it is considered that the principle of the proposed change of use is acceptable in planning policy terms.

Character of the Area:

18. The area is largely comprised of semi-detached and terraced residential dwellings, with the exception of Highwood Primary School, of which the site in question lies adjacent to.
19. Whilst the proposed C2 use would result in the loss of a residential dwelling, the primary nature of the proposed use would be quasi-residential and as such it is considered to be appropriate due to the existing residential form in the directly local area. A
20. In regards to the proposed side extension, it would comply with the Borough Design Guide in that it would be significantly set back from the building line, as well as set down from the ridge height of the host dwelling’s roof. Its design is considered to be modest and subservient to the host building and therefore the impact on the character of the area would be minimal.

Neighbouring Amenity:

21. It is considered that due to the nature of the proposals and minor the increase in mass and form of the building, there are no additional concerns on loss of light, overbearing or overlooking grounds.
22. Noise and Disturbance: the nearest residential developments are those located to the east and south of the site (nos. 1-3 and 14-24 Fairwater Drive and no.1 Kingfisher Drive). It is acknowledged that the proposal could result in a minor increase in the level of users within the site. However, the increase would be minimal and nuisance from vehicles particularly during the later hours of the day would see only a minor change from the vehicle usage of its original use as a residential dwelling. This is due to the day/night rotor proposed for the carers, which would include one carer in the institution at a time to look after the vulnerable adults in residence. The only exceptional circumstance where there would be more than one member of staff is in the case of a medical emergency.
23. Additionally, the external amenity areas would be used in the same way compared to a family dwelling. The WBC Environmental Health Officer has also raised no objections to the proposal. It follows that no unacceptable impact in terms of noise

and disturbance would be likely to occur.

Access and Movement:

24. Policy CP6 of the Core Strategy seeks to manage travel demand with sustainable travel modes, appropriate vehicle parking and minimisation of traffic impacts. Policy CC07 and Appendix 2 of the MDD Local Plan set out the minimum parking requirements for development proposals. Policy CP9 states that the scale of development must reflect the existing and proposed levels of facilities, services and accessibility at or in the location.
25. The site benefits from an existing vehicular access from Fairwater Drive, which would be utilised with the current scheme, and it is considered acceptable for access by all modes of transport, particularly in a medical emergency.
26. The WBC Highways Officer has raised no objections to this application on parking or highways grounds, however they have recommended conditional approval to ensure adequate on-site parking provision is secured and retained in the interests of highway safety, convenience and amenity.
27. On this basis, the proposal would be considered acceptable in terms of being sustainably located, and would not result in unacceptable parking pressures on site or surrounding streets, nor prejudice highway safety as a

Flooding and Drainage:

28. Policies CC09 and CC10 of the MDD Local Plan relate to flooding and drainage issues. The proposal site is located within Flood Zone 1 with low risk of surface water flooding. The Drainage Officer for this application has concluded that due to the nature of the proposals it is not classed as vulnerable development and they consequently raised no objections to the application.

Community Infrastructure Levy (CIL)

29. Community Infrastructure Levy: As the proposal is for a change of use and there is minimal additional floorspace proposed (less than 100sqm), the development is not CIL liable in accordance with the adopted CIL charging schedule.

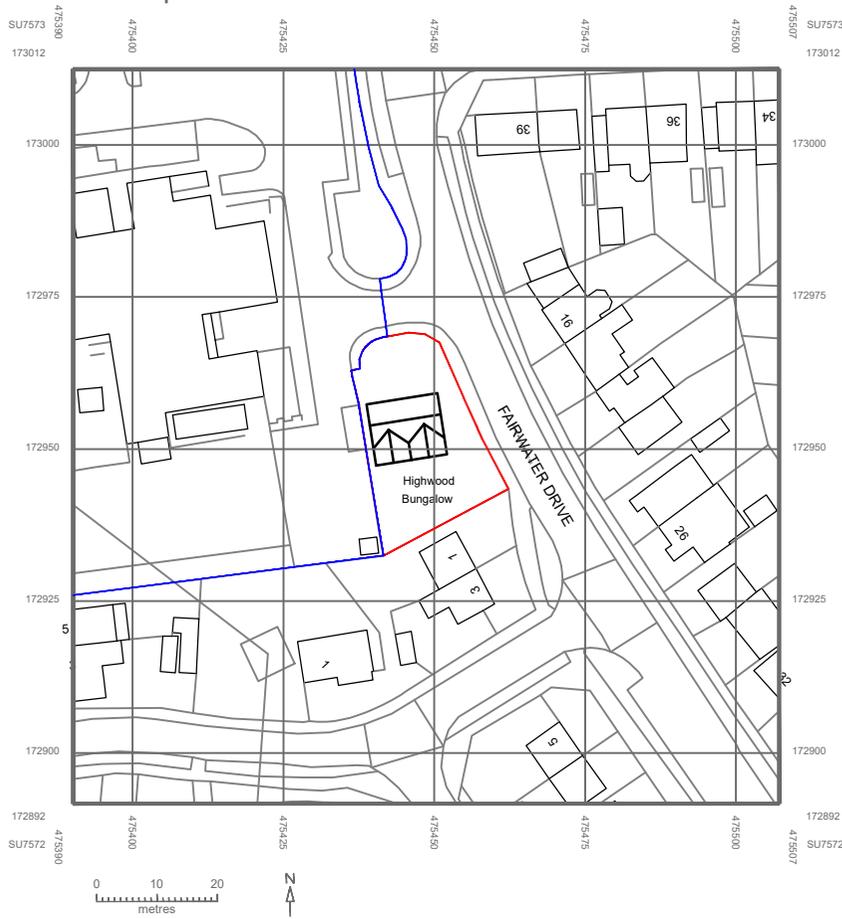
The Public Sector Equality Duty (Equality Act 2010)
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<i>In determining this application the Council is required to have due regard to its obligations under the Equality Act 2010. The key equalities protected characteristics include age, disability, gender, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief.</i>

CONCLUSION:

30. The proposed change of use from a residential dwelling (Class C3) to a Residential institution/nursing home (C2) is considered acceptable in principle, as the proposal would make a positive contribution to providing an essential community facility for supporting vulnerable adults within the Borough and help ensure essential infrastructure is delivered. The proposed extension to the building would be modest in scale and would not cause any harm to the character of the area, form of the building. As such, the proposal is considered to accord with national and local planning policies, and is therefore recommended for approval.

MapServe



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Thames Valley Surveying

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 Campus, London Road, Reading, RG1 5AQ
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Client: Wokingham Borough Council

Project: Highwood Bungalow

Title: Location Plan

Drawn by SM

Date 11/11/20

Scale: 1:1250@A4

Drawing Ref: 200178 - A001

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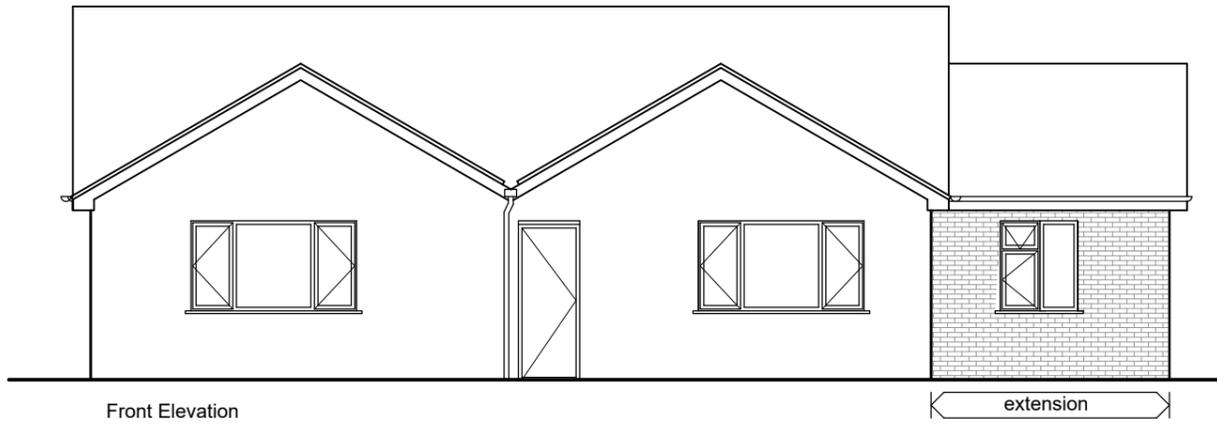


Thames Valley Surveying

Green Bank, University of Reading London Road
 Campus, London Road, Reading, RG1 5AQ
 T. 01189 869 531
 W. www.thamesvalleysurveying.co.uk

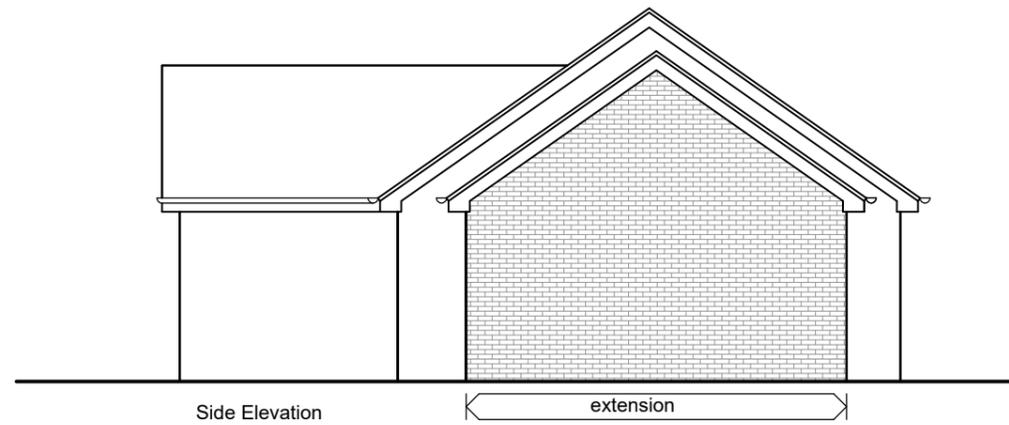
Client:	Wokingham Borough Council
Project:	Highwood Bungalow
Title:	Proposed Block Plan
Drawn by	SM
Date	11/11/20
Scale:	1:200@A4
Drawing Ref:	200178 - A005

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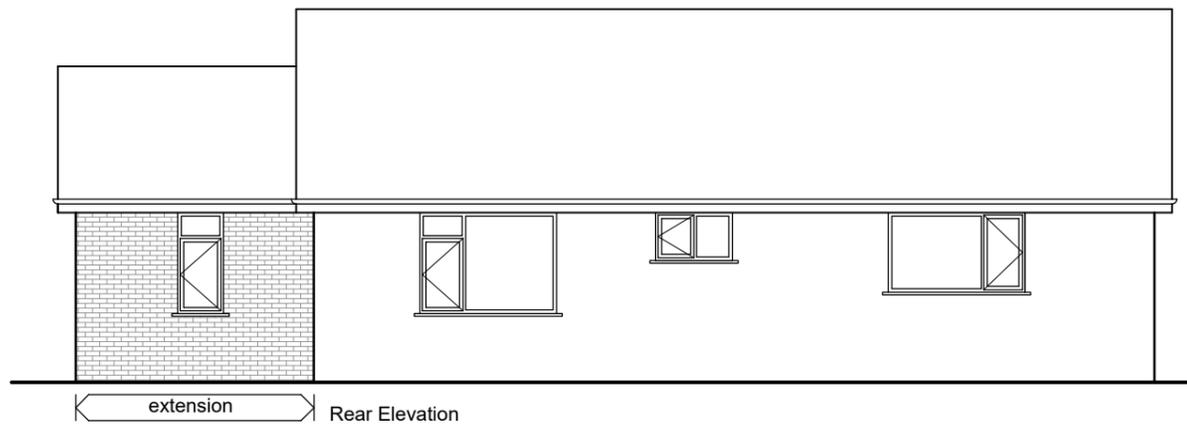
Front Elevation

extension



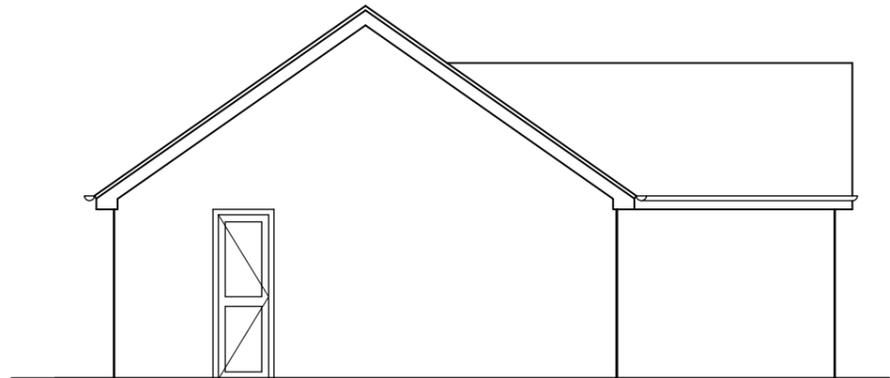
Side Elevation

extension



extension

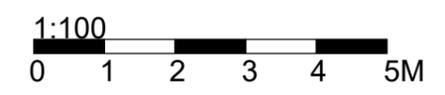
Rear Elevation



Side Elevation

Disclaimer Notes:
 • The Contractor must verify all dimensions on site
 • Do not rely on scaled dimensions from this drawing

Materials - to match existing	
Roof	- interlocking concrete tiles, white uPVC soffit and fascia
Rainwater Goods	- black uPVC gutters and downpipes
Walls	- brickwork
Windows	- white uPVC



Rev	Date	Description	By	Checked
A	01/12/20	Amended following client comments	SM	JW

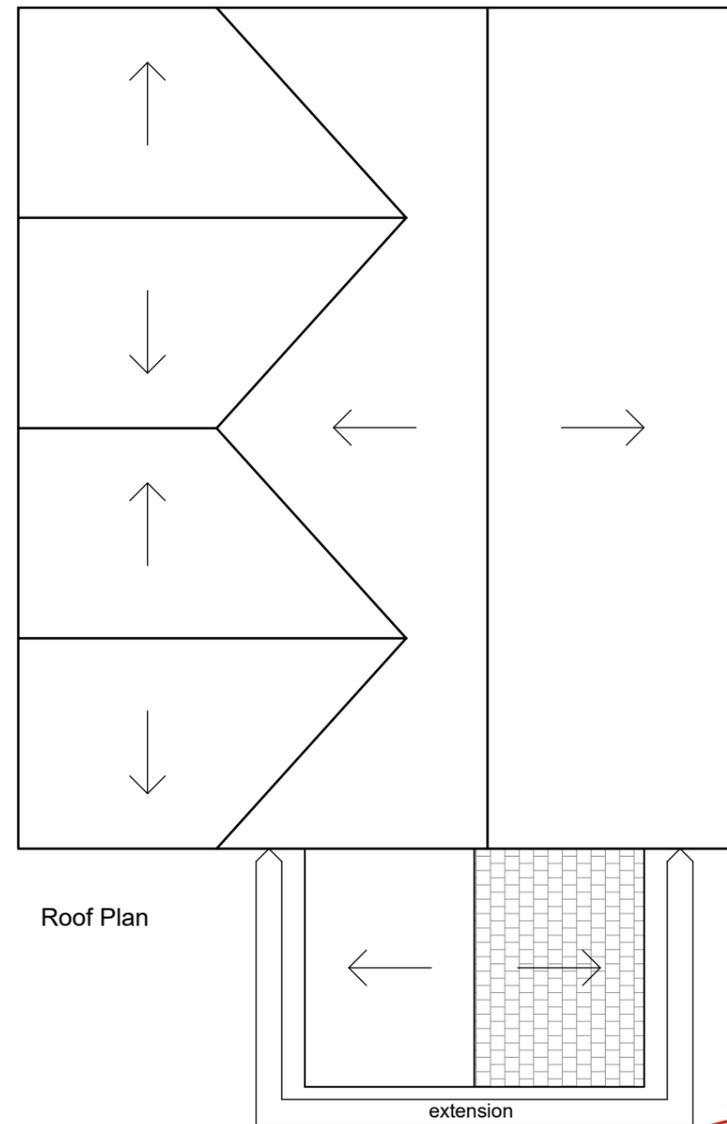
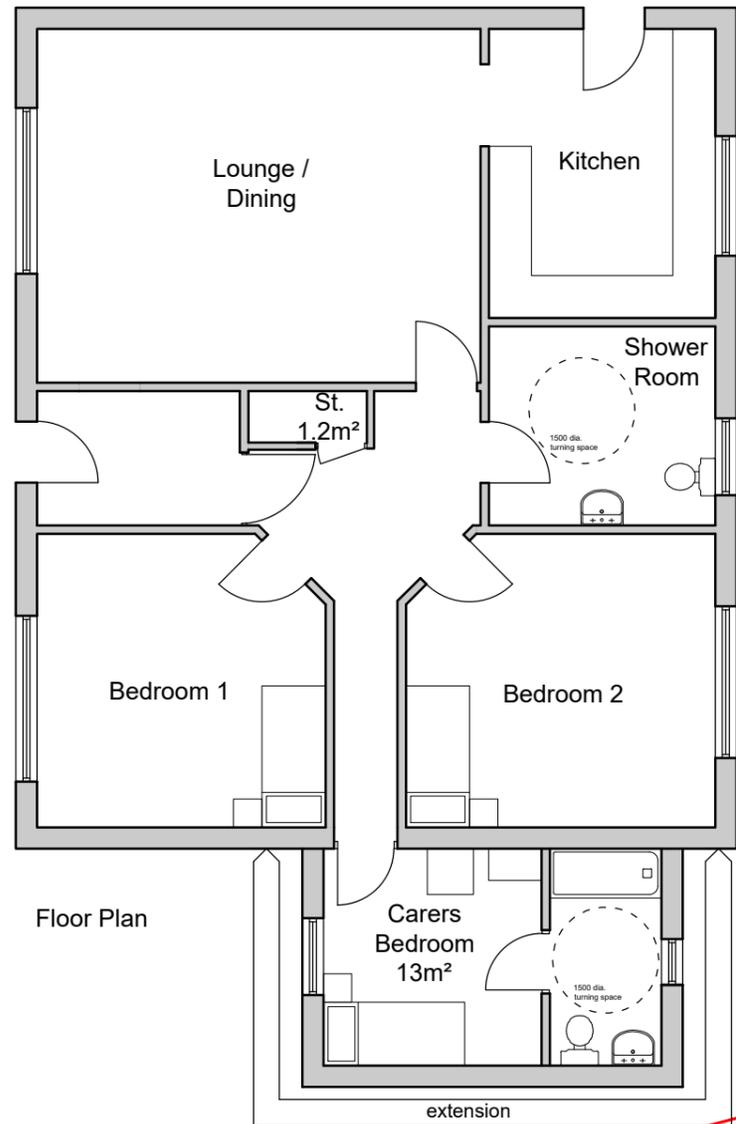


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Client:	Wokingham Borough Council
Project:	Highwood Bungalow
Title:	Proposed Elevations
Drawn by	SM
Date	12/11/20
Scale:	1:100@A3
Drawing Ref:	200178 - A007
Revision:	A

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Disclaimer Notes:
 • The Contractor must verify all dimensions on site
 • Do not rely on scaled dimensions from this drawing



Rev	Date	Description	By	Checked
B	01/12/20	Amended following client comments	SM	JW
A	25/11/20	Amended following client comments	SM	JW



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Client:	Wokingham Borough Council
Project:	Highwood Bungalow
Title:	Proposed Floor and Roof Plan
Drawn by:	SM
Date:	12/11/20
Scale:	1:100@A3
Drawing Ref:	200178 - A006
Revision:	B

Boundary Line

Boundary Line

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PLANNING REF : 203344
PROPERTY ADDRESS : The Oakwood Centre
: Headley Road, Woodley, Wokingham
: RG5 4JZ
SUBMITTED BY : Woodley Town Council
DATE SUBMITTED : 06/01/2021

COMMENTS:
No objections.

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